

**LODI SITE PLAN & ARCHITECTURAL
REVIEW COMMITTEE (SPARC)**

Teleconference Meeting

Streaming Link: <https://www.facebook.com/CityofLodi/>

AGENDA – Regular Meeting

Date: May 13, 2020

Time: Regular Session @ 5:15 PM

SPECIAL TELECONFERENCE NOTICE

Pursuant to Executive Order N-29-20:

The Brown Act, Government Code Section 54953, contains special requirements that apply when members of a legislative body participate in a public meeting by telephone. Certain of these requirements have been suspended by Paragraph 3 of Executive Order N-29-20, executed by the Governor of California on March 17, 2020 to mitigate the spread of the coronavirus known as COVID-19. In particular, the Executive Order suspends that provision of the Brown Act that requires noticing, posting of agendas, and public access to each location where a member will be participating telephonically, as well as provisions that require physical presence of members of the legislative body or the public for purposes of a quorum or to hold a meeting. Executive Order N-29-20 allows an agency to conduct a teleconference meeting that provides members of the public telephonic or other electronic participation in place of making a physical location for the public to observe the meeting and provide public comment, consistent with other provisions of the Brown Act.

The following members of the City of Lodi Site Plan and Architectural Review Committee (SPARC) are listed to permit them to appear telephonically at the Site Plan and Architectural Review Committee Meeting on May 13, 2020: Committee Members Mitchell Slater (Vice Chair)(PC Liaison), Pete Rosado, Roger Stafford and Trenton Diehl.

Public Comment:

As always, members of the public can send written comments to the SPARC prior to the meeting by emailing sparccomments@lodi.gov. These emails will be provided to the members of the SPARC and will become part of the official record of the meeting.

Members of the public who wish to verbally address the SPARC during the meeting should email those comments to sparccomments@lodi.gov. Comments must be received before the Chair announces that the time for public comment is closed. Staff will read three minutes of each email into the public record. **IMPORTANT:** identify the Agenda Item Number or description in the subject line of your email. Example: Public Comment for Agenda Item Number 4a Sunset.

Pursuant to the Americans with Disabilities Act (ADA) and Executive Order N-29-20, if you need special assistance to provide public comment in this meeting, please contact the Office of the Community Development Department at (209) 333-6711 or sparccomments@lodi.gov at least 48hours prior to the meeting in order for the City to make reasonable alternative arrangements for you to communicate your comments. If you need special assistance in this meeting for purposes other than providing public comment, please contact the Community Development Department at (209) 333-6711 or sparccomments@lodi.gov at least 48 hours prior to the meeting to enable the City to make reasonable arrangements to ensure accessibility to this meeting (28 CFR 35.160 (b) (1)).

Viewing:

Members of the public may view and listen to the meeting at: <https://www.facebook.com/CityofLodi/>

For information regarding this agenda please contact:
Kari Chadwick @ (209) 333-6711
Community Development Secretary

1. ROLL CALL

2. MINUTES – None

3. COMMENTS BY THE PUBLIC (NON-AGENDA ITEMS)

If you wish to address the Committee, please refer to the Special Teleconference Notice at the beginning of this agenda. Individuals are limited to one appearance during this section.

4. REVIEW ITEMS

- a. Request of Site Plan and Architecture Review Committee (SPARC) to consider Site Plan and Architectural Review for the Box Car development, a new “food court”-style dining establishment, at 216 N Church Street (Applicant: Christopher Bennitt; File Number: 2019-23 SP; CEQA Determination: Exempt Per Section 15301)

NOTE: The above item is a quasi-judicial hearing and requires disclosure of ex parte communications as set forth in Resolution No. 2006-31

5. REGULAR BUSINESS

- a. Discuss current procedures

6. COMMENTS BY SPARC MEMBERS & STAFF (NON-AGENDA ITEMS)

7. ADJOURNMENT

Pursuant to Section 54954.2(a) of the Government Code of the State of California, this agenda was posted at least 72 hours in advance of the scheduled meeting at a public place freely accessible to the public 24 hours a day.

****NOTICE:** Pursuant to Government Code §54954.3(a), public comments may be directed to the legislative body concerning any item contained on the agenda for this meeting before (in the case of a Closed Session item) or during consideration of the item.

Right of Appeal:

If you disagree with the decision of the commission, you have a right of appeal. Only persons who participated in the review process by submitting written or oral testimony, or by attending the public hearing, may appeal.

Pursuant to Lodi Municipal Code Section 17.72.110, actions of the Planning Commission may be appealed to the City Council by filing, within ten (10) business days, a written appeal with the City Clerk and payment of \$300.00 appeal fee. The appeal shall be processed in accordance with Chapter 17.88, Appeals, of the Lodi Municipal Code. Contact: City Clerk, City Hall 2nd Floor, 221 West Pine Street, Lodi, California 95240 – Phone: (209) 333-6702.



CITY OF LODI
SITE PLAN AND ARCHITECTURAL REVIEW COMMITTEE
Staff Report

MEETING DATE: January 13, 2019

APPLICATION NO: 2019-23 SP U

REQUEST: Request of Site Plan and Architecture Review Committee (SPARC) to consider Site Plan and Architectural Review for the Box Car development, a new “food court”-style dining establishment, at 216 N Church Street (Applicant: Christopher Bennitt; File Number: 2019-23 SP; CEQA Determination: Exempt Per Section 15332)

LOCATION: 216 N Church Street
APN: 043-023-01

APPLICANT: Christopher Bennitt
4643 Quail Lakes Drive, Suite 123
Stockton, CA 95207

PROPERTY OWNER: Christopher Bennitt
4643 Quail Lakes Drive, Suite 123
Stockton, CA 95207

RECOMMENDATION

Staff recommends the Site Plan and Architectural Review Committee recommend approval of the applicant’s proposal by the Planning Commission.

PROJECT/AREA DESCRIPTION

General Plan Designation: Downtown Mixed Use
Zoning Designation: DMU (Downtown Mixed Use)
Property Size: 13,600 sq. ft. / 0.31 acres

SUMMARY

The applicant, Christopher Bennitt, is proposing to develop a multi-tenant “food court”-style development at the southeast corner of Lockeford Street and Church Street. The proposed project would include the sale of alcohol by one or more of the tenants.

Two discretionary approvals are required:

- Site plan and architectural review of the proposed buildings; and
- Conditional Use Permit for the sale of alcohol

Because the project requires approval of a Conditional Use Permit by the Planning Commission, the site plan and architectural review is also being elevated to the Planning Commission. The SPARC will provide a recommendation only on the site plan and architectural review.

BACKGROUND

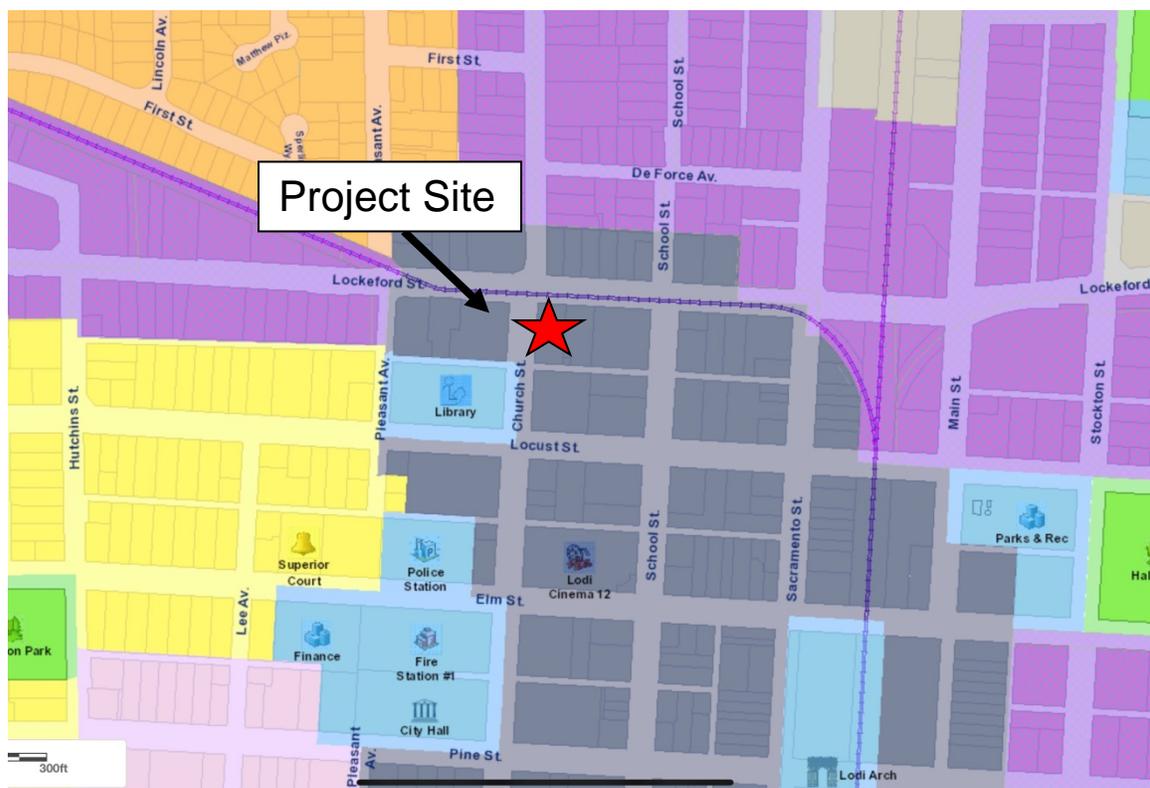
The following sections describe the site and its regulatory setting:

- General Plan and Zoning
- Downtown Development Standards and Guidelines
- Downtown Parking District
- Existing Land Uses

General Plan and Zoning

The site is designated Downtown Mixed Use on both the General Plan Land Use Map and the Zoning Map, as shown below.

