

<p>CARNEGIE FORUM 305 WEST PINE STREET LODI, CALIFORNIA</p>	<p><b>AGENDA</b> <b>LODI</b> <b>PLANNING COMMISSION</b></p>	<p>REGULAR SESSION WEDNESDAY, AUGUST 13, 2008 @ 7:00 PM</p>
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For information regarding this agenda please contact:

**Kari Chadwick @ (209) 333-6711**  
**Community Development Secretary**

***NOTE:** All staff reports or other written documentation relating to each item of business referred to on the agenda are on file in the Office of the Community Development Department, located at 221 W. Pine Street, Lodi, and are available for public inspection. If requested, the agenda shall be made available in appropriate alternative formats to persons with a disability, as required by Section 202 of the Americans with Disabilities Act of 1990 (42 U.S.C. Sec. 12132), and the federal rules and regulations adopted in implementation thereof. To make a request for disability-related modification or accommodation contact the Community Development Department as soon as possible and at least 24 hours prior to the meeting date.*

1. ROLL CALL
2. MINUTES – “June 25, 2008” & “July 9, 2008”
3. PUBLIC HEARINGS
  - a. Continued from July 7, 2008 - Request for Planning Commission approval of a Use Permit to allow a Type-41 on-sale beer and wine license at Midtown Taqueria located at 1040 West Kettleman Lane Suite 4

**NOTE: The above item is a quasi-judicial hearing and requires disclosure of ex parte communications as set forth in Resolution No. 2006-31**

- b. Request for Planning Commission approval of a Use Permit to allow a Type 2 (Winery) Alcoholic Beverage Control License and Live Entertainment at 112 W. Pine Street. (Applicant: Benson Ferry Vineyard, DBA; File Number: 08-U-08)

**NOTE: The above item is a quasi-judicial hearing and requires disclosure of ex parte communications as set forth in Resolution No. 2006-31**

- c. Review and recommend to the City Council Transit Oriented Development Design Guidelines. (Applicant: City of Lodi Community Development Department; File Number: N/A; CEQA Status: Exempt)

4. PLANNING MATTERS/FOLLOW-UP ITEMS
  - a. Confirm appointments to the Site Plan and Architectural Review Committee and the Greenbelt Task Force.
5. ANNOUNCEMENTS AND CORRESPONDENCE
6. ACTIONS OF THE CITY COUNCIL
  - a. Summary Memo Attached.
7. GENERAL PLAN UPDATE/DEVELOPMENT CODE UPDATE
8. ACTIONS OF THE SITE PLAN AND ARCHITECTURAL REVIEW COMMITTEE
9. ART IN PUBLIC PLACES
10. COMMENTS BY THE PUBLIC
11. COMMENTS BY THE PLANNING COMMISSIONERS & STAFF

## 12. ADJOURNMENT

Pursuant to Section 54954.2(a) of the Government Code of the State of California, this agenda was posted at least 72 hours in advance of the scheduled meeting at a public place freely accessible to the public 24 hours a day.

*\*\*NOTICE: Pursuant to Government Code §54954.3(a), public comments may be directed to the legislative body concerning any item contained on the agenda for this meeting before (in the case of a Closed Session item) or during consideration of the item.*

### Right of Appeal:

If you disagree with the decision of the commission, you have a right of appeal. Only persons who participated in the review process by submitting written or oral testimony, or by attending the public hearing, may appeal.

Pursuant to Lodi Municipal Code Section 17.72.110, actions of the Planning Commission may be appealed to the City Council by filing, within ten (10) business days, a written appeal with the City Clerk and payment of \$300.00 appeal fee. The appeal shall be processed in accordance with Chapter 17.88, Appeals, of the Lodi Municipal Code. Contact: City Clerk, City Hall 2<sup>nd</sup> Floor, 221 West Pine Street, Lodi, California 95240 – Phone: (209) 333-6702.

**LODI PLANNING COMMISSION  
REGULAR COMMISSION MEETING  
CARNEGIE FORUM, 305 WEST PINE STREET  
WEDNESDAY, JUNE 25, 2008**

1. CALL TO ORDER / ROLL CALL

The Regular Planning Commission meeting of June 25, 2008, was called to order by Chair Mattheis at 7:00 p.m.

Present: Planning Commissioners – Cummins, Hennecke, Kiser, Olson, White, and Chair Mattheis

Absent: Planning Commissioners – Kirsten

Also Present: Planning Manager Peter Pirnejad, Deputy City Attorney Janice Magdich, Junior Planner Immanuel Bereket, and Administrative Secretary Kari Chadwick

2. MINUTES

“June 11, 2008”

MOTION / VOTE:

The Planning Commission, on motion of Vice Chair Kiser, Olson second, approved the Minutes of June 11, 2008 as written.

3. PUBLIC HEARINGS

None

4. PLANNING MATTERS/FOLLOW-UP ITEMS

Planning Manager Pirnejad thanked Commissioner White for his service on the Commission.

5. ANNOUNCEMENTS AND CORRESPONDENCE

None

6. ACTIONS OF THE CITY COUNCIL

a. Chair Mattheis stated that staff is available to answer any questions regarding the summary memo that is a part of the packet.

7. GENERAL PLAN UPDATE/DEVELOPMENT CODE UPDATE

a. Recommend adoption of new regulations for the creation of Flag Lots within the City of Lodi.

- Planning Manager Pirnejad stated that Staff has received some requests to continue this item, so that further discussion can be had with area contractors and representatives. He suggested that we continue with the presentation and discussion, Chair Mattheis agreed.
- Planning Manager Pirnejad gave a brief presentation based on the staff report. He stated that these guidelines were created to give staff (Planning, Fire, and Public Works) minimum guidelines to follow. Each department has their own minimum standards. The current zoning ordinance does not have language for these standards to address this type of application. Staff has also received guidance from the Planning Commission on what they would like to see when the applications are brought before them for approval.
- Planning Manager Pirnejad responded to the emails received (attached to the back of the minutes) requesting a continuance of this item as summarized below:
  - John Beckman had several concerns that can be read in detail in his email letter (attached)
    - Planning Manager Pirnejad responded to Mr. Beckman’s concerns by stating:

- Within the body of the letter Mr. Beckman states that developers that build on flag lots should be consulted during the creation of the policy to which Planning Manager Pirnejad stated that that was the intention of staff all along. Staff first brought this item before the Planning Commission back on April 23, 2008 to promote open discussion regarding this issue.
  1. There is more to approving flag lots than just promoting infill - Fire Concerns, Access issues, Lot size issues, big enough = external street.
  2. This concern is encouraged NOT required.
  3. The minimum lot size is the amount of area that is buildable, the flag portion of the lot is unbuildable and is not considered in the total lot size.
  4. The front yard setback for the back lot is reduced from the current requirement of 20 feet because the pole offers them the necessary access to the lot.
  5. No additional parking required – additional parking is provided in the driveway.
  6. This ordinance isn't meant to limit infill, it is intended to address the street presence for fire, public safety, and neighborhood character. Infill creation shouldn't be the only criteria.
  7. This item was to address Planning Commission's concerns regarding privacy of the existing dwelling surrounding the proposed project.
  8. This item is consistent with the direction that the State of California is heading.
- Joe Murphy
  - Mr. Murphy is requesting a continuance, which staff is recommending for this item.
- Pat Patrick
  - Mr. Patrick is requesting a continuance, which staff is recommending for this item.
- Planning Manager Pirnejad stated that he sent an email response to all the parties concerned letting them know that staff would be recommending the continuance of this item to promote further discussion.
- Vice Chair Kiser stated that if the lot will accommodate more than two dwellings he thinks they should still be able to put more than two. He also asked about the two-story dwellings being set back an extra 5'. Planning Manger Pirnejad stated that only the second story will need to be set back the extra 5' to avoid a tall wall.
- Vice Chair Kiser asked about the alley access issue. Planning Manager Pirnejad stated that staff wanted to leave alley access available for discussion on a case by case basis.

Discussion opened up to the Public

- Jeffery Kirst came forward to offer his opinions. Mr. Kirst handed out a rendition of a flag lot showing three separate parcels that would not be able to be done if these guidelines are approved.

Discussion occurred regarding the different ideas for Flag Lots.

Discussion closed to the Public

- Planning Manager Pirnejad stated that these guidelines were not intended to give staff a reason to deny Mr. Kirst's application. His application supersedes these guidelines. In working with Mr. Kirst to get this project to work several considerations have had to be made and issues address that have not come up in the past and this just strengthens the need for a set of guidelines for staff to follow.
- Commissioner Olson stated that she understands the intent of creating guidelines, but doesn't want to see an example like Mr. Kirst's denied because of these guidelines. She would also like to know why there is a requirement for two covered parking spaces. Planning Manager

*Continued*

Pirnejad stated that the requirement for two covered parking spaces is a part of our current Municipal Code.

- Vice Chair Kiser asked about the visibility of the dwelling on the middle lot of the example that Mr. Kirst provided. Planning Manager Pirnejad stated that the purpose is to provide visual access for Police and Fire to all the lots.

Discussion was re-opened to the Public

- Jeffery Kirsts stated that there have been other situations where Administrative Deviations have been granted for lot size. The parking issue would be similar to apartments, when a car is parked in the right-of-way the police are called and the car towed.
- Chair Mattheis asked how the lot size and the relationship of the pole size for Mr. Kirst’s example fit with the neighborhood. Mr. Kirst stated that the issue he had with the pole size was where to start the measurement for the set back for the dwelling units.

Discussion we re-closed to the Public

- Chair Mattheis stated that he would like to look more at the set back issues as related to the pole part of the flag. Planning Manager Pirnejad stated that the staff should state the intent of the setback and let the developer show that they are trying to meet that intent. Chair Mattheis asked if this could be amended in the future when the new zoning code is adopted. Planning Manager Pirnejad stated that it could be altered and will need to be written into the new zoning code.
- Commissioner White asked if there could be separate guidelines set up for a one to two parcel split, one to three and so on. Chair Mattheis agreed with this suggestion. Commissioner White also suggested that staff keep the two covered parking spaces.

MOTION / VOTE:

The Planning Commission, on motion of Vice Chair Kiser, Olson second, continued the request of item 7a to a date to be determined. The motion carried by the following vote:

- Ayes: Commissioners – Cummins, Hennecke, Kiser, Olson, White, and Chair Mattheis
- Noes: Commissioners – None
- Absent: Commissioners – Kirsten

b. Recommend adoption of amended regulations for Site Plan and Architectural Review within the City of Lodi.

- Planning Manager Pirnejad gave a brief presentation based on the staff report. Staff is recommending option two.
- Chair Mattheis asked about the percentage of the applications that would need to be divided. Planning Manager Pirnejad stated that the majority of the applications fall under the discretionary category currently. Staff feels that there will be more of the non-discretionary type applications coming forward in the future.