Figure 1: General Plan



The Downtown Mixed Use land use designation is described as follows:

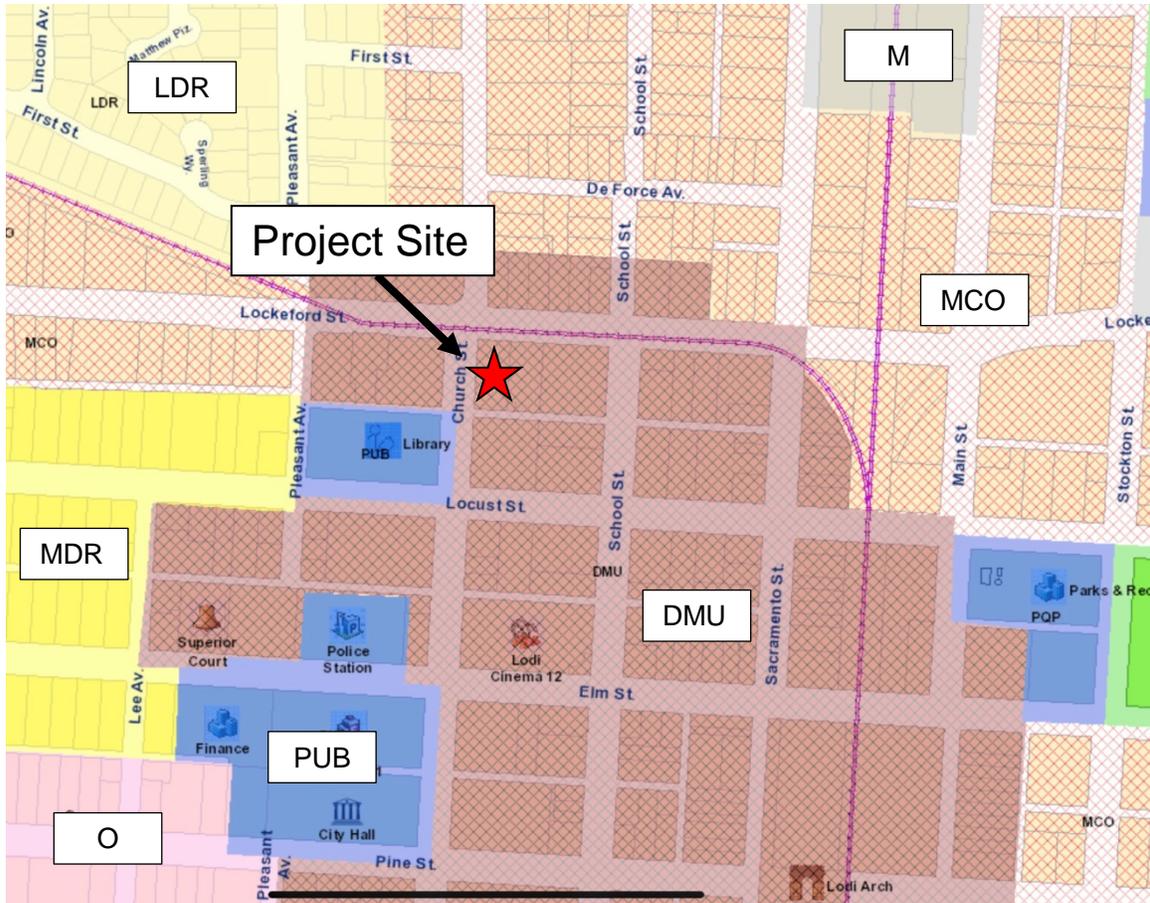
Downtown Mixed Use

Downtown Mixed Use is intended for a variety of commercial, office, public, and medium- and high-density residential uses on infill sites in the vicinity of Lodi's downtown. This classification encompasses an expanded downtown area, across the railroad tracks and extending past Main Street. This designation seeks to enable improved connections and coherence, but does not seek to expand downtown to the Eastside neighborhood; rather, the Eastside is expected to maintain its own character.

Retail uses or eating and drinking establishments are required at the ground level (see Chapter 4: Community Design and Livability for specified sites). This category will maintain the mix, scale and character of downtown development, while providing

opportunities for redevelopment of vacant, and underutilized sites. The maximum FAR for this designation is 3.0, which includes all residential and non-residential uses combined. At this intensity all parking is expected to be provided offsite; if on-site parking is provided, lower development intensities, as specified in the Zoning Ordinance, would be allowed. [Lodi General Plan Land Use Element, emphasis added]

Figure 2: Zoning



The Downtown Mixed Use (DMU) zoning district is described as follows:

*DMU (Downtown Mixed Use) District. The DMU zoning district is applied to the downtown area, and is intended to allow for a **broad range of commercial land uses primarily located on the ground floor**. Residential uses may also be allowed in this district. The maximum FAR is 3.00. The DMU zoning district is consistent with the downtown mixed use land use designation of the general plan. [Lodi Zoning Code, emphasis added]*

The site is in the northernmost portion of the Downtown Mixed Use area.

Downtown Development Standards and Guidelines

The site is located within the area covered by the City's Downtown Development Standards and Guidelines, adopted in 1997 to "...create and establish land use policies and design guidelines..." and "... serve as the tool kit for shaping Downtown Lodi's future."

The Downtown Development Standards and Guidelines that apply to this project include:

- Land Use
- Building Height and Setbacks
- Parking, Access and Landscaping
- Design Guidelines

Applicable standards and guidelines are discussed later in this report.

Downtown Parking District

The project site is located in the Downtown Parking District, established by the City in the 19##s to provide funding for the construction of shared parking, including the parking structure at Sacramento and Pine streets. By providing shared parking lots in the downtown, the Parking District eliminates the need for on-site parking for new uses in the downtown.

Existing Land Use

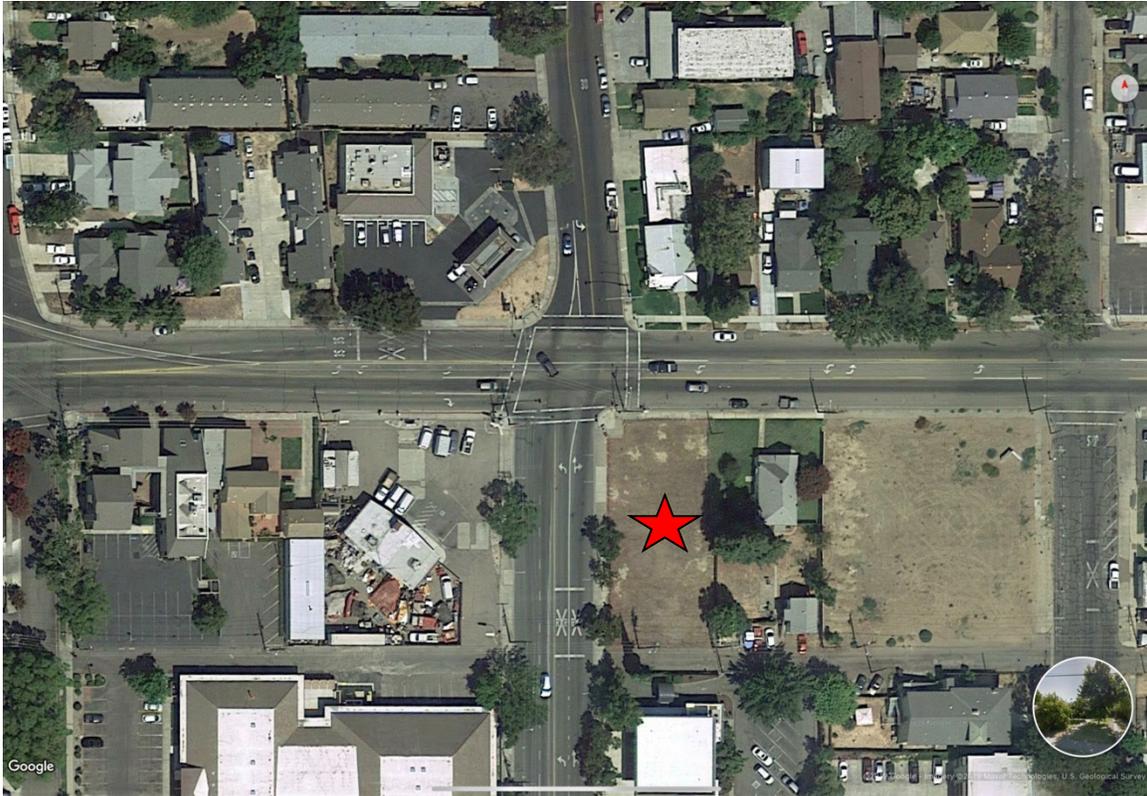
The site is currently vacant, as shown below (the home at left is on another parcel and is not part of the proposed project).

Figure 3: Existing Site



The area north of the site, across Lockeford Street, is a mix of commercial uses and single family homes, as shown in the aerial photo later in this report. Surrounding the site on the south side of Lockeford Street are a mix of mostly commercial uses, including several recently approved new commercial uses.

Figure 4: Aerial Photo

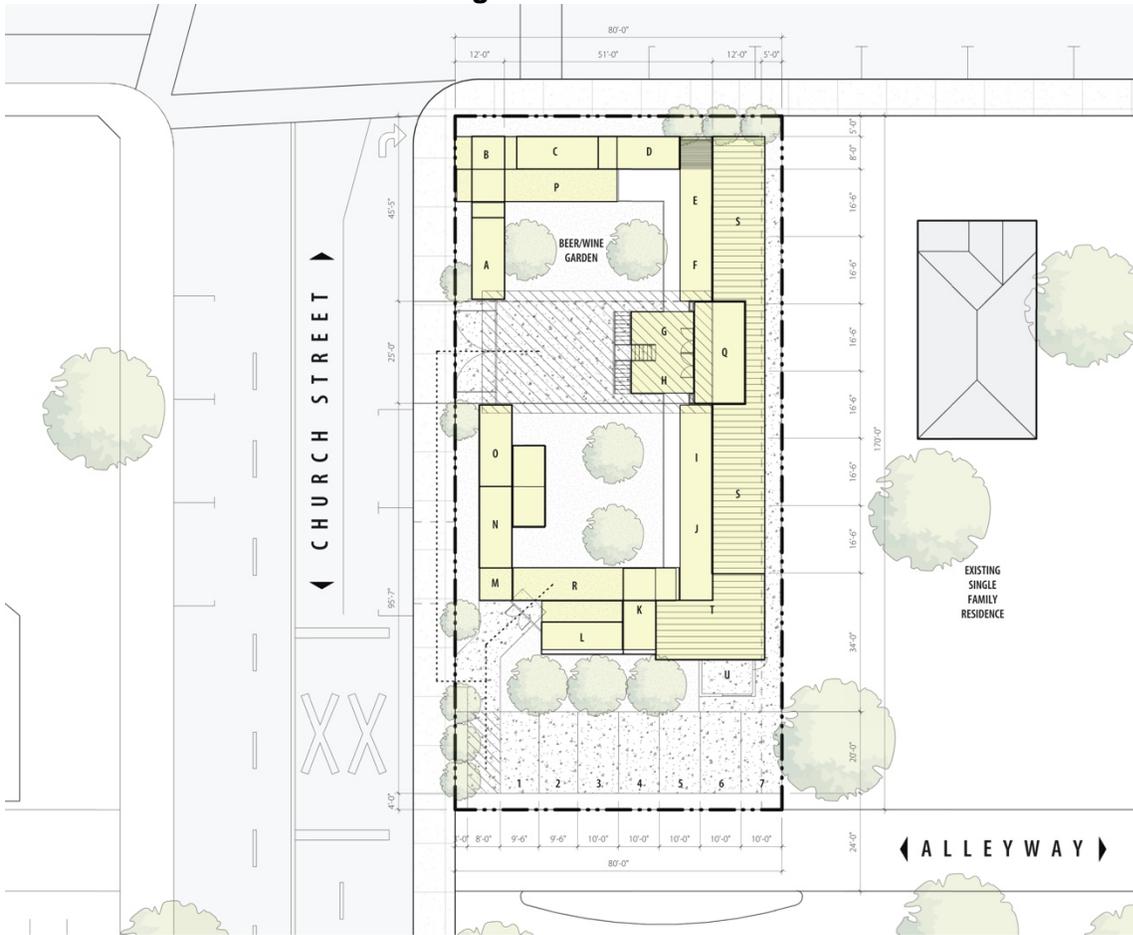


PROPOSED PROJECT

The applicant is proposing the construction of a “food court” type use, with a modern architectural design. Alcohol sales are also proposed, as described below.

The applicant’s proposal arranges repurposed shipping containers and site-built structures around a central courtyard, with kitchens on the east side of the complex (in the site-built portion of the structures). A second floor seating area would be also available to patrons. An overall site plan th the first and second floor plans are shown below.

Figure 5: Overall Site Plan



TOTAL: KEY

Figure 6: First Floor Plan

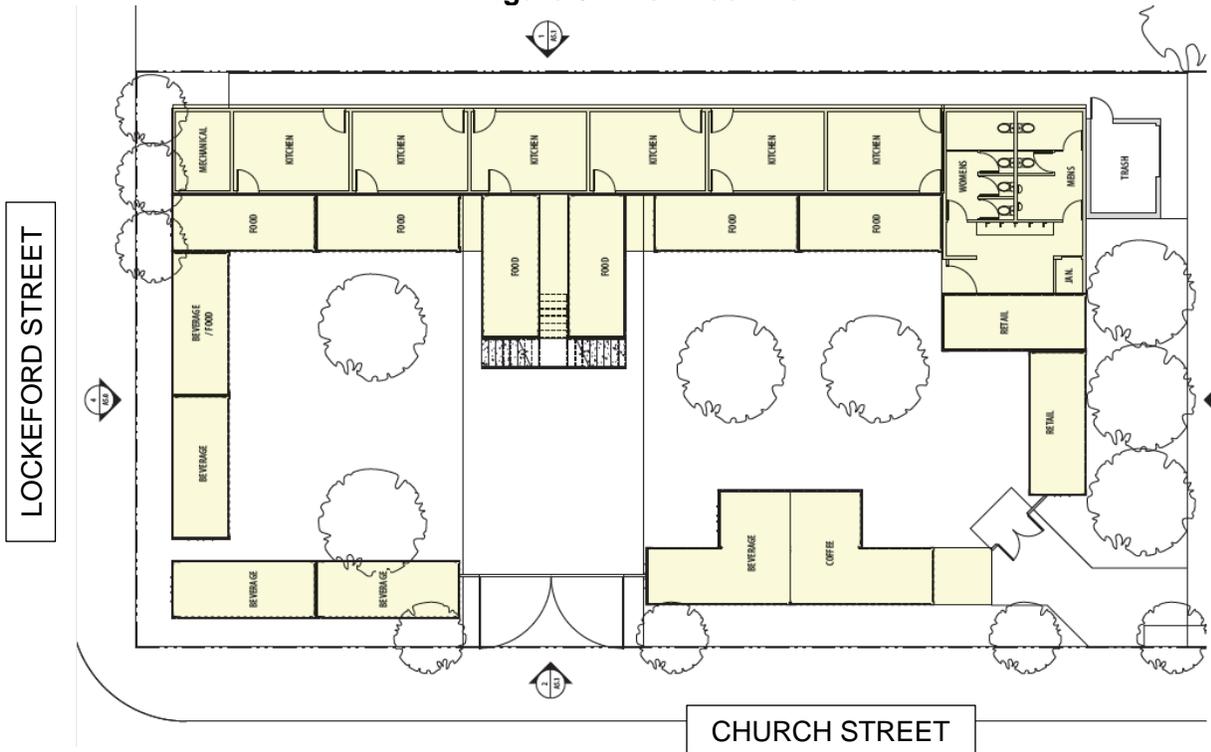
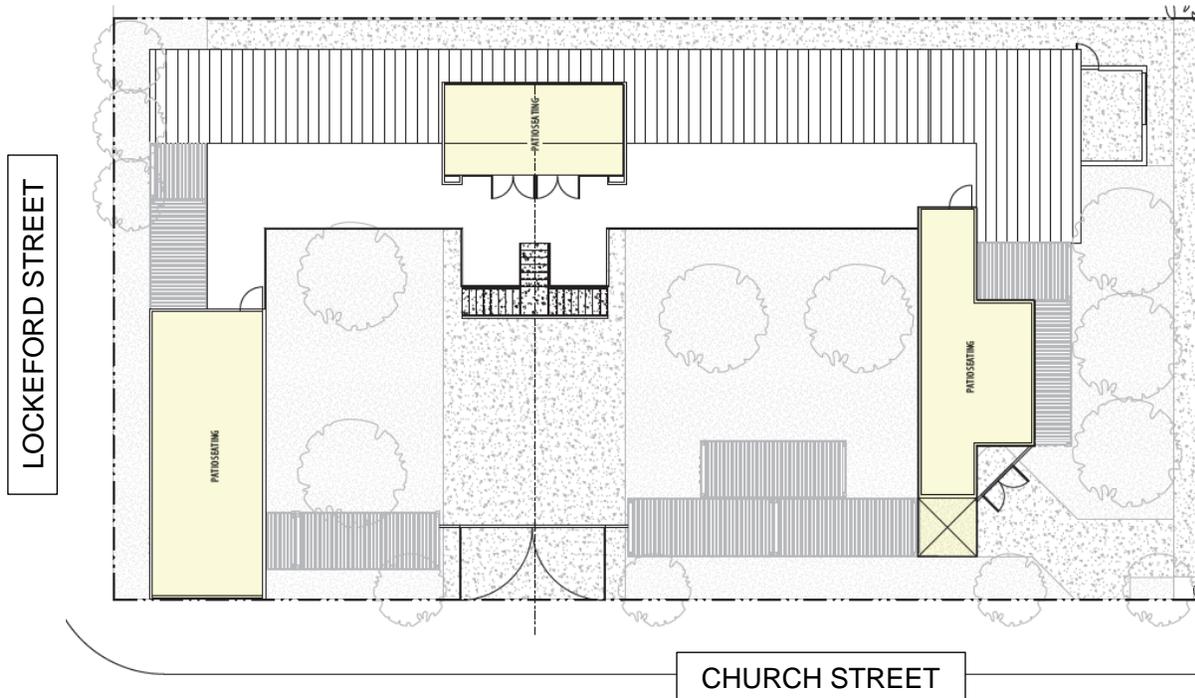


Figure 7: Second Floor Plan



Architectural Design

The proposed design uses steel shipping containers as a primary design theme. An illustration of the proposed project from the same vantage point as Figure 3, above, is shown below. Detailed elevations and site plans for the proposed project are included in the attachments to this report.

Figure 8: Perspective Illustration



The applicant is proposing a color palette that uses mostly shades of brown and grey. The shipping containers are proposed to be painted in a colors called “Rust” and “Burnt Orange.” The complete proposed color palette is shown below.

MATERIAL PALLET



The proposed project is similar to other recent food court projects elsewhere in California. The example below is the existing “Steelcraft” location in Bellflower, built several years ago. The same company has other locations in the same style in Bellflower and Garden Grove, both in Southern California.

Figure 9: Steelcraft Bellflower



This type of eating venue features a number of small eateries sharing a common seating area. Patrons purchase food and beverages from one or more vendors and select a table in the shared seating area.

Food courts (or “halls”) and similar establishments are a major trend in the restaurant industry. A recent article cited this statistic:

“From 2010 to 2017, there was a roughly 700% increase in the number of food halls in the United States, and in 2017 the number was expected to double by 2019.”¹

Similar types of food courts are in operation or planned in Sacramento, Stockton, and other local cities and in cities throughout California.

Locally, Lodi has seen a substantial increase in restaurants in the downtown area and elsewhere in town. Many of these dining venues offer alcoholic beverages.

Potential Tenants

The applicant proposes a mix of tenants in which at least half of the center’s sales would be created by food uses. Alcohol sales and merchandise would comprise the balance of the sales. A potential mix of tenants, compiled by the applicant, is shown below.

Table A: Potential Tenant Mix

Size	Tenant Type	Hours
240 SF	Coffee	7am – 9pm
240 SF	Desserts	5pm – 9pm
320 SF	Wine	11am – 10 pm
320 SF	Beer	11am – 10pm
350 SF	Tacos/Food	11am – 9 pm
350 SF	Pizza/Food	11am – 9pm
350 SF	Desserts	5pm – 9pm
350 SF	Deli	11am – 7pm
350 SF	Mediterranean/Food	11am – 7pm
350 SF	Poke/Food	11am – 7pm
160 SF	Bicycle Shop	10am – 5pm
160 SF	Merchandise	10am – 5pm

The mix of tenants, and their hours of operation, can be expected to vary over time, although as discussed in the Analysis section of this report the City can regulate hours of operation as they relate to alcohol sales. The City can also require that food sales comprise most of the revenues at the center.

Alcohol Sales

The applicant is proposing the sale of alcohol for on-site consumption, which requires a Use Permit. This proposal will be considered by the Planning Commission; the SPARC does not need to take action on this request.

ANALYSIS

The following sections address several topics associated with the proposed project:

¹ “The Origins of the Food Hall and Its Booming Popularity,” February 15, 2019. WTTW TV Chicago. <https://interactive.wttw.com/playlist/2019/02/15/food-halls>

- Development Standards
- Architectural Design
- Parking

Staff's analysis of these issues is provided below.

Development Standards

The following development standards of the DMU (Downtown Mixed Use) zoning district apply to the proposed project:

Front setback:	None
Street Side setback:	None
Side setback:	None
Rear setback:	None
Height Limit:	Minimum 20 feet, Maximum 6 stories/75 feet
Floor area ratio:	3.0 maximum

The project complies with all of these standards. The buildings have elements that are less than 20 feet in height, but the second-floor roof is at least 20 feet high at its highest point and meets the minimum height standard.

Architectural Design

The applicant is proposing a building which makes a distinctive architectural statement. The use of repurposed shipping containers as part of a building, which has been used in in many places in the US, would be new to Lodi (except where shipping containers are used for storage).

This "industrial" aesthetic represents a departure from other buildings in the vicinity, which represent a variety of more traditional styles (as shown in the photos below).