MOTION / VOTE:

The Planning Commission, on motion of Vice Chair Kiser, Olson second, approved the recommendation of Staff to amended regulations for Site Plan and Architectural Review within the City of Lodi as stated in Resolution PC 08-16. The motion carried by the following vote:

- Ayes: Commissioners – Cummins, Hennecke, Kiser, Olson, White, and Chair Mattheis
- Noes: Commissioners – None
- Absent: Commissioners – Kirsten

8. ACTIONS OF THE SITE PLAN AND ARCHITECTURAL REVIEW COMMITTEE

*Continued*

a. Report on Special Meeting of June 23, 2008.

- Vice Chair Kiser gave a brief report regarding the approval of the new Speed-e Car Wash.

9. ART IN PUBLIC PLACES

None

10. COMMENTS BY THE PUBLIC

None

11. COMMENTS BY STAFF AND COMMISSIONERS

Chair Mattheis thanked Commissioner White for his service.

12. ADJOURNMENT

There being no further business to come before the Planning Commission, the meeting was adjourned at 8:30 p.m.

ATTEST:

Peter Pirnejad  
Co-Interim Community Development Director

**Kari Chadwick**

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**From:** John Beckman [johnb@biadelta.org]  
**Sent:** Wednesday, June 25, 2008 1:35 PM  
**To:** Peter Pirnejad  
**Cc:** 'Jaclyn U'; Kari Chadwick  
**Subject:** Planning Commission meeting tonight

Peter,

I understand you are not feeling well – hope you get better for tonight’s meeting. I’d like to talk with you about item 7.A.

Directly to the point, amendments to design standards need to be discussed with those who are most likely to be complying with those standards. If a flag lot policy is to be adopted by the Planning Commission then developers who build on flag lots should be consulted during the creation of this policy. The proposed policy creates the following issues:

- 1) The restriction of only two allowable lots is overly burdensome and is in direct conflict with the goals of promoting infill within the City.
- 2) If there are only to be two lots on a flag lot then the lot fronting on the street should be allowed to have a direct driveway to the street and not be required to use the pole portion of the flag as the driveway.
- 3) The width of the pole portion of the flag should not be excluded from the minimum lot dimensions in all cases. There should be guidelines that state when it will be excluded, but it should be allowed in some cases. Again this is overly burdensome and conflicts with the goals of the City to promote infill.
- 4) The pre-determination of what side is to be considered the front, rear and side yards in all cases is again overly burdensome and conflicts with the goals of promoting infill.
- 5) The parking requirements are overly burdensome and directly conflict with the principles of smart-growth and the promotion of walkable neighborhoods where residents will not need two vehicles per home.
- 6) The street presence requirement appears arbitrary and capricious. There may be several legitimate reasons for not maintaining a “presence to the street” or an “orientation to the street” for flag lots. The more restrictions placed on infill development the less likely it is to occur. Each restriction must be justified in relation to the likelihood it will limit the amount of infill created.
- 7) The limitations on second floor windows and decks should be proportional to the characteristics of the neighborhood and not a blanket prohibition based on orientation.
- 8) Requiring the maximization of onsite water absorption and retention is again overly burdensome and will hinder the promotion of infill development.

I would like to see this matter carried over to the next planning commission meeting so these matters can be discussed in greater detail in a forum more conducive to a constructive dialogue and not at a public hearing. Please call me to discuss.

Thank you,

John Beckman  
Chief Executive Officer, BIA Delta  
509 W. Weber Avenue, Suite 410  
Stockton, CA 95203  
(209) 235-7831 Office  
(209) 327-5363 Cell

*Knowledge is Power, but Ignorance is Truly Bliss*

# Item 7a.

**Peter Pirnejad**

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**From:** Joe Murphy [jmurphy@dillonandmurphy.com]  
**Sent:** Wednesday, June 25, 2008 3:33 PM  
**To:** Peter Pirnejad  
**Cc:** J. Jeffrey Kirst  
**Subject:** Planning Commission

Dear Mr. Pirnejad,

I understand there is an item scheduled for Planning Commission tonight having to do with changing the ability to create flag lots. Over the past 23 plus years, I have processed numerous flag lots in the City of Lodi and surrounding communities. I have always found them to be a positive addition to the neighborhood and a great opportunity for infill. In the past, they have given another option to entry level and affordable housing home purchasers.

I only found out about the agenda item today and I have a conflict that does not allow me to attend tonight's hearing. It would be beneficial to have a dialogue with Engineers, Land Surveyors and developers who deal with flag lots before the City changes the criteria that allow them. I respectfully request a continuance of this item to the next available hearing date.

Thank you for your attention on this matter.

Joe Murphy  
Dillon & Murphy  
209-334-6613  
209-334-0723 (Fax)

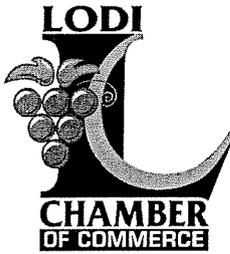
**Kari Chadwick**

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**From:** Pat Patrick [PPatrick@lodichamber.com]  
**Sent:** Wednesday, June 25, 2008 4:36 PM  
**To:** Peter Pirnejad; Kari Chadwick  
**Cc:** jjkirst@gmail.com; John Beckman - Ext; Russ Munson; Blair King  
**Subject:** URGENT- Flag Lot Adoption Postponed ltr to City.doc  
**Importance:** High  
**Attachments:** Flag Lot Adoption Postponed ltr to City.doc

Peter / Kari:

URGENT!!! for tonight's meeting.  
Pat Patrick



June 25, 2008

TO: Peter Pirnejad

FR: Pat Patrick

RE: Planning Commission Review of Flag Lot Recommendations

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Peter:

Some of our Chamber members have raised some concern about staff's recommendations for Flag Lots coming before the Planning Commission tonight.

It seems that they have not had too much input into this recommendation and are asking for some time before the Planning Commission makes any kind of a new ruling.

The Chamber is respectfully requesting no decision be made tonight but allow the parties who are concerned some time to offer their input into the process with the commission.

I just learned of this today and can not be present tonight to make this request personally. I would however if my schedule allowed because this seems to have considerable interest and concern from the building community.

The Chamber is asking staff to recommend postpone adoption of new guidelines for Flag Lots so more practical input can be given by the business community on these new development standards.

Sincerely,  
Pat Patrick  
President / CEO

**LODI PLANNING COMMISSION  
REGULAR COMMISSION MEETING  
CARNEGIE FORUM, 305 WEST PINE STREET  
WEDNESDAY, JULY 9, 2008**

1. CALL TO ORDER / ROLL CALL

The Regular Planning Commission meeting of July 9, 2008, was called to order by Chair Mattheis at 7:00 p.m.

Present: Planning Commissioners – Heinitz, Hennecke, Kiser, Olsen, and Chair Mattheis

Absent: Planning Commissioners – Cummins and Kirsten

Also Present: Planning Manager Peter Pirnejad, Senior Planner David Morimoto, Junior Planner Immanuel Bereket, and Building Inspector II Bob Vrabel

Chair Mattheis welcomed Commissioner Heinitz back to the Planning Commission.

2. MINUTES

None

3. PUBLIC HEARINGS

- a) Notice thereof having been published according to law, an affidavit of which publication is on file in the Community Development Department, Chair Mattheis called for the public hearing to consider the request for approval of a Use Permit to allow a Type-41 on-sale beer and wine license at Midtown Taqueria located at 1040 West Kettleman Lane Suite 4. (Applicant: Joe Doumit.; File Number: 08-U-07)

Planning Manager Pirnejad gave a brief PowerPoint presentation based on the staff report. Staff is recommending approval.

Chair Mattheis asked about the title on the Resolution stating that this license is for Taqueria Santa Cruz located at 2533 West Kettleman Lane Suite 403. Planning Manager Pirnejad stated that the resolution for Taqueria Santa Cruz was used as a boiler plate and the incorrect information in the title was missed, but will be changed to Midtown Taqueria located at 1040 West Kettleman Lane Suite 4.

Chair Mattheis asked if the restaurant was currently open. Senior Planner Morimoto stated that the restaurant that was operating in that location has been closed for several months.

Commissioner Heinitz stated that he has some concerns regarding the fact that the applicant is not present and there has been bad history in the area. He would like to the item continued to another date when the applicant can be present.

Commissioner Olson has some questions of the applicant as well regarding the menu showing tequila, but the license request is for a type 41 (beer and wine). Vice Chair Kiser and Commissioner Hennecke agreed.

Hearing Opened to the Public

- None

Public Portion of Hearing Closed

MOTION / VOTE:

The Planning Commission, on motion of Vice Chair Kiser, Heinitz second, continued the request for approval of a Use Permit to allow a Type-41 on-sale beer and wine license at Midtown Taqueria located at 1040 West Kettleman Lane Suite 4 to a date certain of July 23, 2008. The motion carried by the following vote:

Continued

Ayes: Commissioners – Heinitz, Hennecke, Kiser, Olson, and Chair Mattheis  
Noes: Commissioners – None  
Absent: Commissioners – Cummins and Kirsten

- b) Notice thereof having been published according to law, an affidavit of which publication is on file in the Community Development Department, Chair Mattheis called for the public hearing to consider the request for the Planning Commission to amend conditions of approval for the project site plan and architecture for the building located at 335 East Kettleman Lane (Applicant, Kenneth J. Gini, File # 07-SP-02).

Planning Manager Pirnejad gave a brief PowerPoint presentation based on the staff report. He also pointed out that staff did receive a letter of concern (attached to be back of the minutes) from Mr. Marchick. Staff is recommending approval.

Commissioner Heinitz stated that he feels that the applicant has tried to meet all the original conditions in good faith and after a visit to the site feels the project is well done.

Vice Chair Kiser stated that he also visited the site and believes that the roll-up doors are too narrow to drive a car through. He also feels that the project has enhanced the area.

Commissioner Olson asked if the intent was to make the roll-up doors ADA accessible. Chair Mattheis stated it was not.

Hearing Opened to the Public

- Ken Gini, applicant, came forward to answer questions. He stated that his intent was to meet the requirements set by the Planning Commission. Mr. Gini also stated that the doors will remain closed.

Public Portion of Hearing Closed

- Chair Mattheis stated his appreciation that the applicant has been working with staff to get the conditions modified.

MOTION / VOTE:

The Planning Commission, on motion of Commissioner Heinitz, Kiser second, approved the request for the Planning Commission to amend conditions of approval for the project site plan and architecture for the building located at 335 East Kettleman Lane as stated in Resolution P.C. 08-17. The motion carried by the following vote:

Ayes: Commissioners – Heinitz, Hennecke, Kiser, Olson, and Chair Mattheis  
Noes: Commissioners – None  
Absent: Commissioners – Cummins and Kirsten

Chair Mattheis thank Bob Vrabel, Building Inspector, for attending the meeting.