Figure 10: Retail Buildings, NEC Lockeford Street @ N School Street



Figure 11: Gas Station, NWC Lockeford Street @ N School Street



Figure 12: Retail Buildings, N Sacramento Street south of W Lockeford Street



Figure 13: Lodi Public Library, N Church Street



Figure 14: BBVA/Compass Bank, N Church Street



The proposed project is similar in style to the recently approved bowling alley center at Lockeford/N Sacramento, which reflects a more modern, industrial aesthetic, as shown below.

Figure 15: Bowling Alley Exterior



Figure 16: Bowling Alley Interior



The proposed Box Car facility is located in an area without a clearly defined architectural character and which already contains buildings in a variety of traditional and more modern styles. There is no clearly defined “context” within which the proposed project would need to fit.

The City’s “Downtown Development Standards & Guidelines” (1997), which apply to an area that includes the Box Car location, provide suggestions for the design of new building. These include recommendations about “Building Composition,” “Storefronts,” “Building Increment,” “Special Architectural Features” and other considerations.

Taken as a whole, the recommendations in the Guidelines generally call for buildings which mimic the older portions of the downtown, as shown in the example of desired design shown in the excerpt to the right.

The architectural recommendations, however, are not mandatory and are presented as,

“... statements to encourage improvement which is sensitive to the Downtown.”

And while the illustrations and standards suggest historically themed designs, the Guidelines explicitly state that,

“No particular architectural style is prescribed.”

Staff has reviewed the proposed Box Car facility in terms of the recommendations of the Guidelines. In summary, staff’s determination is that the proposed project’s architectural design and materials do not directly respond to the guidelines, primarily because the “historic” type features which the Guidelines generally seek are not provided.

However, staff does feel that the project’s design implements the City’s desire for an improved and revitalized downtown, albeit in a more modern form. As noted, the Box Car project is also similar in many respects to the recently approved bowling alley project, which is currently under construction.



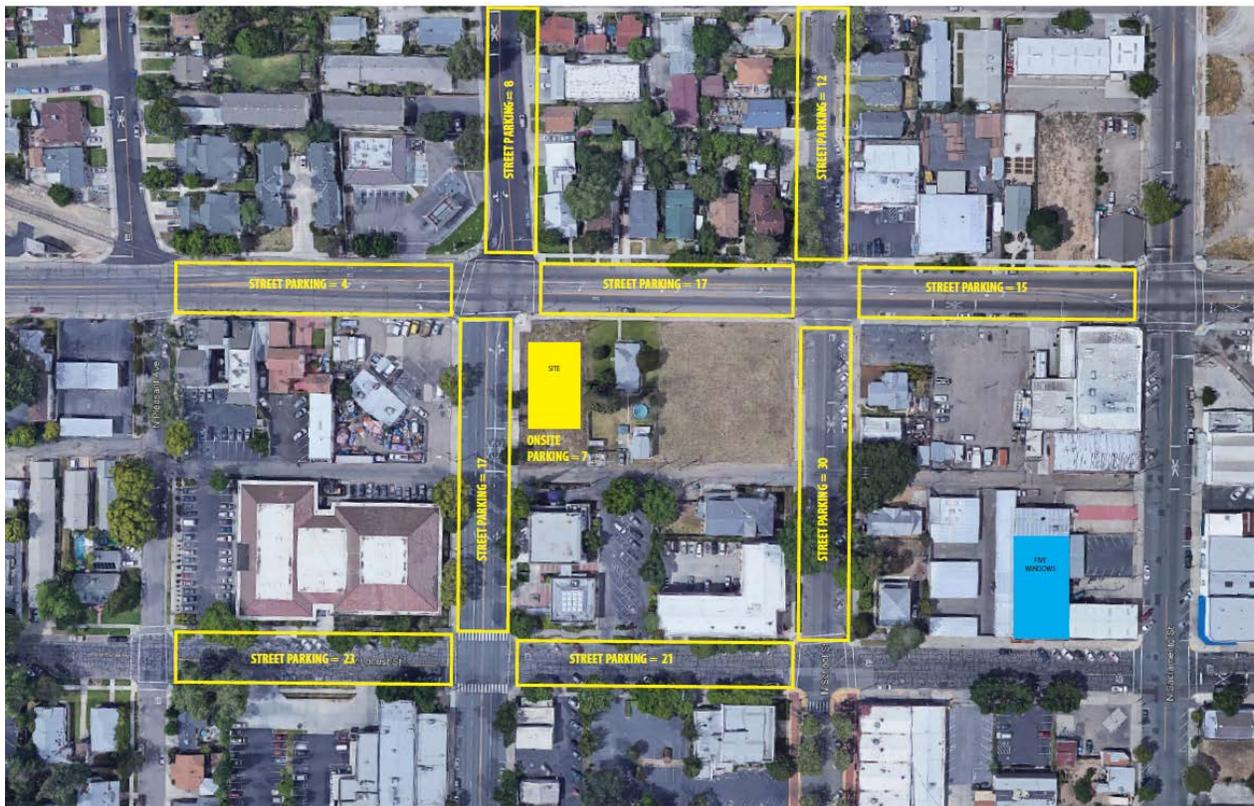
Parking

Because the project is located in the Downtown Parking District, no onsite parking is required (other than ADA-accessible spaces). The project's location in the Downtown Parking District eliminates the requirement to provide on-site parking. The City-owned downtown parking structure, combined with available onsite parking, is considered to provide adequate parking.

If the project were located elsewhere in Lodi, where standard parking requirements are in place, parking would be based on the number of seats. The Zoning Code requires one parking space for each four seats in a "Restaurant/bar/night club." Because parking is not required, staff has not prepared an analysis of the number of spaces that would be needed for this project.

The applicant has prepared the exhibit below, showing the locations of onstreet parking spaces within a short walking distance of the project site (a total of 137 spaces). The applicant feels that this parking, which will be mostly available during the evening and weekend hours when the proposed project will be most active, will be sufficient to meet the needs of patrons. The project will provide seven onsite parking spaces, including one ADA-compliant space.

Figure 17: Available Onstreet Parking



Source: Applicant

ANALYSIS AND FINDINGS

Staff's analysis of the required findings for Site Plan and Architectural Review is provided below.

Findings: Site Plan and Architectural Review

Approval of a Site Plan and Architectural Review requires that the following findings can be made:

17.40.020.E: The applicable review authority may approve a site plan and architectural approval application only after first finding that:

1. *The design and layout of the proposed project would:*

- a. Be consistent with the development and design standards/guidelines of the applicable zoning district;*
- b. Not interfere with the use and enjoyment of neighboring existing or future developments, and not create traffic or pedestrian hazards;*
- c. Maintain and enhance the attractive, harmonious, and orderly development contemplated by this development code; and*
- d. Provide a desirable environment for its occupants, neighbors, and visiting public through good aesthetic use of durable materials, texture, and color.*

2. *The proposed development:*

- a. Would not be detrimental to the public health, safety, or welfare or materially injurious to the properties or improvements in the vicinity; and*
- b. Has been reviewed in compliance with the California Environmental Quality Act (CEQA) and the Lodi Environmental Review Guidelines.*

The project has been reviewed in accordance with the City's General Plan, the Municipal Development Code standards, and the City's Design Guidelines. Based upon City evaluation and analysis, staff recommends that the SPARC adopt the findings of this report and approve the proposed project.

Staff recommends that the Commission make the following findings in order to approve the Site Plan and Architectural Review for the proposed project:

- 1. *The design is consistent and compatible with the design standards/guidelines of the applicable zoning district. Comment: The applicant is proposing a modern-themed development that is consistent with other recent approvals in the vicinity and uses colors and materials in an imaginative way. The proposed project would in staff's opinion be a positive attribute for Lodi. While the design is not "historic," it would add to the level of quality of development in the downtown and, by attracting new customers, would add to the overall level of vitality of this area.*
- 2. *The project will not interfere with the use and enjoyment of neighboring existing or future developments, and does not create traffic or pedestrian hazards. Comment: The proposal does not include any new buildings or improvements that would create traffic or pedestrian hazards. The proposal has reviewed by the Public Works department, which found no issues related to public safety.*
- 3. *The project will maintain and enhance the affirmative, harmonious, and orderly development contemplated by the Development Code. Comment: The proposed new buildings are visually compatible with the rest of the commercial center and neighborhood. The project is similar in its aesthetic to the recently approved bowling center, and will add to the overall level of quality of development in the area.*

4. *The project provides a desirable environment for its occupants, neighbors, and visiting public through good aesthetic use of durable materials, texture, and color. Comment: The project design promotes visual environments that are of high aesthetic quality and variety.*
5. *The project will not be detrimental to the public health, safety, or welfare or materially injurious to the properties or improvements in the vicinity. Comment: The project is consistent with the Lodi Development Code and adopted City standards. The project proposal is an investment into an existing vacant lot which currently does nothing to contribute to the vitality of the downtown area. All improvements are on private property and approval of the proposed project would not result in any significant effects relating to traffic, noise, air quality, or water quality. The site of the proposed project can be adequately served by all required utilities and public services.*
6. *The project has been reviewed in compliance with the California Environmental Quality Act (CEQA) and the Lodi Environmental Review Guidelines. Comment: The project is found to be categorically exempt from CEQA review under 14 CCR §15332. Class 32 exemptions consist of projects characterized as infill developments that, a) are consistent with the applicable general plan designation and applicable plan policies as well as applicable zoning designation and regulations, b) occur within city limits on a project site of no more than five acres substantially surrounded by urban development, c) the project site has no value as habitat for endangered, rare, or threatened species, d) approval of the project would not result in any significant effects relating to traffic, noise, air quality or water quality, and e) the site can be adequately served by all required utilities and public services. The project proposal for a change of use meets the requirements of the Class 32 CEQA exemption*

CONDITIONS OF APPROVAL

Staff's recommended conditions of approval for the proposed project are included in the attachments to this report. In addition to the Committee's Common Design Requirements, staff recommends approval of the applicant's request, subject to the proposed conditions of approval, which are included in the attachments to this staff report.

PUBLIC HEARING NOTICE

Legal Notice for the Use Permit was published in the Lodi News Sentinel on Saturday, May 2, 2020. Twenty-seven (27) public hearing notices were sent to all property owners of record within a 300-foot radius of the project site as required by California State Law §65091 (a) 3. Public notice also was mailed to interested parties who expressed their interest of the project.

RECOMMENDED MOTION AND OPTIONS

If the Commission agrees with staff's recommendation, the following motion is suggested:

"I move that the Commission find that the proposed site plan and architectural review application for the Boxcar development at 216 N Church Street is exempt from CEQA as a Class 32 exemption and that the Commission approve the proposed development number 2019-23 SP, subject to conditions of approval."

If the Commission does not agree with staff's recommendation, the following options are available:

- Deny the proposed project.
- Approve the proposed project with revised conditions of approval.

- Continue this item to a future meeting and provide direction to staff and the applicant on changes desired by the Commission and/or additional information needed for the Commission to make a decision on the project.

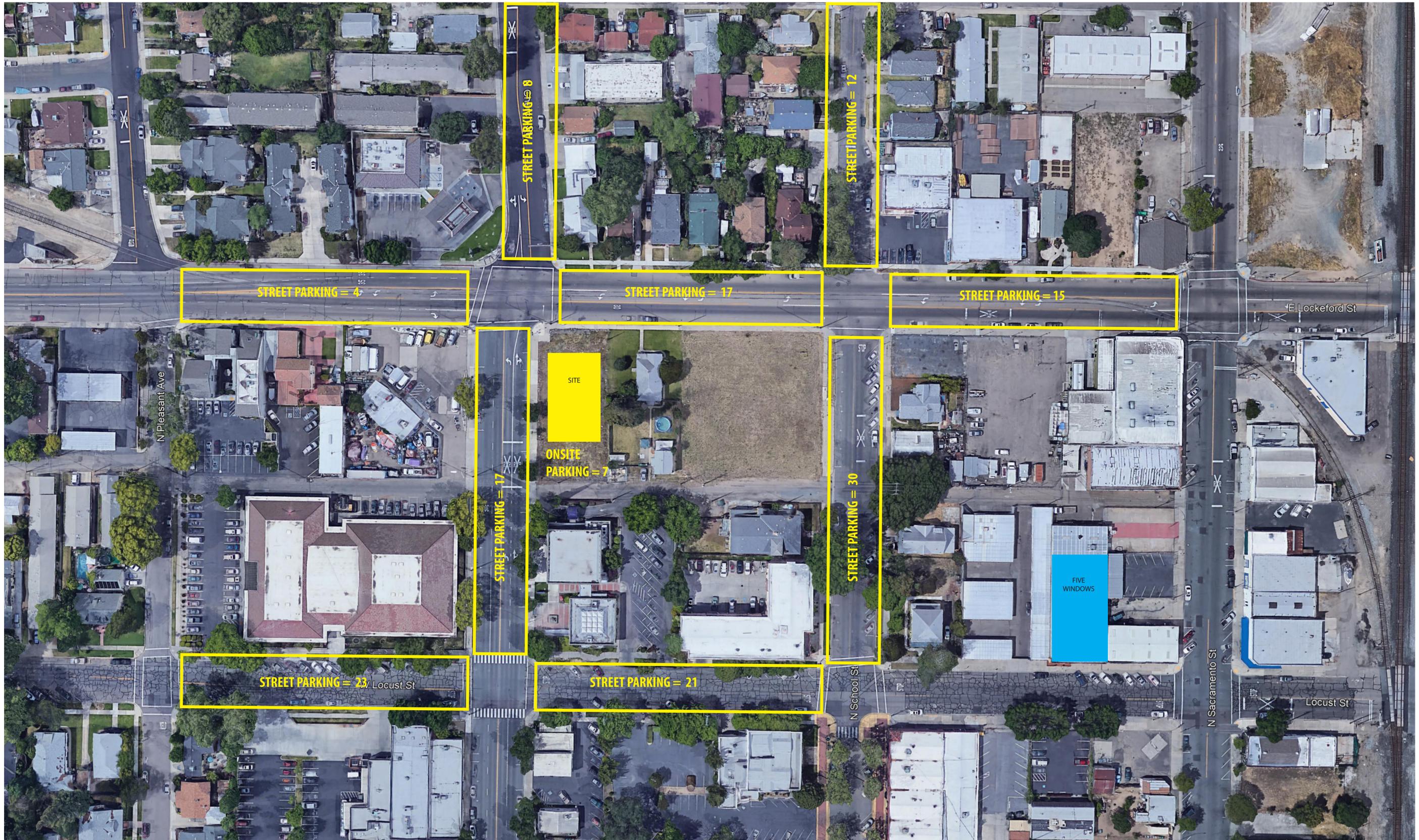
Respectfully Submitted,

Eric Norris
Contract Planner

John R. Della Monica Jr.
Community Development Director

ATTACHMENTS:

- A. Detailed Elevations and Site Plan
- B. Conditions of Approval



PARKING PLAN

BOX CAR

10.11.19

SITE APPROVAL

**TOTAL OFF STREET PARKING WITHIN 1 BLOCK RADIUS: 147 SPACES
+ 7 ON-SITE + 10 BICYCLE**



PARKING REQUIREMENTS

CODE	SPACES	REQ'D.	PROVD.
CBC TABLE 11B-208.2	STANDARD STALLS	-	6
	VAN ACCESSIBLE STALLS	1	1
	TOTAL STALLS	-	7
CGBCS 5.106.4.1.1	SHORT-TERM BICYCLE PARKING	-	8

STRUCTURAL MATRIX

STRUCTURE	USE	SQUARE FOOTAGE
A	BEVERAGE	160
B	BEVERAGE	160
C	BEVERAGE	160
D	BEVERAGE/FOOD	160
E	FOOD	160
F	FOOD	160
G	FOOD	160
H	FOOD	160
I	FOOD	160
J	FOOD	160
K	RETAIL	160
L	RETAIL	160
M	TOWER/BEVERAGE	64
N	COFFEE	160
O	BEVERAGE	160
P	UPSTAIRS PATIO	320
Q	UPSTAIRS PATIO	200
R	UPSTAIRS PATIO	1400
S	KITCHEN	1400
T	RESTROOMS	500
U	TRASH ENCLOSURE	120

TOTAL: 6,244 SF

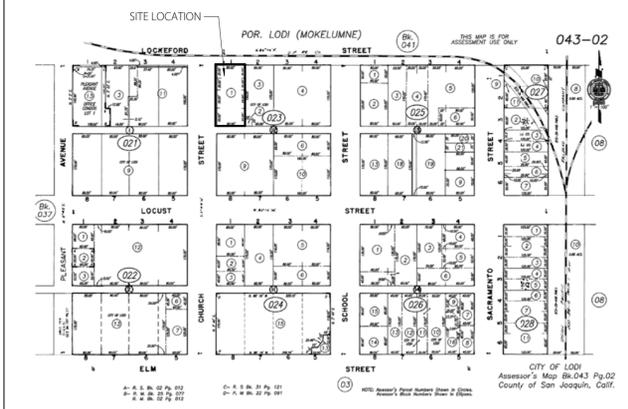
KEYNOTES

LEGEND

- PROPERTY LINE
- ASSUMED ACCESSIBLE PATH OF TRAVEL FOR ADDITIONAL INFO, REFER TO SHEET G4.0 ACCESSIBILITY STANDARDS.
- [Hatched Box] PROPOSED STRUCTURE
- [Diagonal Lines Box] PROPOSED CANOPY
- [Dotted Box] NEW CONCRETE WALK / FLAT WORK
- [Stippled Box] DECOMPOSED GRANITE

GENERAL NOTES

1. VERIFY ALL DIMENSIONS, EXISTING CONDITIONS AND METHODS OF CONSTRUCTION PRIOR TO START OF ANY WORK. NOTIFY ARCHITECT OF ANY DISCREPANCIES.
2. UNLESS OTHERWISE NOTED OR INDICATED, ALL DIMENSIONS SHOWN ARE TO FACE OF FRAMING, CONC. SLAB AND/OR CMU.
3. CONTRACTOR SHALL WORK TOWARD DIMENSIONS WITH TOLERANCES +/- SHOWN.
4. PATCH AND REPAIR ALL EXISTING ADJACENT SURFACES AFFECTED BY THE IMPROVEMENTS PROPOSED IN THESE CONTRACT DOCUMENTS. RESTORE FINISHES TO THESE AREAS TO MATCH EXISTING.
5. THE PEDESTRIAN PATH OF TRAVEL (P.O.T.) IS INDICATED AS A BARRIER FREE, COMMON ACCESS ROUTE WITHOUT ANY ABRUPT VERTICAL CHANGES EXCEEDING 1/2" BEVELED AT 1:2 MAX. SLOPE, EXCEPT THAT LEVEL CHANGES DO NOT EXCEED 1/4" VERTICAL. THE PATHS SURFACE IS AT LEAST 48" WIDE, SLIP RESISTANT, STABLE, FIRM AND SMOOTH. PASSING SPACES AT LEAST 60"x60" ARE LOCATED NOT MORE THAN 200' APART. PART OF P.O.T. W/ CONTINUOUS GRADIENTS HAVE 60" LEVEL AREAS NOT MORE THAN 400' APART. THE CROSS-SLOPE DOES NOT EXCEED 2% AND THE SLOPES IN THE DIRECTION OF TRAVEL THAT EXCEED 5% ARE CONSTRUCTED AS ACCESSIBLE RAMPS. THE P.O.T. IS FREE OF OVERHEAD OBSTRUCTIONS UP TO 80" MIN. OBJECTS PROJECTING FROM WALLS W/ LEADING EDGES BETWEEN 27" & 80" A.F.F. DO NOT PROTRUDE MORE THAN 4" FROM THE WALL. THE CONTRACTOR SHALL VERIFY THAT ALL BARRIERS IN THE P.O.T. HAVE BEEN OR WILL BE REMOVED UNDER THIS PROJECT AND COMPLIES W/ CBC 11B-402, & 11B-403.
6. (WHEN PROVIDED) ALL GRATES IN PATH OF TRAVEL MUST COMPLY WITH DOOR REQUIREMENTS. PROVIDE A 24" MIN. STRIKE SIDE CLEARANCE ON THE PULL SIDE AND A SMOOTH SURFACE AT THE BOTTOM 10 INCHES OF THE PUSH SIDE OF GATE. SPECIFY ACCESSIBLE / LEVER HARDWARE.
7. PROVIDE BUILDING ENTRANCE SIGN (INTERNATIONAL SYMBOL OF ACCESSIBILITY) AT ACCESSIBLE PUBLIC ENTRANCE DOOR(S). MINIMUM SIZE REQUIRED IS 6'x6'.
8. PROVIDE TACTILE STAIR LEVEL IDENTIFICATION SIGN IN EACH FLOOR LEVEL LANDING.