Vice Chair Kiser asked for a copy of the ADA regulations used to determine access requirements for this project. Building Inspector Vrabel handed Vice Chair Kiser a copy.

Planning Manager Pirnejad noted that Mr. Doumit, applicant for item 3a has arrived.

4. PLANNING MATTERS/FOLLOW-UP ITEMS

- a. Update Planning Commission on Vineyard Christian Middle School Use Permit, 2301 West Lodi Avenue.

Planning Manager Pirnejad stated that this item and the next are for information only and are in response to discussions had with Commissioners. Staff thought that bringing items of a controversial nature back to the Commission would help to show what has worked and what hasn't.

*Continued*

Commissioner Heinitz stated that out of the three major objectors of the project two have moved and the grand-daughter of the third is attending the school.

Planning Manager Pirnejad gave a brief PowerPoint presentation based on the staff report.

- b. Update Planning Commission on Wine Country Card Room Use Permit, 1800 South Cherokee Lane.

Planning Manager Pirnejad gave a brief PowerPoint presentation based on the staff report.

Vice Chair Kiser asked who owns the ABC License for the bar. Planning Manager Pirnejad stated that a different person owns the license. Vice Chair Kiser stated that that was not how it was presented.

Item Opened for Public Comment

- Steven Schnider, Lawyer for the applicant came forward to clear up the ABC License question. The ownership of the bar in the current location is owned by the same person that owns the cardroom. There is a different owner for the bar on Sacramento Street.

Public Portion Closed

- c. Appoint two representatives from the Planning Commission to attend Development Code Update workshop style meetings.

Planning Manager Pirnejad stated that this item is to ask for two volunteers from the Commission to sit in on meetings with the development community regarding the Flag Lot Development Code item.

Chair Mattheis asked if these meetings are going to be noticed. Planning Manager Pirnejad stated that they will not be noticed. Chair Mattheis asked if there would be any benefit to having these meetings be noticed and open to the public. Planning Manager Pirnejad stated that after the initial meeting the item will then be brought back as a noticed public hearing for the public to voice their opinions.

Commissioner Heinitz and Vice Chair Kiser volunteered to sit in on the Flag Lot meetings.

5. ANNOUNCEMENTS AND CORRESPONDENCE

Planning Manager Pirnejad welcomed Randy Heinitz to the Commission.

Chair Mattheis reminded the Commission that there should not be any discussion of agenda items prior to the meeting being called to order.

6. ACTIONS OF THE CITY COUNCIL

- a. Chair Mattheis stated that questions can be asked regarding the summary memo in the packet.

Planning Manager Pirnejad pointed out a few items of interest.

7. GENERAL PLAN UPDATE/DEVELOPMENT CODE UPDATE

- a. Discussion of possible Heritage Tree Ordinance.

Chair Mattheis stated his favor for having a Heritage Tree Ordinance.

Commissioner Heinitz stated his favor for having a Vintage (Heritage) Tree Ordinance. There are several trees that come to mind on the east side of town that preserving them would work in well with any Redevelopment Plan.

Commissioner Olson agreed.

Vice Chair Kiser agreed.

Discussion was had as to how to incorporate some sort of tree preservation into the new Development Code.

8. ACTIONS OF THE SITE PLAN AND ARCHITECTURAL REVIEW COMMITTEE

None

*Continued*

9. ART IN PUBLIC PLACES

None

10. COMMENTS BY THE PUBLIC

None

11. COMMENTS BY STAFF AND COMMISSIONERS

None

12. REORGANIZATION

- a. Planning Commission Chair & Vice Chair

MOTION / VOTE:

The Planning Commission, on motion of Commissioner Heinitz, Olson second, approved the appointment of Commissioner Kiser to the position of Chair by a unanimous vote.

MOTION / VOTE:

The Planning Commission, on motion of Commissioner Hennecke, Kiser second, approved the appointment of Commissioner Cummins to the position of Vice Chair by a unanimous vote.

- b. Appoint Planning Commission Representative to SPARC, Art In Public Places, and Greenbelt Task Force.

SPARC

Commissioner Kiser volunteered to stay on SPARC. Chair Mattheis pointed out that tradition is to have the Vice Chair be the representative to this committee and would like to first see if Commissioner Cummins has any objections or conflicts to taking this seat before taking any other nominations. Everyone agreed.

Planning Manager Pirnejad stated that in Commissioner Cummins' email he states that he is already sitting on the Greenbelt Task Force.

Art In Public Places

Chair Mattheis stated that because Commissioner Kirsten was recently appointed to this committee that he should remain the representative.

Greenbelt Task Force

Commissioner Heinitz mentioned that maybe Commissioner Mattheis would like to sit on this one. Chair Mattheis would like to wait until the next meeting for nomination on this item considering Commissioner Cummins is already a member and wouldn't want to bump him off if it was his desire to remain a member.

13. ADJOURNMENT

There being no further business to come before the Planning Commission, the meeting was adjourned at 8:57 p.m.

ATTEST:

Peter Pirnejad  
Co-Interim Community Development Director

# Item 3a.

**CITY OF LODI  
PLANNING COMMISSION  
Staff Report**

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**MEETING DATE:** August 13, 2008  
**APPLICATION NO:** Use Permit: 08-U-07  
**REQUEST:** Continued from July 7, 2008 - Request for Planning Commission approval of a Use Permit to allow a Type-41 on-sale beer and wine license at Midtown Taqueria located at 1040 West Kettleman Lane Suite 4.  
(Applicant: Joe Doumit. File Number: 08-U-07)  
**LOCATION:** 1040 West Kettleman Lane, Suite 4  
APN: 060-040-19  
**APPLICANT:** Joe Doumit  
3754 J Street  
Sacramento, CA 95816  
**PROPERTY OWNER:** The same as above.

**RECOMMENDATION**

Staff recommends that the Planning Commission approve the request of Joe Doumit for a Use Permit to allow a Type-41 on-sale beer and wine license at Midtown Taqueria located at 1040 West Kettleman Lane Suite 4, subject to the conditions in the attached resolution.

**PROJECT/AREA DESCRIPTION**

**General Plan Designation:** NCC, Neighborhood Community Commercial  
**Zoning Designation:** PD, Planned Development (15)  
**Property Size:** 79,715 sq. ft. (Restaurant is approximately 3,600 square feet.)

The adjacent zoning and land use are as follows:

**North:** C-S, Commercial Shopping. Various retail and commercial shops are located north of the project site.  
**South:** LDR, Low Density Residences.  
**East:** C-S, Commercial Shopping. A grocery store and various retail establishments are located east of the project site.  
**West:** C-S, Commercial Shopping. Restaurants, shops and residential lots are located west of the project site.

**SUMMARY**

The applicant, Mr. Joe Doumit, is requesting approval for a Use Permit to allow a Type-41 on-sale beer and wine license at Midtown Taqueria. The project area contains a variety of commercial businesses such as a grocery store, restaurants and various retail stores. Because the census tract for the project site currently does not have an over concentration of liquor licenses and since Midtown Taqueria is a full service restaurant, staff does not anticipate any problems with issuing the

license. Staff is therefore recommending that the Planning Commission approve the request for an on-sale alcohol license (ABC Type 41) to serve beer and wine, subject to the conditions in the attached resolution.

## **BACKGROUND**

Midtown Taqueria is a full service restaurant that wishes to operate in the City of Lodi. The applicant is a restaurant owner and operator in the city of Sacramento. The applicant also owns the subject property. The subject property was previously used by a pizzeria with an ABC Type 41 License. However, the pizzeria has been out of business since March of this year and the owner of the previous business returned the ABC License to the Department of Alcoholic Beverage Control. In accordance with the Department of Alcoholic Beverage Control who require that all new business must obtain its own ABC License, the applicant has applied for ABC License and must obtain a conditional Use Permit for Midtown Taqueria from the City. To increase sales and attract customers, the applicant is requesting approval from the City to serve beer and wine at the restaurant.

## **ANALYSIS**

According to the applicant, Midtown Taqueria will offer a full breakfast, lunch and dinner menu of authentic Mexican favorites such as: tacos, burritos, nachos, tostadas, quesadillas and various combination plates. The restaurant will open from the hours of 9:00 a.m. to 9:00 p.m. Monday through Saturday and 10:00 a.m. to 8:00 p.m. on Sunday. The restaurant is approximately 3,600 square feet in size and provides seating for approximately 35-40 guests based on the floor plan provided by the applicant. On site parking is provided in the plaza which satisfies the parking requirement. In accordance with the State Department of Alcoholic Beverage Control (ABC) requirements, receipts from alcohol sale shall not be in excess of food sales receipts. ABC requires that restaurants with alcohol license must operate and maintain the premise as a bona fide eating establishment. Staff has contacted the Lodi Police Department for comment on the proposed on-sale beer and wine application and they do not anticipate alcohol related problems with the restaurant.

Section 17.72.040 of the Lodi Municipal Code requires a Use Permit for new Off-Sale and On-Sale alcohol licenses as well as changes in license type. The City established the Use Permit requirement to gain local control over whether or not a license is appropriate for a particular location. ABC primarily controls issuance based on concentration of licenses within a particular Census Tract. Census Tract 43.06 covers the area south of Kettleman Lane, west of Sacramento Street, north of Harney Lane, and east of Ham Lane. According to ABC, Census Tract 43.06 contains 7 existing on-sale licenses with 8 on-sale licenses allowed based on the ABC criteria. Because there is no over concentration, the Planning Commission is not required to make a finding of public necessity or convenience in order to approve the on-sale Type-41 license. However, the Planning Commission will need to determine whether or not to grant the request based on the use and the location of the project.

Because Midtown Taqueria is a full service restaurant that would like to sell beer and wine with food, staff does not anticipate the alcohol sales portion of the business to create any problems. This operation would be similar to other restaurants within Lodi. The Planning Commission and the Planning staff have generally supported restaurants that wish to acquire an ABC on-sale beer and wine license because restaurants that serve beer and wine in conjunction with food sales have not created alcohol related problems. If problems or concerns related to the sale of alcoholic beverages occur in the future, staff and/or the Planning Commission may initiate a public hearing where the Commission would have the ability to amend conditions or revoke the Use Permit.

**ENVIRONMENTAL ASSESSMENTS:**

The project was found to be categorically exempt according to the California Environmental Quality Act, Article 19, Guidelines §15321, Class 21 (a) (2). The project is classified as an “Enforcement Action by Regulatory Agencies” because it is the “adoption of an administrative decision or order enforcing...the lease, permit, license, certificate, or entitlement for use or enforcing the general rule, standard, or objective.” No significant impacts are anticipated and no mitigation measures have been required.

**PUBLIC HEARING NOTICE:**

Legal Notice for the Use Permit was re-advertised on July 30, 2008 and 46 public hearing notices were sent to all property owners of record within a 300-foot radius of the subject property as required by Government Code §65091 (a) (3).

**ALTERNATIVE PLANNING COMMISSION ACTIONS:**

- Approve the Use Permit with Alternate Conditions
- Deny the Use Permit
- Continue the Request

Respectfully Submitted,

Concur,

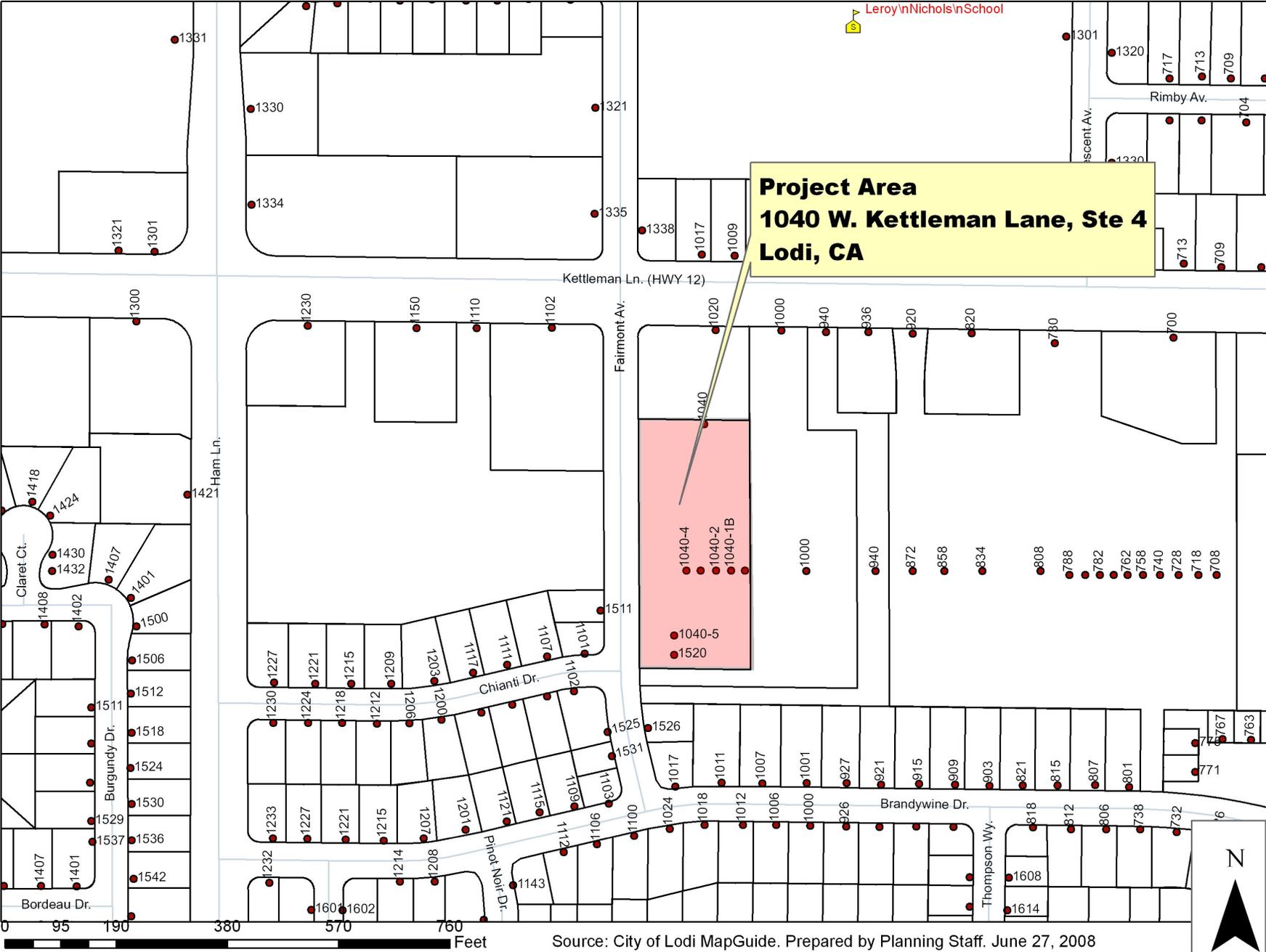
Immanuel Bereket  
Junior Planner

Peter Pirnejad  
Co-Interim Community Development Director

**ATTACHMENTS:**

1. Vicinity Map
2. Aerial Map
3. Floor Plan
4. Menu
5. Names and locations of existing ABC Licenses on Tract 43.06
6. Draft Resolution

# Vicinity Map



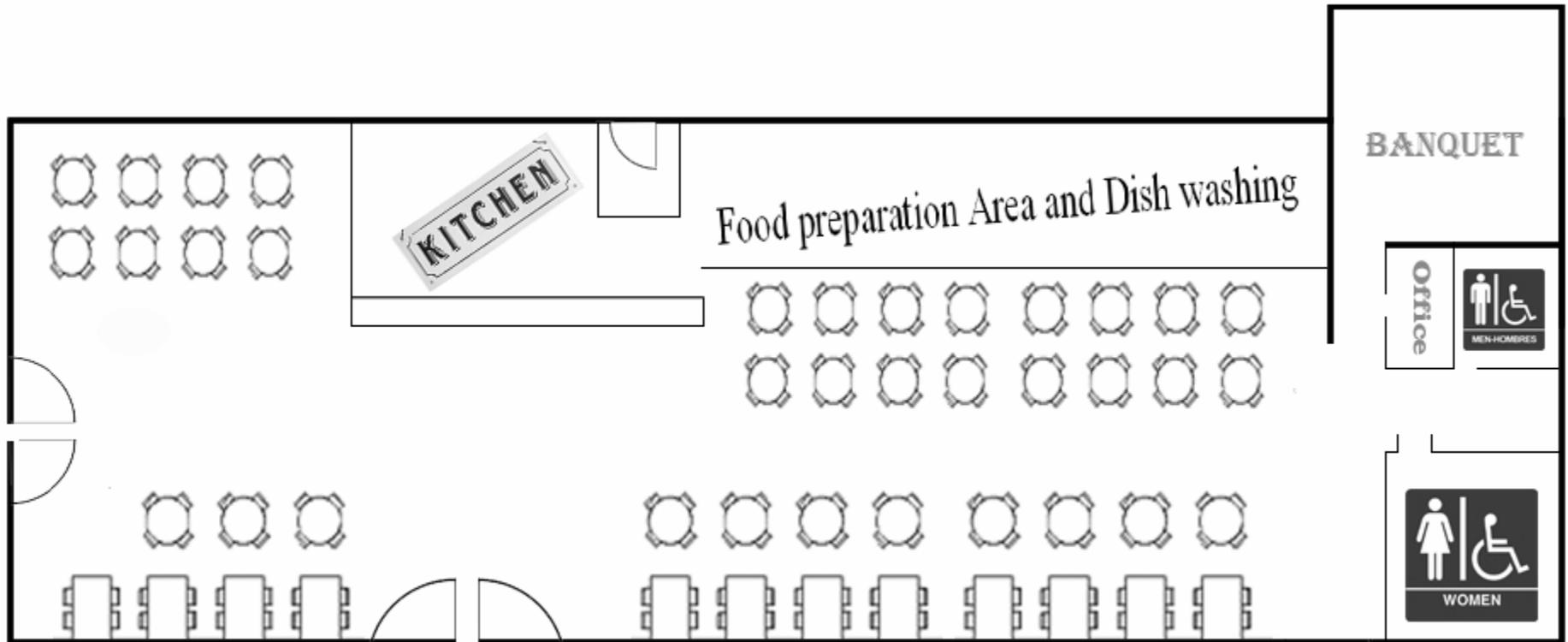
Leroy Nichols School

Source: City of Lodi MapGuide. Prepared by Planning Staff. June 27, 2008

# Aerial Map



# Floor Plan



MIDTOWN TAQUERIA  
1040 WEST KETTLEMAN LANE SUITE 4  
LODI, CA 95241

Torta - Meat, Cheese, Sour Cream, 5.38  
 acamole, Lettuce, And Tomatoes  
 Torta Ahogada - Pork, Refried Beans, Hot Sauce, 5.38  
 & Onions

# Tortas

## Breakfast

Breakfast plate is served with rice, refried beans, lettuce, tomatoes, & choice of tortillas

Huevos Con Jamon - Scrambled eggs with Ham 7.19  
 Huevos Con Chorizo - Scrambled eggs with Sausage 7.19  
 Huevos Con Tosino - Scrambled eggs with Bacon 7.19  
 Huevos Rancheros - Two Sunny side eggs on tortilla 7.19  
 \*add any meat for \$1.00 more  
 Vegetarian Burrito - Egg with stewed tomatoes, rice, 5.00  
 Cheese, Beans, and Salsa  
 Breakfast Burrito - Eggs with Ham or Bacon or 5.00  
 Sausage, Cheese, Beans, and Salsa  
 Mochaca - Scrambled Eggs with Beef, Tomato 7.19  
 Onion, Jalapino & Red Salsa  
 Pozole 5.52



## Drinks

\*Fountain Drinks: 1.86  
 Cola, Diet Cola, Sprite, Lemonade, Nestea, Mr. Pibb  
 \*Aguas Fresca: 1.86  
 Horchata (Rice Water) Pina, Jamaica, Melon  
 \*Bottle Drinks: 2.09  
 Coca Cola, Pina, Naranja, Limon, Mandarine  
 \*Cervezas Bottles: Corona, Pacifico, Modelo 3.25  
 \*Cervezas Mexicanas Draft Glass 3.02 Pitcher 9.97  
 \*Bud Lite Draft Glass 2.78 Pitcher 9.14  
 \*Margaritas 3.75  
 \*Wine 2.95



## Menudo

Sabado Y Domingo  
 Saturday & Sunday \$5.52

Choice of Beans:

Refried, Black or Pinto Beans

POZOLE DAILY 5.52

### Meat Choices

Asada.....Beef  
 Pollo..... Boiled Chicken  
 Pollo Asado.....Grilled Chicken  
 Carnitas.....Shredded Pork  
 Al Pastor.....Mex. BBQ Pork  
 Chile Verde.....Green Chile Pork  
 Chille Colorado..... Red Chile Pork  
 Chorizo.....Sausage  
 Buche  
 Lengua

# East Sac Midtown taqueria



Authentic Flavors of Mexico

Enjoy the best traditional dishes, prepared with the highest quality ingredients.