PARCEL MAP



ARCHITECT
NJ ASSOCIATES
 212 W. PINE STREET, STE #1
 LODI, CA 95240
 209.727.2040
 www.njarchitecture.com

CLIENT
Christopher Bennett

PROJECT
BOX CAR EATERY
 216 Church Street
 Lodi CA 95240



ARCHITECTS STAMP

REVISIONS

No.	Description	Date	By

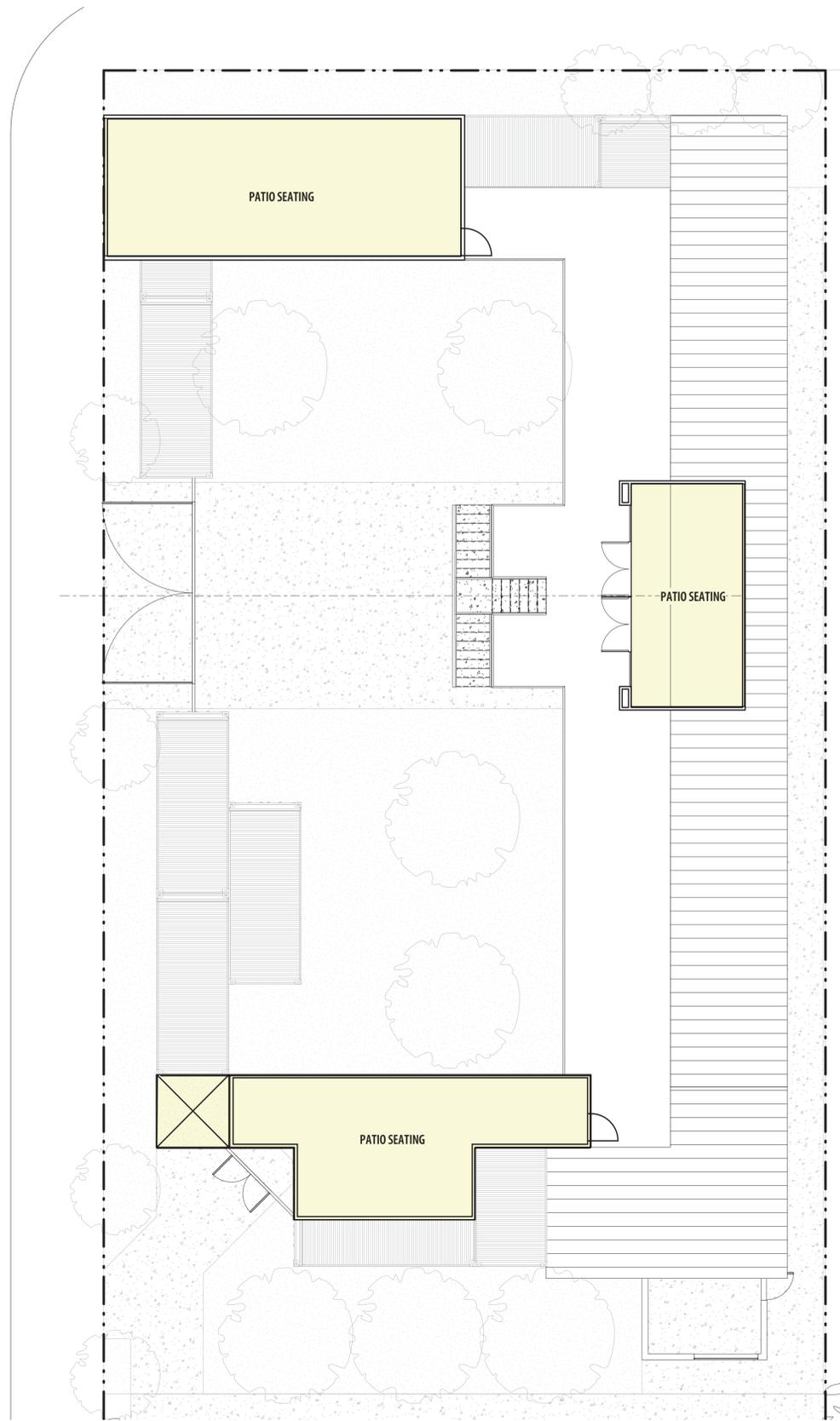
Project No.: 19027
 Drawn By:
 Checked By: W

NOT FOR CONSTRUCTION
SITE APPROVAL
 10.11.19

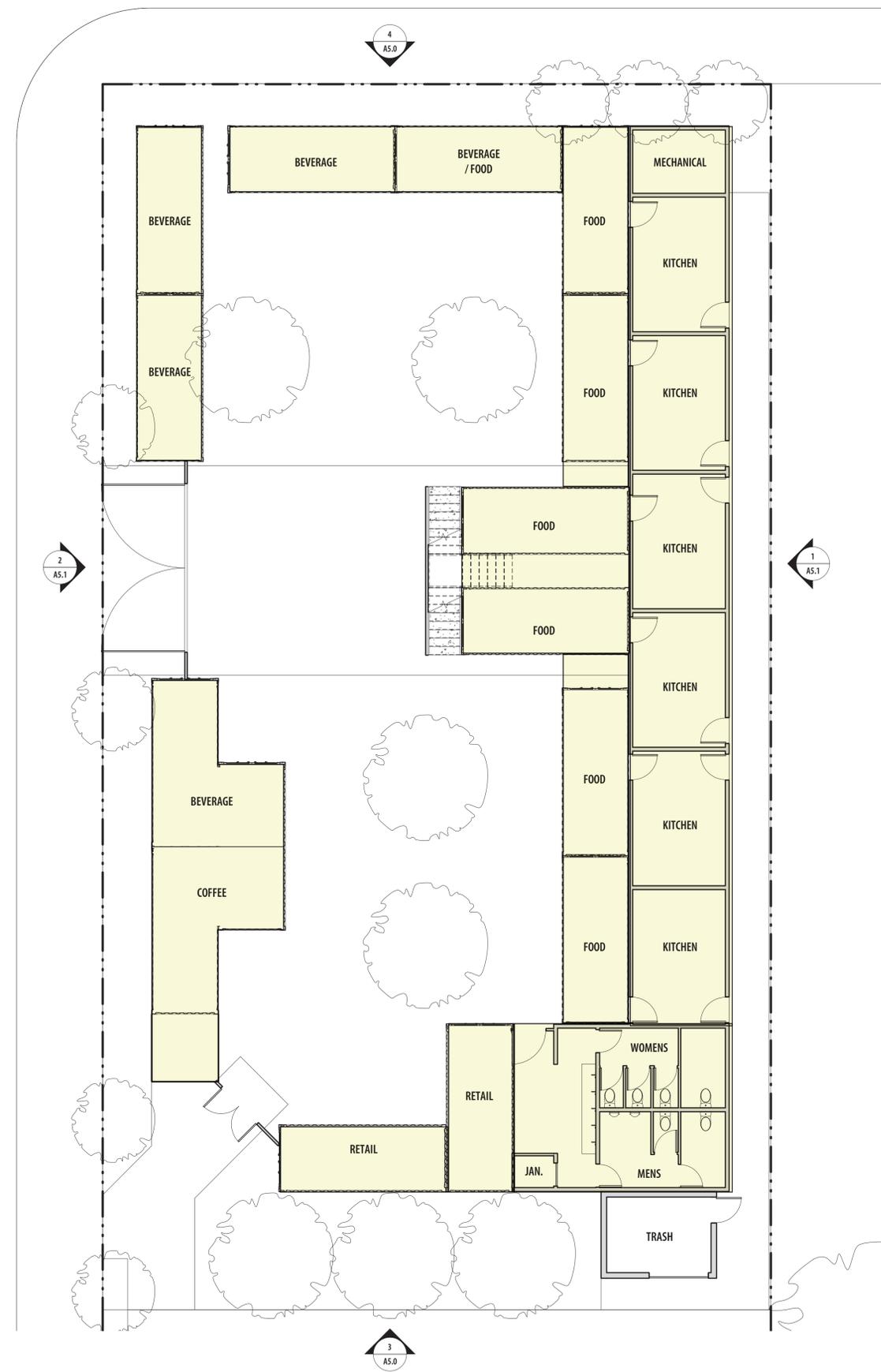
SITE PLAN

A1.0

1 SITE PLAN
 A1.0 1/16" = 1'-0"



2 SECOND FLOOR PLAN
A2.0 1/8" = 1'-0"



1 FIRST FLOOR PLAN
A2.0 1/8" = 1'-0"



ARCHITECTS STAMP

REVISIONS

No.	Description	Date	By

Project No.: 19027
Drawn By: JV
Checked By:

SITE APPROVAL
10.11.19

FLOOR PLANS

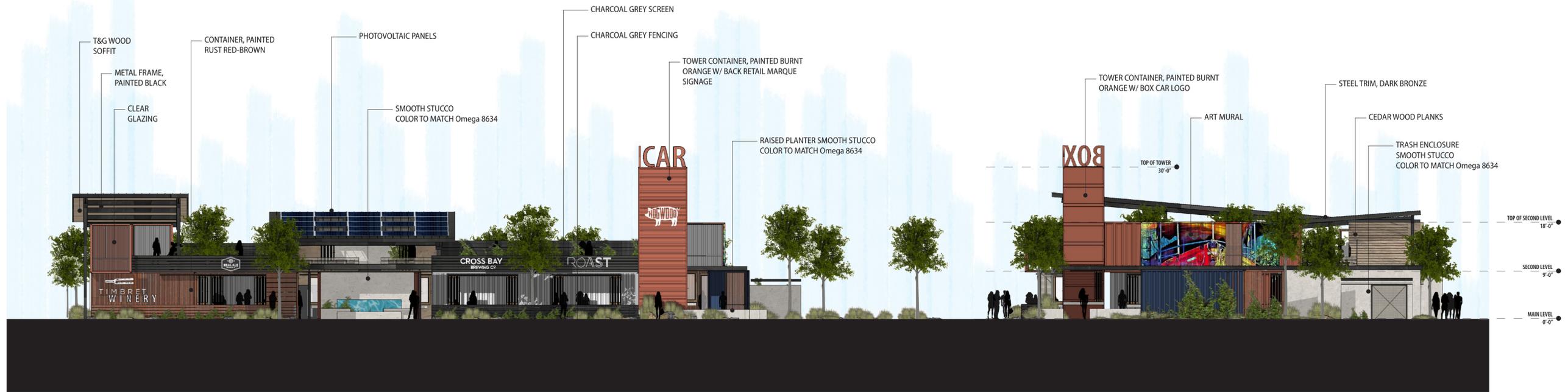
REVISIONS

No.	Description	Date	By

Project No.: 19027
Drawn By:
Checked By: VV

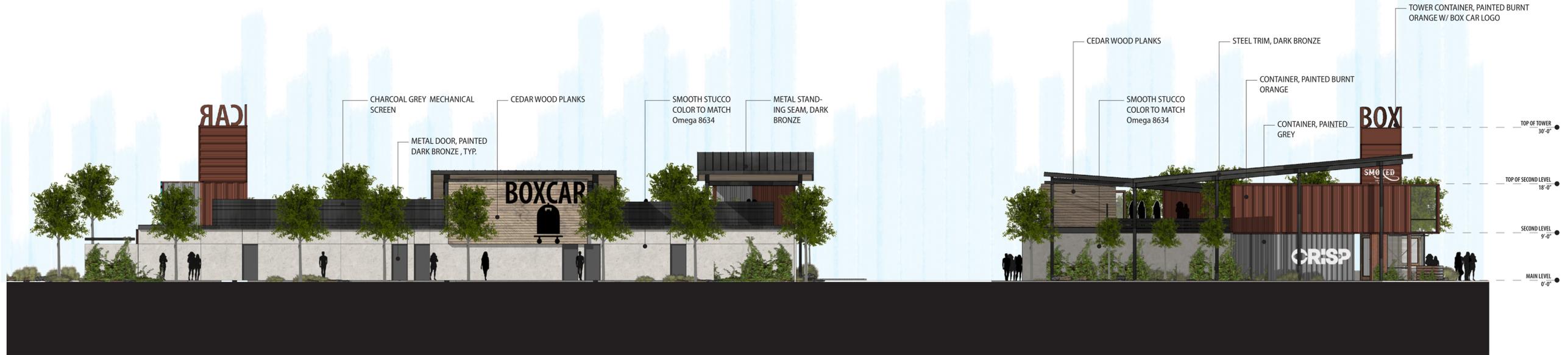
SITE APPROVAL
10.11.19

EXTERIOR ELEVATIONS



WEST ELEVATION (CHURCH STREET)