### Daily Specials

1040 West Kettleman Lane suite 4  
 Lodi, CA 95241  
 Open 9 am to 10 pm

Breakfast, Lunch & Dinner

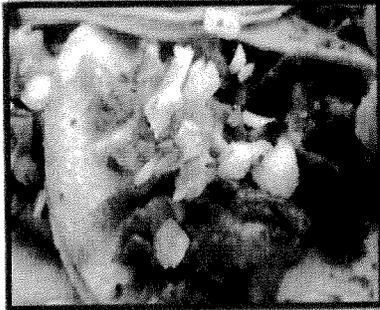


Watch Your Favorite Sports  
 On Our HD TV  
 Cervezas Special During Games

We do Catering & Delivery  
 Free Salsa Bar



# Tacos



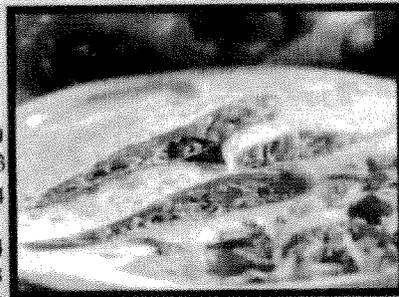
<b>Regular</b>	- Choice of Meat, Onion, Cilantro & Salsa, on Soft Tortilla	1.76
<b>Super</b>	- Choice of Meat, Cheese, Guacamole, Sour Cream, Lettuce, Tomatoes on Soft Tortilla	3.53
<b>Vegetarian</b>	- Beans, Rice, Salsa, Cheese, Guacamole, Sour Cream, Lettuce, Tomatoes on Soft Tortilla	3.53
<b>Crispy</b>	- Meat, Guacamole, Sour Cream, Lettuce, Cheese, & Salsa on Crispy Corn Tortilla	2.92
<b>Taco Salad</b>	- Choice of Meat or Vegetarian with Beans, Rice, Salsa, Cheese, Guacamole, Sour Cream, Lettuce, and Tomatoes	5.43
<b>Fish Taco</b>	- Grilled Fish, Lettuce, Tomatoes & Salsa	2.37
<b>Super</b>	- Fish or Shrimp Taco	3.99

# Nachos



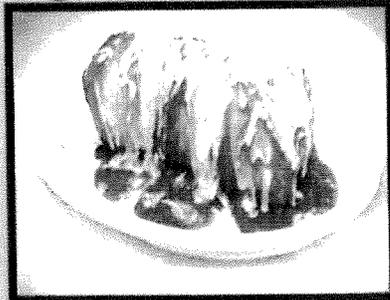
<b>Regular</b>	- Jack Cheese, Guacamole, Salsa, Sour Cream, and Beans	3.57
<b>Regular</b>	- plus meat	4.59
<b>Super Nachos</b>	- Larger Sized of Regular with Meat	5.52

# Quesadillas



<b>Regular</b>	- Flour Tortilla with melted Cheese	2.60
<b>Regular Plus Meat</b>	-	3.43
<b>Super</b>	- Meat, Sour Cream, Guacamole, Salsa & Lettuce	4.64
<b>Vegetarian</b>	- Same as super with no Meat	4.64
<b>Grande</b>	- Same as Super with Larger Tortilla	5.43

# Burritos



<b>Regular</b>	- Choice of Meat, Whole Beans, Rice, & Salsa	5.10
<b>Super</b>	- Meat, Beans, Rice, Salsa, Guacamole, Sour Cream, and Cheese	5.43
<b>Jumbo</b>	- Same as Super plus Lettuce	5.99
<b>Vegetarian</b>	- Rice, Beans, Salsa, Guacamole, Sour Cream, Lettuce, & Cheese	5.10
<b>Shrimp or Fish Burrito</b>	- Rice, Beans, Salsa, Guacamole, Sour Cream, Cheese	6.73
<b>Jr. Burrito</b>	- With Refried Beans and Cheese	3.57
<b>Wet Burrito</b>	- With Enchilada Sauce and Melted Cheese	add only 1.58
<b>Chimichanga</b>	-	5.43



# Tostadas

<b>Regular</b>	- Choice of meat, Refried Beans, Lettuce, sour cream, Guacamole, Salsa, Tomatoes	3.71
<b>Vegetarian</b>	- Same as regular with whole beans, no meat	3.71
<b>Pescado o Cameron</b>	- Fish or Shrimp	3.99

# Platillos

## Special Plates

Special Plates are served with rice, Refried beans, sour cream, guacamole, lettuce, tomatoes, & choice of flour or corn tortillas.

<b>Carne Asada</b>	- Two Pieces of Sirloin Steak Grilled	9.61
<b>Steak Ranchero</b>	- Grilled with Jalapenos, Onions, and Tomatoes	9.61
<b>Pollo Asada</b>	- Grilled Chicken	9.61
<b>Carnitas</b>	- Shredded Pork	9.61
<b>AL PASTOR</b>	- Mexican BBQ Pork	9.61
<b>Steak a la Mexicana</b>	- Marinated Steak Grilled with Onions, Tomatoes & Bell Pepper	9.61
<b>Flautas de Pollo</b>	- Fried Chicken Taquitos	9.61
<b>Fajitas</b>	- Choice of Beef, Chicken, Pork, Grilled Tomato, Onion, Bell Pepper	9.61
<b>Enchiladas</b>	- Choice of Beef, Chicken, or Cheese 2	7.89
	3	9.61
<b>Tamales</b>	- 2 Chicken or Pork Tamales: Rice, Salsa, Lettuce, Beans	9.61
<b>Chille Relleno</b>	- Pasilla with Cheese inside	9.61
<b>Combo Plate</b>	- Shrimp with Steak, Chicken or Pork	10.67



# Mariscos

## Seafood Plates

Special Plates are served with rice, Refried beans, Avacado lettuce, tomatoes, & choice of flour or corn tortillas.

<b>Pescado Platillo</b>	- Fish Filet Fried or Grilled	10.16
<b>A La Diabla</b>	- Spicy Prawns	10.16
<b>Al Mojo</b>	- Garlic Prawns	10.16
<b>Rancheros</b>	- Grilled Prawns, Onion, Tomatos, Jalepinos	10.16
<b>Mojarras</b>	- Whole Fish fried	10.16
<b>Quesadillas Marina</b>	- Seafood Quesadillas with Sour Cream, Guacamole, Lettuce, Cheese, & Salsa	8.82
<b>Enchiladas Marina</b>	- Seafood Enchiladas with Rice, Beans, Sour Cream, Guacamole, Lettuce, Tomato	8.82
<b>Cameron A Laplauche Shrinmp Fajitas</b>	- Grilled Onion, Tomatos, Bell Pepper	10.16
<b>Cockteles Camaron</b>	- Shrimp Cocktail	9.28
<b>Ceviche</b>	-	3.81





California Department of Alcoholic Beverage  
Control  
For the County of **SAN JOAQUIN** - (On-Sale  
Licenses)  
and Census Tract = 0043.06

Report as of 8/7/2008

	License Number	Status	License Type	Orig. Iss. Date	Expir Date	Primary Owner and Premises Addr.	Business Name	Mailing Address	Geo Code
1)	179005	ACT	40	11/18/1985	5/31/2009	LODI ATHLETIC CLUB L-PSHIP 1900 S HUTCHINS ST LODI, CA 95240  Census Tract: 0043.06	TWIN ARBORS ATHLETIC CLUB	11290 PYRITES WAY GOLD RIVER, CA 95670	3902
2)	839719	ACT	47	3/4/2008	2/28/2009	MARTINEZDEROJAS MARIA G 728 W KETTLEMAN LN LODI, CA 95240  Census Tract: 0043.06	EL ROSAL		3902
3)	846530	ACT	41	11/17/1998	7/31/2008	JU SHANGUO 1110 W KETTLEMAN LN LODI, CA 95240  Census Tract: 0043.06	YEN CHING		3902
4)	423007	ACT	41	5/16/2005	4/30/2009	ANAYAMORALES VICTOR 1413 S CHURCH ST LODI, CA 95240  Census Tract: 0043.06	MAR Y TIERRA RESTAURANT	503 S CENTRAL AVE LODI, CA 95240	3902
5)	423501	ACT	41	1/26/2006	12/31/2008	VELVET RESTAURANTS INC 1421 S HAM LN LODI, CA 95242  Census Tract: 0043.06	VELVET GRILL & CREAMERY	25 SIERRA VISTA RD SANTA BARBARA, CA 93108	3902
6)	424723	ACT	41	5/19/2005	4/30/2009	PRIETO JORGE 226 W KETTLEMAN LN LODI, CA 95240  Census Tract: 0043.06	HAPPY BURRO MEXICAN RESTAURANT		3902
7)	434242	ACT	47	2/10/2006	1/31/2009	SANCHEZ ELENA 1110 W KETTLEMAN LN LODI, CA 95240  Census Tract: 0043.06	CASA MEXICANA		3902

**RESOLUTION NO. P.C. 08-17**

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LODI APPROVING THE REQUEST OF JOE DOUMIT FOR A USE PERMIT TO ALLOW ON-SALE BEER AND WINE AT MIDTOWN TAQUERIA LOCATED AT 1040 WEST KETTLEMAN LANE SUITE 4.  
(FILE # 08-U-07)**

**WHEREAS**, the Planning Commission of the City of Lodi has held a duly noticed public hearing, as required by law, on the requested Use Permit, in accordance with the Lodi Municipal Code Section 17.72.070; and

**WHEREAS**, the project proponent is Joe Doumit; and

**WHEREAS**, the property owner is Joe Doumit., 3754 J Street., Sacramento, CA 95816; and

**WHEREAS**, the property is located at 1040 West Kettleman Lane Suite 4, Lodi, CA (APN 027-420-14); and

**WHEREAS**, the property has a General Plan designation of NCC, Neighborhood Community Commercial and is Zoned P-D, Planned Development; and

**WHEREAS**, the Use Permit to allow the sale of beer and wine for on-site consumption within the restaurant is an enforcement action in accordance with the City of Lodi Zoning Ordinance; and

**WHEREAS**, Census Tract 43.06 in which the restaurant is located currently has no over concentration of licenses allowing on premise consumption of alcoholic beverages the Planning Commission is not required to find public convenience or necessity in order to permit the issuance of an additional Alcohol Beverage Control license in this tract; and

**WHEREAS**, the State Department of Alcoholic Beverage Control (ABC) has training available that clearly communicates State law concerning the sale of alcoholic beverages; and

**WHEREAS**, all legal prerequisites to the adoption of this Resolution have occurred.

Based upon the evidence within the staff report and project file the Planning Commission finds:

1. The project is categorically exempt according to the California Environmental Quality Act, §15321, Class 21 (a) (2) (Enforcement Action by Regulatory Agency). The permit is being granted under adoption of an administrative decision or order enforcing the ABC license and enforcing Section 17.72.070 of the Zoning Ordinance and no significant impacts are anticipated and no mitigation measures have been required.
2. In order to comply with the State Department of Alcoholic Beverage Control, a Type-41 on-sale beer and wine license requires the sale of alcoholic beverages be secondary to food sales, food sales are the major activity of the project proponent's business.
3. The sale of alcoholic beverages as part of a restaurant is a public convenience that does not typically create alcohol related problems.
4. The sale and consumption of alcohol can sometimes result in behavior problems that can lead to law enforcement issues that require police intervention. However, staff does not anticipate alcohol related problems since the sale of alcohol is secondary to the sale of food.
5. The proposed use is expected to be compatible with the surrounding use and neighborhood.
6. The granting of the Use Permit is consistent with the City's General Plan and Zoning Ordinance.

**NOW, THEREFORE, BE IT DETERMINED AND RESOLVED** by the Planning Commission of the City of Lodi that Use Permit Application No. 08-U-07 is hereby approved, subject to the following conditions:

1. The project proponent will defend, indemnify, and hold the City, its agents, officers, and employees harmless of any claim, action, or proceeding to attack, set aside, void, or annul this permit, so long as the City promptly notifies the developer of any claim, action, or proceedings, and the City cooperates fully in defense of the action or proceedings.
2. The project proponent shall operate and abide by the requirements and conditions of the State of California Department of Alcoholic Beverage Control License Type-41.
3. The Type-41 License shall be limited to on-site sale and consumption of beer and wine during the hours that the restaurant is open for dining.
4. There shall be no off-sale of alcoholic beverages and the sale of food shall compose more than 50 percent of gross sales receipts.
5. Prior to the issuance of a Type-41 license, the project proponent and the on-site manager shall complete Licensee Education on Alcohol and Drugs as provided by the State Department of Alcoholic Beverage Control.
6. The project proponent shall insure that the serving of alcohol does not cause any condition that will cause or result in repeated activities that are harmful to the health, peace, welfare or safety of persons residing or working in the surrounding area. This includes, but is not limited to: disturbances of the peace, illegal drug activity, public drunkenness, drinking in public, harassment of passerby, assaults, batteries, acts of vandalism, loitering, excessive littering, illegal parking, excessive loud noises (especially in the late night or early morning hours), traffic violations or traffic safety based upon last drink statistics, curfew violations, lewd conduct, or police detention and arrests.
7. The conditions of the Use Permit are subject to review by staff and the Planning Commission for compliance.
8. No variance from any City of Lodi adopted code, policy or specification is granted or implied by the approval of this resolution.

Dated: August 13, 2008

I hereby certify that Resolution No. 08-17 was passed and adopted by the Planning Commission of the City of Lodi at a regular meeting held on August 13, 2008 by the following vote:

AYES: Commissioners:

NOES: Commissioners:

ABSENT: Commissioners:

**ATTEST:** \_\_\_\_\_  
**Secretary, Planning Commission**

# Item 3b.

**CITY OF LODI  
PLANNING COMMISSION  
Staff Report**

---

**MEETING DATE:** August 13, 2008

**APPLICATION NO:** Use Permit: 08-U-08

**REQUEST:** Request for Planning Commission approval of a Use Permit to allow a Type 2 (Winery) Alcoholic Beverage Control License and Live Entertainment at 112 W. Pine Street. (Applicant: Benson Ferry Vineyard, DBA; File Number: 08-U-08)

**LOCATION:** 112 West Pine Street, Lodi. (APN: 043-034-04)

**APPLICANTS:** Benson Ferry Vineyards, LLC  
114 West Pine Street  
Lodi, CA 95241

**PROPERTY OWNER:** Lodi Hotel Investors Ltd.  
1001 6TH ST #200  
Sacramento, CA 95814

**RECOMMENDATION:**

Staff recommends that the Planning Commission approve the request of Benson Ferry Vineyards, LLC for a Use Permit to allow a Type 2 Winemaker Liquor License and Live Entertainment at 112 West Pine Street, subject to the conditions in the attached resolution.

**PROJECT/AREA DESCRIPTION**

**General Plan Designation:** DC, Downtown Commercial.  
**Zoning Designation:** C-2, General Commercial.  
**Property Size:** 27,095 sq. ft. Tasting room and other rooms used by the project proponent measures approximately 2,400 square ft.

The adjacent zoning and land use are as follows:

- North:** C-2, General Commercial. The uses north of the project site are a mixture of retail commercial, offices and service businesses that are part of the downtown commercial area. The downtown area generally extends north to Lockeford Street. There are also scattered bars and restaurants that serve alcohol in the downtown area.
- South:** C-2, General Commercial. There are a variety of commercial uses to the south, including retail stores, offices, eating establishments and a residential hotel. This area is included in the downtown area that extends south to Lodi Ave.
- East:** C-2, General Commercial. The uses to the east of the project are area a mixture of retail commercial, offices and service businesses that are part of the downtown commercial area.
- West:** C-2, General Commercial. The area to the west is near the edge of the Downtown District area. There are a mixture of retail commercial, offices and service businesses that are part of the downtown commercial area.

## **Summary**

The project proponent, Benson Ferry Vineyard, LLC., is requesting a Use Permit to allow a Type 2 (Winery) Alcoholic Beverage Control License at 112 West Pine Street. The project proponent is requesting a Type 2 ABC license, which allows on and off sale of wine, wine display and wine tasting. The Type 2 license is required by the Alcohol Beverage Control (ABC) for the operation of a winery and wine tasting. The Benson Ferry Vineyard intends to use this location to promote the sale of their product as well as products of other selected wineries. Each participant's wine will be sold either by the bottle or case through wine tasting and from displays. Small to midsize wineries, who already hold Type-2 (Winery) Alcoholic Beverage Control License and are interested in being represented in downtown, will be offered a place for display of their products. The project proponent is also requesting approval of a Use Permit for live music on Thursday, Friday, and Saturday nights from 6:00 p.m. to 10:00 p.m. Live music will be conducted either within the wine tasting facility or in the rear courtyard of the Lodi Hotel.

## **Background**

The applicant is proposing to establish a wine tasting room that would promote the sale of their wine products by the bottle or case through wine tasting and display. They also intend to sell wine related gift items such as decanters, stemware, and cork screws. Wearable logo items will also be made available for purchase, as well as gourmet packaged food items. The proposed use is consistent with the City's vision of making Lodi a wine tasting tourist destination point. Wine tasting rooms are a large part of this vision for they not only compliment other downtown businesses such as restaurants and boutiques, but add to the character of the City by promoting local wineries and tourism.

The property is located in Downtown Lodi and is zoned C-2, General Commercial. The proposed wine-tasting room along with adjoining commercial spaces on Pine and School Streets comprise the ground floor commercial space of the Lodi Hotel building. The residential hotel, which has been converted to senior residential apartments, occupies the upper two floors. The commercial spaces are physically separate from the residential units and have specific entrances onto the street. The C-2 Zone permits the sale of alcoholic beverages, subject to the approval of a Use Permit. Properties within Downtown Lodi are subject to The Downtown Development Standards and Guidelines, which augment the requirements of the Zoning Code. The Downtown Standards and Guidelines describe business service, retail and eating and drinking establishments as permitted uses within the Downtown District. The proposed use does not conflict with the C-2 Zone or the Downtown Development Standards and Guidelines.

## **Analysis**

The project proponent, Benson Ferry Vineyards, is seeking to establish a wine tasting room that would house small to midsize local wineries. The tasting room will be known as Lodi Wine Cellars and will be housed within existing commercial space located at 112 West Pine Street. The applicants hope that the proposed Lodi Wine Cellars will entice a variety of wineries to conduct wine tasting under one roof. Benson Ferry Vineyards as well as other select wineries will offer tasting of their products at the proposed Lodi Wine Cellars. The still unnamed wineries will transfer their duplicate Type-02 (Wine) licenses to the 112 West Pine Street address so they can conduct wine tasting, displaying and sale of their products. Those that do not hold Type-02 license will be required to obtain all necessary licenses from the State and as well as Use Permits from the City. There will be no wine crushing at this proposed wine tasting room.

The project proponent, Benson Ferry Vineyards, is requesting approval of a Use Permit to allow the sale of bottles and cases of wine, a wine tasting operation, use of the wine tasting space for various events such as wine dinners catered by outside vendors; private mixers; wine tasting and parties for organizations and businesses who want to rent space; and to host feature wine tasting events that may include educational components. The Type 2 license is required by the Alcohol Beverage Control (ABC) for the operation of a winery and wine tasting. The project is located in a C-2 zone and the building offers space for the proposed type of operation. The wine tasting room will be located within the City's

downtown and will support and compliment other downtown businesses. The Lodi Wine Cellars will feature an office, a wine tasting room, display room, kitchen, wine tasting bar and a storage basement that will not be accessible to the public. The project is within the Downtown Parking District and therefore does not need to provide on-site parking. Customers and employees can utilize the parking structure or other public parking spaces in the downtown area.

As mentioned earlier, the proposed Lodi Wine Cellars will house the business office of Benson Ferry Vineyards as well as other wineries. The offices will be segregated from the tasting area. There is an existing basement that will be used for storage and private meetings, but will not be open to the public. Tasting of the various wineries varietals will be on a scheduled basis. Tasting hours will be as follows:

Monday – Thursday	3:00 p.m. – 11:00 p.m.
Friday – Saturday	3:00 p.m. – 12:00 a.m.
Sunday	By appointment only, but will close no later than 7:00 p.m.

The Lodi Municipal Code requires a Conditional Use Permit for the sale of alcohol. The provision covering alcohol sales is found in Section 17.72.040 of the Zoning Code and applies to off-sale as well as on-sale alcohol outlets. The City established the Use Permit requirement to gain local control over whether or not a license is appropriate for a particular location. The State Department of Alcoholic Beverage Control primarily controls issuance based on concentration of licenses within a particular Census Tract. The State Department of Alcoholic Beverage Control does not restrict the number of on-sale beer and wine licenses as is the case for off sale licenses. The approval of this Use Permit application will not require a finding of public convenience or necessity as defined by the ABC for the issuance of the permit.

The C-2 zoning is designated for a variety of general commercial uses, including wine tasting. The proposed wine tasting room is consistent with the types of uses one would expect to find in a General Commercial zone. All of the wine tasting operations will take place within the building space and/or the shared courtyard in the back. The proposed use is consistent with the City’s vision of making Lodi a wine tasting tourist destination point. Wine tasting rooms are a large part of this vision for they not only compliment other downtown businesses such as restaurants and boutiques but add to the character of the City by promoting local wineries and viticulture. The intended nature of this operation does not promote excessive onsite consumption of alcohol and, therefore, staff does not expect any problems with this type of establishment.