SOUTH ELEVATION (ALLEYWAY)



EAST ELEVATION

NORTH ELEVATION (LOCKEFORD STREET)

MATERIAL PALLET





NORTH WEST CORNER - LOCKEFORD & CHURCH STREET

BOX CAR

| 10.11.19

| SITE APPROVAL



WEST ELEVATION - CHURCH STREET

BOX CAR

| 10.11.19

| SITE APPROVAL



SITE AERIAL

BOX CAR

10.11.19

SITE APPROVAL

CONDITIONS OF APPROVAL

Project No. 2019-23 (SPARC APPROVAL): for the development of “Box Car,” a new restaurant/food court use at the southeast corner of W Lockeford Street and N Church Street.

Assessor’s Parcel Number: 043-023-01

SPARC Action: May 13, 2020

Conditions of Approval	Timing/ Implementation	Enforcement/ Monitoring	Verification (Date and Signature)
General Conditions			
<p>1. The Applicant shall review and sign below verifying the “Acceptance of the Conditions of Approval” and return the signed page to the City of Lodi Planning Department. Project approval is not final until a signed copy of these conditions is filed with the City.</p> <hr/> <p>Applicant Signature Date</p>	<p>Must Occur to Finalize Approval</p>	<p>Community Development Department, Planning Division</p>	

Conditions of Approval	Timing/ Implementation	Enforcement/ Monitoring	Verification (Date and Signature)
<p>2. The applicant, to the fullest extent permitted by law, shall indemnify, defend, and hold harmless the City, and its elected and appointed officials, directors, officers, employees, agents, departments, agencies, volunteers and instrumentalities thereof, from any and all claims, damages, losses, expenses (including reasonable attorney's fees and costs), demands, law suits, writs of mandamus, and other actions and proceedings (whether legal, equitable, declaratory, administrative or adjudicatory in nature), and alternative dispute resolutions procedures (including but not limited to arbitrations, mediations, and other such procedures) (collectively "Actions"), brought against the City, and/or any of its officials, directors, officers, employees, agents, departments, agencies, v o l u n t e e r s , and instrumentalities thereof, that challenge, attack, or seek to modify, set aside, void, or annul any action of, or any permit or approval issued by, the City and/or any of its officials, directors officers, employees, agents, volunteers, departments, agencies, and instrumentalities thereof (including actions approved by the voters of the City), for or concerning the Project, whether such Actions are brought under CEQA, the Planning and Zoning Law, the Subdivisions Map Act, Code of Civil Procedure Section 1085 or 1094.5, or any other state, federal, or local statute, law, ordinance, rule, regulation, or any decision of a court of competent jurisdiction. It is expressly agreed that the City shall have the right to approve, which approval will not be unreasonably withheld, the legal counsel providing the City's defense and that applicant shall reimburse City for any costs and expenses directly and necessarily incurred by the City in the course of the defense. City may, at its election, conduct the defense, or obtain independent legal counsel in defense of any claim related to this indemnification, applicant shall pay all of the costs related thereto, including without limitation reasonable attorney fees and costs. City shall promptly notify the applicant of any Action brought and City shall cooperate with applicant in the defense of the Action. The defense and indemnification obligations required herein are undertaken in addition to, and shall not in any way be limited by any</p>	Ongoing	Community Development Department, Planning Division	

	additional insurance obligations.			
3.	The project shall be developed in accordance with the project as approved by the Site Plan and Architectural Review Committee on May 13, 2020, including the approved site plan, architectural elevations, colors and materials, etc., attached to these conditions of approval and hereby incorporated in their entirety. The applicant may request modifications or revisions to the approved project as outlined in the Zoning Code.	Ongoing	Community Development Department, Planning Division	

Prior to Issuance of Building Permit				
	Conditions of Approval	Timing/ Implementation	Enforcement/ Monitoring	Verification (Date and Signature)
Planning Conditions				
4.	This approval does not include signs. The applicant shall submit all signs for review and approval by the Planning Department.	Prior to Issuance of Building Permits	Community Development Department, Planning Division	
5.	<p>A Final Landscape Plan shall be submitted to the Planning Department for review and approval. The Final Landscape Plan shall provide for the following:</p> <ul style="list-style-type: none"> On-site landscaping shall not obstruct the traffic signal at the intersection of Church Street and Lockeford Street. The street trees along Church Street shall be preserved. Any modification to the existing street trees shall require the approval of the Public Works Director. 	Prior to Issuance of Building Permits	Community Development Department, Planning Division; Public Works Department	
Building and Safety Conditions				
6.	The construction of the new building and related site improvements shall require a building permit. All plan submittals shall be based on the City of Lodi Building Regulations and currently adopted 2016 California Building code. Please review our policy handouts for specific submittal	Prior to Issuance of Building	Building and Safety Department	

	procedures.	Permits		
7.	Fire rated separation may be required between different occupancies in the building as per 2016 CBC, Section 508.4 and Table 508.4	Prior to Issuance of Building Permits	Building and Safety Department	
8.	All buildings and structures shall have the required fire separation distance separation to the property lines as specified by the 2016 CBC, Table 602 or shall provide the required fire rated protection of walls, limitations of protected and unprotected openings for each building, as per 2016 CBC, Sections 705, Tables 601, 602 ,705.8 and 716.5	Prior to Issuance of Building Permits	Building and Safety Department	
9.	Plans shall provide occupancy load calculations for each area of the building based on square footage and the applicable occupant load factor from Table 1004.1.2. 2016 CBC, Section 1004.1.2	Prior to Issuance of Building Permits	Building and Safety Department	
10.	<p>If the occupant load for any area of the building exceeds 49, the plans shall show:</p> <p>a) A minimum of two (2) exits that are separated by a minimum of 1/2 (1/3 in sprinklered buildings) of the diagonal distance of the area served. 2016 CBC, Section 1007.1.1</p> <p>b) Exit doors shall swing in the direction of egress travel. 2016 CBC, Section 1010.1.2.1</p> <p>The exit doors and exit access doors shall be equipped with panic hardware. 2016 CBC, Section 1010.1.10</p> <p>d) A means of illuminating the egress path of travel in case of power failure, including path to the egress doors, the corridor and the exterior landings. The emergency power system shall provide back up power for the duration of at least 90 minutes and shall illuminate the path of travel at the rate of an average of 1 foot candle at floor level. 2016 CBC, Sections 1008.1 thru 1008.3.5</p> <p>e) Show locations of required illuminated exit signs. 2016 CBC, Section</p>	Prior to Issuance of Building Permits	Building and Safety Department	

	<p>1013</p> <p>f) Provide complete and adequate details and locations of the required tactile exit signs at the following locations:</p> <ol style="list-style-type: none"> 1. Each grade-level exterior exit door shall be identified by a tactile exit sign with the word, "EXIT." 2. Each exit access door from an interior room or area that is required to have a visual exit sign, shall be identified by a tactile exit sign with the words, "EXIT ROUTE." 2016 CBC, Section 1013.4 			
11.	<p>Site Plan to show all building entrances and ground level exits shall be connected on an accessible route to other buildings on the site, public transportation stops, accessible parking and passenger loading zones and to public streets and sidewalks. 2016 CBC, Sections 11B-206.4.1, 11B-404, 11B-Division 4.</p>	<p>Prior to Issuance of Building Permits</p>	<p>Building and Safety Department</p>	
12.	<p>Plans to specify walkways and sidewalks along accessible routes of travel (1) are continuously accessible, (2) have maximum 1/2" changes in elevation, (3) are minimum 48" in width, (4) have a maximum 2% cross slope, and (5) where necessary to change elevation at a slope exceeding 5% (i.e., 1:20) shall have ramps complying with 2016 CBC, Section 11B-405 or 11B-406 as appropriate. Where a walk crosses or adjoins a vehicular way, and the walking surfaces are not separated by curbs, railings or other elements between the pedestrian areas and vehicular areas shall be defined by a continuous detectable warning which is 36" wide, complying with 2016 CBC, Sections 11B-247.1 & 11B-705.1.2.5.</p>	<p>Prior to Issuance of Building Permits</p>	<p>Building and Safety Department</p>	
13.	<p>Preliminary plans appear to show 2nd floor dining areas that are accessed by a single staircase. The 2016 CBC, Section 11B-206.2.5 requires an accessible route to be provided to all functional areas of restaurants, bars, cafeterias and banquet facilities, including raised or sunken areas and outdoor area. Plans shall show an accessible route (elevator or ramp) to the 2nd floor dining area.</p>	<p>Prior to Issuance of Building Permits</p>	<p>Building and Safety Department</p>	