In addition to a Use Permit, the project proponent is also requesting a Live Entertainment permit to allow live music on Thursday, Friday, and Saturday nights from 6:00 p.m. to 10:00 p.m. For entertainment there will be music from bands that will perform jazz and other similar music. Live entertainment will end on or before 10:00 p.m. The applicant will not have a cover charge and the patrons will be age 21 and over. The proposed operation is similar to the Cellar Doors located at 21 N. School Street, which hosts a number of wineries, live music, and private parties.

The ABC Use Permit and the Live Entertainment requests have been reviewed by various City Departments including the Lodi Police Department. Each department recommends approval subject to the conditions in the attached resolution. The Community Development Department is of the opinion that the proposed project is consistent with the surrounding land uses and zoning; and with the City’s policy to encourage the wine production and wine tasting industry. Staff is therefore recommending approval of a Live Entertainment permit for Benson Ferry Vineyards/Lodi Wine Cellars and for a Type 2 ABC Wine Growers license to operate a winery at this location, subject to the conditions included in the attached Resolution.

**ENVIRONMENTAL ASSESSMENTS:**

The project was found to be Categorically Exempt according to the California Environmental Quality Act, Article 19 §1532, Class 21 (a) (2). The project is classified as an “Enforcement action by regulatory agencies” because it is the “adoption of an administrative decision or order enforcing ...the lease, permit,

license, certificate, or entitlement for use or enforcing the general rule, standard, or objective.” No significant impacts are anticipated and no mitigation measures have been required.

**PUBLIC HEARING NOTICE:**

Legal Notice for the Use Permit was published on July 30, 2008. A total of 53 notices were sent to all property owners of record within a 300-foot radius of the subject property.

**ALTERNATIVE PLANNING COMMISSION ACTIONS:**

- Approve the Use Permit with Alternate Conditions
- Deny the Use Permit
- Continue the Request

Respectfully Submitted,

Concur,

Immanuel Bereket  
Junior Planner

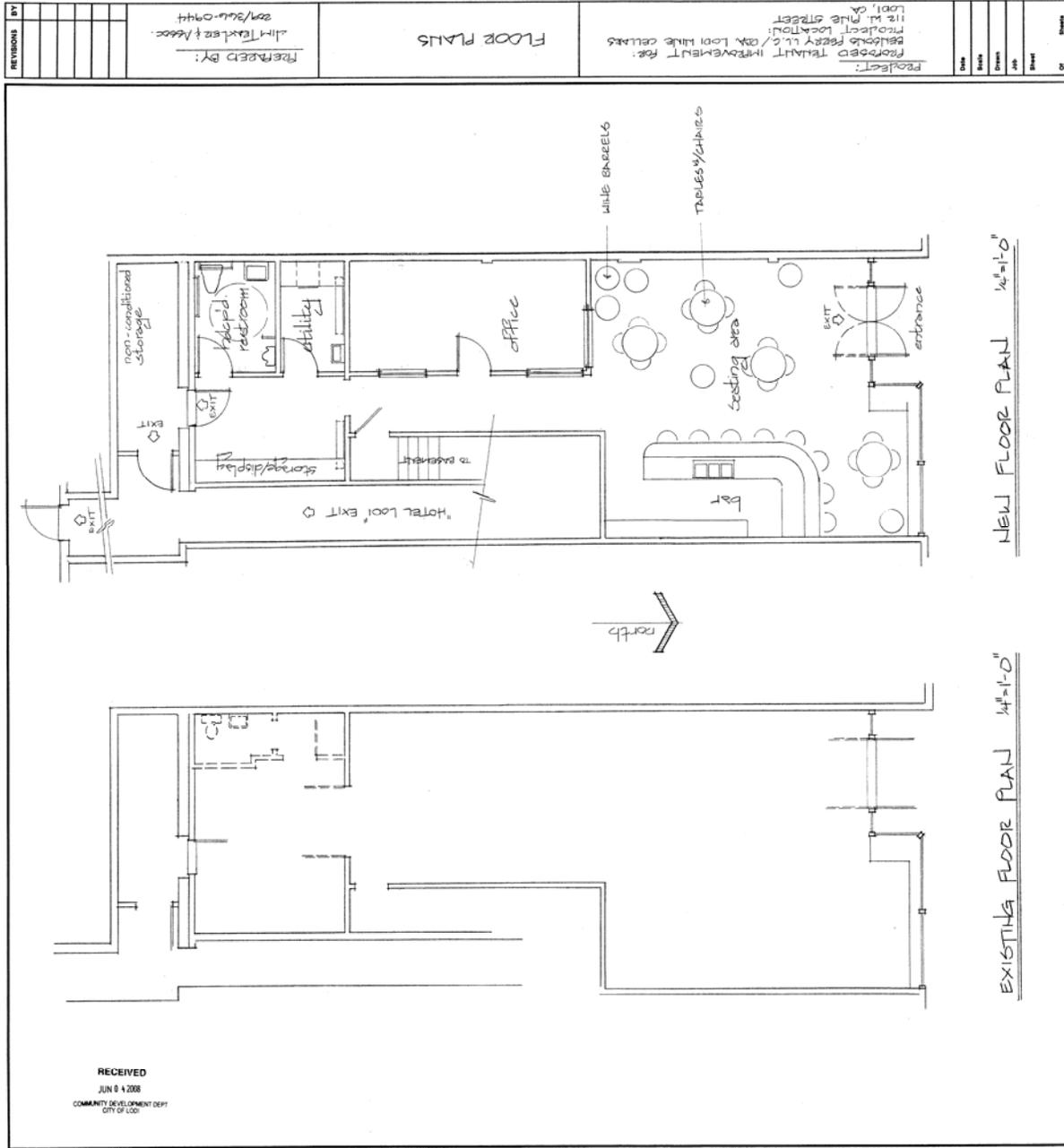
Peter Pirnejad  
Co-Interim Community Development Director

**ATTACHMENTS:**

1. Floor Plan
2. Resolutions



# Floor Plan



RECEIVED  
 JUN 8 4 2008  
 COMMUNITY DEVELOPMENT DEPT  
 CITY OF LEO

DATE: 6/10/08 10:00 AM

Project:	PROPOSED TALENT IMPROVEMENT FAC.
Location:	BRAND PARKY L.O. / OAK LOON WINE CELLARS
Address:	112 W. 9th STREET
City:	LEO, CA
Scale:	
Date:	
Drawn:	
Check:	
Sheet:	
Of:	

FLOOR PLANS

Prepared By: alr/ta/2008-07-14

REVISIONS	BY

**RESOLUTION NO. P.C. 08-19**

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LODI APPROVING THE REQUEST OF THE BENSON FERRY VINEYARDS LLC FOR A USE PERMIT FOR A TYPE-2 WINERY ABC LICENSE TO PERMIT A WINE TASTING OPERATION WITH ON-AND-OFF SALE OF WINE AT 112 WEST PINE STREET.**

**(FILE # 08-U-08)**

**WHEREAS**, the Planning Commission of the City of Lodi has held a duly noticed public hearing, as required by law, on the requested Use Permit, in accordance with the Lodi Municipal Code Section 17.72.070; and

**WHEREAS**, the project proponent is Benson Ferry Vineyards, LLC; and

**WHEREAS**, the property owner is Lodi Hotel Investors LTD, 1001 6<sup>th</sup> Street #200, Sacramento, CA 95814; and

**WHEREAS**, the property has a General Plan designation of Downtown Commercial and is zoned C-2, General Commercial; and

**WHEREAS**, the project property is located at 112 West Pine Street, Lodi, CA (APN 043-034-04); and

**WHEREAS**, the requested Use Permit to allow the operation of on-site wine tasting and sale of wine is an enforcement action in accordance with the City of Lodi Zoning Ordinance; and

**WHEREAS**, Census Tract 42.04 in which the project is located permits the proposed use; and

**WHEREAS**, the State Department of Alcoholic Beverage Control has training available that clearly communicates State law concerning the sale of alcoholic beverages.

**WHEREAS**, all legal prerequisites to the adoption of this Resolution have occurred.

Based upon the evidence within the staff report and project file the Planning Commission finds:

1. The project is categorically exempt according to the California Environmental Quality Act, §15321, Class 21 (a) (2) (Enforcement Action by Regulatory Agency). The permit is being granted under adoption of an administrative decision or order enforcing the ABC license and enforcing Section 17.72.070 of the Zoning Ordinance and no significant impacts are anticipated and no mitigation measures have been required.
2. The project is consistent with the City of Lodi General Plan and Zoning ordinance.
3. The project will be consistent with the surrounding land uses.
4. No land use problems or issues are anticipated as a result of this project.
5. The proposed project as it consistent with the City of Lodi's desire to encourage wine-tasting facilities and other types of tourist related businesses to locate in downtown Lodi.

**NOW, THEREFORE, BE IT DETERMINED AND RESOLVED** by the Planning Commission of the City of Lodi that Use Permit Application No. 08-U-08 is hereby approved, subject to the following conditions:

Community Development Department, Planning:

1. The project proponent will defend, indemnify, and hold the City, its agents, officers, and employees harmless of any claim, action, or proceeding to attack, set aside, void, or annul this permit, so long as the City promptly notifies the developer of any claim, action, or proceedings, and the City cooperates fully in defense of the action or proceedings.

2. The project proponent shall insure that the serving of alcohol provision of live entertainment does not cause any condition that will cause or result in repeated activities that are harmful to the health, peace or safety of persons residing or working in the surrounding area. This includes, but is not limited to: disturbances of the peace, illegal drug activity, public drunkenness, drinking in public, harassment of passerby, assaults, batteries, acts of vandalism, loitering, excessive littering, illegal parking, excessive loud noises (especially in the late night or early morning hours), traffic violations or traffic safety based upon last drink statistics, curfew violations, lewd conduct, or police detention and arrests.
3. The project proponent shall operate and abide by the requirements and conditions of the State of California Department of Alcoholic Beverage Control License Type-2.
4. Prior to the issuance of a Type-2 license, the project proponent and on-site store manager shall complete Licensee Education on Alcohol and Drugs as provided by the State Department of Alcoholic Beverage Control.
5. The project proponent and/or on-site store manager shall insure that the serving of alcohol does not cause any condition that will cause or result in repeated activities that are harmful to the health, peace, welfare or safety of persons residing or working in the surrounding area. This includes, but is not limited to: disturbances of the peace, illegal drug activity, public drunkenness, drinking in public, harassment of passerby, assaults, batteries, acts of vandalism, loitering, excessive littering, illegal parking, excessive loud noises (especially in the late night or early morning hours), traffic violations or traffic safety based upon last drink statistics, curfew violations, lewd conduct, or police detention and arrests.
6. The conditions of the Use Permit are subject to review by staff and the Planning Commission for compliance.
7. No variance from any City of Lodi adopted code, policy or specification is granted or implied by the approval of this resolution.
8. The wine tasting operation shall be conducted in a manner that will not adversely impact neighboring properties or businesses.
9. All temporary and permanent signage proposed in connection with the wine tasting operation shall be reviewed and approved by the Community Development Department.
10. That warehouse and storage of wine shall be ancillary to the office and sales aspect of the operation. The subject facility shall not serve as the primary distribution center for any wineries utilizing the facility.
11. That applicants shall be allowed to use the wine tasting space for various events such as wine dinners catered by outside vendors, private mixers, wine tasting and parties for organizations and businesses who want to rent space, and to host feature wine tasting events that may include educational components.