<p>14.</p>	<p>Plans to specify and show accessible parking spaces in compliance with 2016 CBC, Sections 11B-208.2, 11B-502 and Table 11B-208.2. At least one space shall be van accessible as per 2016 CBC, Section 11B-208.2.4.</p> <p>Provide complete and adequate accessible parking details to specify and show:</p> <p>1) A 9' wide x 18' deep accessible parking space(s) with 5' wide striped access aisle. "Van accessible" parking spaces shall be a minimum 12' wide x 18' deep with a 5' wide access aisle or 9' wide with an 8' wide access aisle. 2016 CBC, Section 11B-502.2</p> <p>The access aisle(s) may be located on either side of the accessible parking space(s), except "van accessible" parking spaces shall have the access aisle located on the passenger side of the accessible parking space. Two accessible parking spaces shall be permitted to share a common access aisle. 2016 CBC, Sections 11B-502.3.4, 11B-502.3</p> <p>The access aisles shall be marked with a blue painted borderline around their perimeter. The area within the blue borderlines shall be marked with hatched lines a maximum of 36" on center in a color contrasting with the aisle surface, preferable blue or white. The words "NO PARKING" shall be painted in 12" high white letters in each access aisle. 2016 CBC, Section 502.3.3</p> <p>The accessible parking spaces and the access aisles shall not exceed 2% cross slope in any direction. 2016 CBC, Section 11B-502.4</p>	<p>Prior to Issuance of Building Permits</p>	<p>Building and Safety Department</p>	
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15.	<p>Plans shall specify location and provide complete and adequate details of the following required parking signage:</p> <p>"Unauthorized vehicles..." signs at all driveway entrances, or located adjacent to and visible from each accessible parking space. Indicate on the plans that the sign shall be filled out with the correct appropriate information to become a permanent part of the sign (Lodi Police Department, 209-333-6727). 2016 CBC, Section 11B-502.8</p> <p>Specify permanently posted reflectorized parking space identification sign at each accessible parking space, 70-sq. inches in area, and mounted a minimum of 60" (80" when located in the path of travel) from bottom of sign to paving. Provide a "van accessible" sign at appropriate parking spaces. The sign may also be posted on a wall at the interior end of the parking space at a height of 36" above the finished walk or grade. An additional sign below the symbol of accessibility shall state "Minimum Fine \$250". 2016 CBC, Section 11B-502.6</p> <p>Specify pavement-marking symbols at each accessible parking space. 2016 CBC, Section 11B-502.6.4</p>	Prior to Issuance of Building Permits	Building and Safety Department	
16.	<p>An automatic fire sprinkler system shall be required for the building due to Lodi Municipal Code 15.20.160 requirements for buildings exceeding 6,000sqft. Fire Sprinklers shall be submitted under a separate permit and cover.</p>	Prior to Issuance of Building Permits	Building and Safety Department	
17.	<p>The building is required to be protected by an automatic fire extinguishing system and therefore an approved fire control room shall be provided. [LMC 15.20.180 & Lodi Fire Standard D-01] Plans shall include a fire control room with the following:</p> <p>a) Fire control rooms shall contain all fire sprinkler system control valves, fire alarm control panels, extra fire sprinkler heads, and other fire equipment required by the Chief.</p> <p>b) Fire control rooms shall be located within the building at a location approved by the Chief, and shall be provided with a means to access the</p>	Prior to Issuance of Building Permits	Building and Safety Department	

	<p>room directly from the exterior with an approved door of minimum dimensions of 36" x 80".</p> <p>c) Durable signage shall be provided on the exterior side of the access door to identify the fire control room. The sign shall indicate "FIRE CONTROL ROOM" with 3" tall letters that contrast with their background.</p> <p>d) Keys shall be located within an approved Knox® Company key box located adjacent to the main entrance door on the exterior of the building at 6' above finished floor. Keys shall include keys for manual pull stations, fire alarm control panel, breakaway locks for PIV, locks for OS &Y chains and exterior doors and essential rooms as determined by the Chief.</p> <p>e) Fire control rooms for commercial buildings shall be a minimum dimension of 5' x 7' not less than 35 square feet.</p> <p>f) The fire sprinkler riser shall be located at least 12" from any wall. Fire riser SHALL NOT block doorway.</p> <p>g) The fire equipment room may contain other building service equipment. This other equipment shall not be within 3' in front of any fire equipment in the room. Rooms may be enlarged to share with other equipment such as electrical. However rooms may not be used as storage areas.</p>			
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18.	Fire sprinkler monitoring alarm system is required by 2016 CFC, Section 903.4. The Fire Alarm System shall be submitted under a separate permit and cover to the Building Department by a C-10 licensed fire alarm contractor.	Prior to Issuance of Building Permits	Building and Safety Department	
19.	All automatic sprinkler systems shall require a fire department connection (FDC) in a location approved by the Fire Chief. On site FDC's shall be within 50 feet on a fire hydrant unless placed along a public street within 20 feet of the property line. FDC's shall be placed on the same side of fire access roads as the fire hydrant. Plans to show the location of the Fire Department Connection within the required distance to a hydrant.	Prior to Issuance of Building Permits	Building and Safety Department	
20.	Plans to specify and show all sales, service and information counters are a maximum of 34" high or specify and show a minimum 36" wide counter area that is not more than 34" aff. 2016 CBC, Sections 11B-227, 11B-904.4	Prior to Issuance of Building Permits	Building and Safety Department	
21.	Separate toilet facilities shall be provided for each sex. Plumbing occupant load shall be calculated using the plumbing occupant load factor specified by 2016 CPC Table A for each area use. The required number of plumbing fixtures (water closets, urinals, lavatories) shall be provided, as specified for A-2 occupancies by 2016 CPC, Table 422.1.	Prior to Issuance of Building Permits	Building and Safety Department	
Public Works Conditions				
22.	<p>Project design and construction shall be in compliance with applicable terms and conditions of the City's Stormwater Management Plan (SMP), and shall employ the Best Management Practices (BMPs) identified in the SMP.</p> <p>a. Stormwater Development Standards will be required for this project.</p> <p>b. State-mandated, year round construction site inspections to assure compliance with the City of Lodi Storm Discharge Permit are required. The fee for the inspections is the responsibility of the developer.</p>	Prior to Issuance of Building Permits	Public Works Department	

23.	Submit a Project Stormwater Plan for a regulated project in compliance with the Multi-Agency Post-Construction Stormwater Standards Manual (PCSP) as adopted by City Council on November 4, 2015.	Prior to Issuance of Building Permits	Public Works Department	
Prior to Issuance of Final Certificate of Occupancy				
24.	All required landscape planting and irrigation shall have been installed consistent with the approved Final Landscape Plan and shall be in a condition acceptable to the Planning Director. The plants shall be healthy and free of weeds, disease, or pests. The irrigation system shall be properly constructed and in good working order.	Prior to Certificate of Occupancy	Community Development Department, Planning Division	
25.	The onsite parking spaces shall be striped in accordance with Chapter 17.32 (Parking and Loading) of the Development Code and City of Lodi Standard Plan 134 to provide seven parking stalls, including ADA compliant stalls as required by applicable codes. The first parking stall shall be located a minimum of 10-feet from the street right-of-way.	Prior to Issuance of Certificate of Occupancy	Community Development Department, Planning Division; Public Works Department	
26.	The proposed parking lot shall not allow cars to back up onto the alley. A separate driveway to the parking lot shall be installed on Church Street.	Prior to Issuance of Certificate of Occupancy	Community Development Department, Planning Division; Public Works Department	
27.	Remove and replace all damaged or non ADA-conforming sidewalk and used driveways fronting the parcel in conformance with City of Lodi Standard Plans, pending project valuation.	Prior to Issuance of Certificate of Occupancy	Community Development Department, Planning Division Public Works	

			Department	
28.	Existing, unused driveways shall be removed and replaced with curb, gutter, and sidewalk in accordance with City of Lodi Standard Plans.	Prior to Issuance of Certificate of Occupancy	Community Development Department, Planning Division; Public Works	
29.	Dedicate any necessary public utility easements for the construction of public improvements.	Prior to Issuance of Certificate of Occupancy	Community Development Department, Planning Division; Public Works Department	

General Compliance Items/Requirements and Information

The following items are noted for the applicant's information. These items are generally required for all projects by City of Lodi ordinances, other local agencies, and state or federal agencies.

PLEASE NOTE: This list is not comprehensive. The project is subject to all applicable standards, fees, policies, rules, and regulations of the City of Lodi and many other agencies, including but not limited to the Central Valley Regional Water Quality Control Board, San Joaquin Valley Air Pollution Control District, and state and federal agencies.

1. The applicant will need to obtain the following permits:
 - a. Building Permit issued from the City of Lodi Building Department.
 - b. Encroachment Permit issued from the City of Lodi Public Works Department for any work within the City's public right of way or on existing public water, wastewater and storm drain infrastructure.
 - c. San Joaquin County well/septic abandonment permit (if needed).
 - d. San Joaquin Valley Air Pollution Control District (SJVAPCD) permits.
2. Payment of the following fees prior to building permit issuance will be required unless noted otherwise:

- a. Installation of water and wastewater services by City Forces if property does not have existing services or current services shall be upsized.
 - b. Abandonment of existing water and wastewater services by City Forces, if applicable.
 - c. Water meter installation fees.
 - d. Filing and processing fees and charges for services performed by City forces per the Public Works Fee and Service Charge Schedule.
 - e. Stormwater Compliance Inspection Fee prior to building permit issuance or commencement of construction operations, whichever occurs first.
 - f. Regional Transportation Impact Fee (RTIF) at the time of building permit issuance.
 - g. Encroachment permit fee.
 - h. Habitat Conservation Fee.
3. Payment of the following fee prior to temporary occupancy or occupancy of the building unless noted otherwise:
- a. Development Impact Mitigation Fees per the Public Works Fee and Service Charge Schedule.
 - b. County Facilities Fees.
- (The fees referenced above are subject to periodic adjustment as provided by the implementing ordinance/resolution. The fee charged will be that in effect at the time of collection indicated above.)
4. As required by the California Green Building Code (CALGreen), project shall participate in the Construction and Demolition Recycling Program.
 5. Prior to any work within City Right-of- Way and Public Utility Easements, the applicant shall obtain an encroachment permit issued by the Public Works Department.
 6. If fire service is needed it shall be installed according to the City of Lodi Design Standards § 4.403.
 7. Irrigation plans and plantings shall conform to the Model Water Efficient Landscaping Ordinance (MWELo) per the Governor's Executive Order B-29-12 adopted on December 31, 2015.
 8. All project design and construction shall be in compliance with the Americans with Disabilities Act (ADA). Project compliance with ADA standards is the developer's responsibility.
 9. The trash enclosure shall conform to the CASQA Development BMP Handbook Section SD-32 and shall be wide enough to provide separate containers for recyclable materials and other solid waste.
 10. One water and wastewater service is permitted per parcel. Existing, unused water and wastewater services shall be abandoned at the

public main by City Forces at the developer's expense, if applicable.

11. All on-site water wells and septic systems shall be abandoned in conformance with San Joaquin County standards prior to building permit issuance. A copy of the abandonment permit shall be submitted to the City after the completion of the abandonment.
12. All existing survey monuments are to be preserved per California Senate Bill 1467. It is the applicant's responsibility to ensure that monuments are properly protected and/or perpetuated. If any of the monuments are to be disturbed or are near the area of construction, a licensed surveyor must confirm that the monuments have been protected and/or perpetuated and the appropriate documentation has been recorded.

DRAFT