Community Development Department, Building Division :

12. The applicant shall submit complete and adequate Tenant Improvement plans to the Building Division for review and approval. The said plans shall be based on the City of Lodi Building Regulations and currently adopted 2007 California Building Code.
13. Egress shall not pass through kitchens, storage rooms, closets or spaces used for similar purposes per 2007 CBC, Section 1014.2(2).
14. All existing gates on the outdoor seating area that are required exits shall meet accessibility requirements as an exit door per 2007 CBC, Section 1133B.1.1.1.4.
15. Fire rated horizontal occupancy separation between the basement and the tasting room shall be provided as per 2007 CBC, Table 508.3.3.

16. The applicant shall provide restrooms conforming with all disabled access requirements (C.C.R., Title 24) and the 2001 California Plumbing Code Table 4-1

Fire Department:

17. Fire sprinklers system shall be provided in the basement and on the first floor per CFC 2007 Sections 903.2.10.1 and 903.2.1.2.
18. A certified fire alarm sprinkler monitoring system shall be required per CFC 2007 Sections 903.4 and 903.4.1 and per Lodi Municipal Code Section 15.20.190.
19. Adequate number of exits shall be provided from the facility and outdoor seating area per Fire sprinklers system shall be provided in the basement and on the first floor per CFC 2007 Sections 903.2.10.1 and 903.2.1.2.

Dated: August 13, 2008

I hereby certify that Resolution No. 08-19 was passed and adopted by the Planning Commission of the City of Lodi at a regular meeting held on August 13, 2008 by the following vote:

AYES: Commissioners:

NOES: Commissioners:

ABSENT: Commissioners:

**ATTEST:** \_\_\_\_\_  
**Secretary, Planning Commission**

**RESOLUTION NO. P.C. 08-20**

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LODI APPROVING THE REQUEST OF THE BENSON FERRY VINEYARDS LLC FOR A LIVE ENTERTAINMENT PERMIT IN CONJUNCTION WITH A WINE TASTING OPERATION AT 112 WEST PINE STREET.**

**(FILE # 08-U-08)**

**WHEREAS**, the Planning Commission of the City of Lodi has held a duly noticed public hearing, as required by law, on the requested Use Permit, in accordance with the Lodi Municipal Code Section 17.72.070; and

**WHEREAS**, the project proponent is Benson Ferry Vineyards, LLC; and

**WHEREAS**, the property owner is Lodi Hotel Investors LTD, 1001 6<sup>th</sup> Street #200, Sacramento, CA 95814; and

**WHEREAS**, the property has a General Plan designation of Downtown Commercial and is zoned C-2, General Commercial; and

**WHEREAS**, the project property is located at 112 West Pine Street, Lodi, CA (APN 043-034-04); and

**WHEREAS**, the requested Use Permit to allow live entertainment in conjunction with the operation of on-site wine tasting and sale of wine is an enforcement action in accordance with the City of Lodi Zoning Ordinance; and

**WHEREAS**, all legal prerequisites to the adoption of this Resolution have occurred.

Based upon the evidence within the staff report and project file the Planning Commission finds:

1. The project is categorically exempt according to the California Environmental Quality Act, §15321, Class 21 (a) (2) (Enforcement Action by Regulatory Agency). The permit is being granted under adoption of an administrative decision and no significant impacts are anticipated and no mitigation measures have been required.
2. The project is consistent with the City of Lodi General Plan and Zoning ordinance.
3. The project will be consistent with the surrounding land uses.
4. No land use problems or issues are anticipated as a result of this project.
5. The proposed project is beneficial as it consistent with the City of Lodi's desire to encourage entertainment facilities and other types of tourist related businesses to locate in downtown Lodi.

**NOW, THEREFORE, BE IT DETERMINED AND RESOLVED** by the Planning Commission of the City of Lodi that Live Entertainment Application is hereby approved, subject to the following conditions:

Community Development Department, Planning:

1. The project proponent will defend, indemnify, and hold the City, its agents, officers, and employees harmless of any claim, action, or proceeding to attack, set aside, void, or annul this permit, so long as the City promptly notifies the developer of any claim, action, or proceedings, and the City cooperates fully in defense of the action or proceedings.
2. The project proponent shall insure that the serving of alcohol provision of live entertainment does not cause any condition that will cause or result in repeated activities that are harmful to the health, peace or safety of persons residing or working in the

surrounding area. This includes, but is not limited to: disturbances of the peace, illegal drug activity, public drunkenness, drinking in public, harassment of passerby, assaults, batteries, acts of vandalism, loitering, excessive littering, illegal parking, excessive loud noises (especially in the late night or early morning hours), traffic violations or traffic safety based upon last drink statistics, curfew violations, lewd conduct, or police detention and arrests.

3. Live entertainment shall be limited to Thursday, Friday and Saturday evenings between the hours of 6:00 p.m. and 10:00 p.m., and shall be limited to the shared courtyard and within the wine tasting facilities.
4. Within 12-months of the date of the start of live entertainment a noticed Public Hearing shall be held to review the status of the operation and any concerns or problems. The project proponent shall notify the Community Development Department of the date live entertainment will commence.
5. Security lighting shall be installed on the shared courtyard and other areas where found necessary and appropriate by the Police Chief and Community Development Director. The lighting shall be shielded and low level to avoid glare on neighboring properties. The applicant is responsible for obtaining all necessary permits and approvals.
6. No variance from any City of Lodi adopted code, policy or specification is granted or implied by the approval of this resolution.
7. The Live entertainment portion of the operation shall be conducted in a manner that will not adversely impact neighboring properties or businesses.

Dated: August 13, 2008

I hereby certify that Resolution No. 08-20 was passed and adopted by the Planning Commission of the City of Lodi at a regular meeting held on August 13, 2008 by the following vote:

AYES: Commissioners:

NOES: Commissioners:

ABSENT: Commissioners:

**ATTEST:** \_\_\_\_\_  
**Secretary, Planning Commission**

# Item 3c.

**LODI  
PLANNING COMMISSION  
Staff Report**

---

**MEETING DATE:** August 13, 2008

**REQUEST:** Review and recommend to the City Council Transit Oriented Development Design Guidelines.

**RECOMMENDATION**

Staff recommends that the Planning Commission review the proposed guidelines for Transit Oriented Development (TOD) and recommend that the City Council adopt said Guidelines.

**SUMMARY**

The City of Lodi received a Community Based Planning grant in the amount of \$75,000 with an in kind contribution of \$9,250 and a fiscal contribution of \$9,500 from street funds and Measure K for a Transit Oriented Development (TOD) Plan. The City was awarded this money to perform a planning study of Transit Oriented Development Design Guidelines. A consultant firm, Moore Iacofano Goltsman (MIG), was selected to lead the development of a workbook.

In the fall of 2007, residents, developers, merchants, community members, and planners worked together in a community planning process to create design guidelines for transit-oriented development (TOD) around the Lodi Multi-Modal Transit Station. TOD is defined as compact, higher-density mixed-use development in a highly pedestrian-oriented environment that encourages people to walk, ride transit, and bike, in addition to driving cars. TOD focuses on the intersection of transportation and land use, a crucial connection in the transformation of existing areas into sustainable communities. TOD addresses environmental concerns by promoting alternate transportation modes, concentrating development in urbanized areas, discouraging greenfield development, and supporting healthy communities.

The intent of Transit Oriented Development Design Guidelines is to partner with property owners, businesses and transportation interests within the designated Railroad Corridor area along Sacramento Street to create a workbook for Transit Oriented Development in Lodi. A committee made up of property owners, staff, and selected consultants, participated in the process of identifying different properties to include in the study area. These properties included both vacant and developed parcels. It is hoped the design guidelines will help to identify proactive steps the City could take to promote the specific types of mixed uses envisioned by the workbook. These steps may include things such as the creation of a special district, waiving or reducing parking requirements for certain uses, special overlay zoning, and other incentives that the City could use to encourage TODs in the corridor. The ultimate goal of this project is to provide design direction for building and streetscape improvements along Sacramento Street adjacent to the Multi-Modal Transit Station, as well as to enhance uses in the Downtown district to make it more transit and pedestrian friendly.

The Transit Oriented Development Design Guidelines reflects both the community's and staff's recommendations on ways to reduce the City's dependency on the use of cars by providing pedestrian and transit access to nearby residences and commercial services. The ultimate intent of the Downtown Lodi TOD Design Guidelines is to provide direction for future development within the specified area around the the Multi-Modal Transit Station (See attached Project Area Map). In

addition to supporting the Multi-Modal Transit Station, the Design Guidelines will also enhance existing uses in the Downtown district to make it a more transit- and pedestrian-friendly place.

## **PROJECT DESCRIPTION**

The Transit Oriented Development area centers in the heart of Lodi around the existing commercial developments in the Downtown District. It builds on the investment made when the City of Lodi refurbished the train depot into a historic multi-modal transit center offering numerous rail and transit options not only within the City of Lodi, but to neighboring communities. The project study area is bounded by Lockeford Street to the north, Lodi Avenue to the south, Main Street to the east and School Street to the west. This study area falls within the ¼-mile radius around the Multi-Modal Transit Station. The project area is more focused to extract significant scenarios recommendations that promote Transit Oriented Development and that can serve as a template to be used throughout the Downtown. Various residential neighborhoods adjacent to the project area form an integral part of the area's overall identity and character. It should be noted that these guidelines are neither a requirement nor a mandate. The document simply establishes guidelines that can be used by any interested party. The guidelines were developed with considerable public input and participation. Public participation and community involvement played critical role in creating the guidelines.

## **ANALYSIS**

### **PLANNING PROCESS**

In September 2007 the consultant team from Moore Iacofano Goltsman (MIG) visited Lodi for a project initiation meeting with City staff. At this initial meeting, the consultant and the City staff worked together to define project objectives, identify key stakeholders and brainstorm work program elements. The City wanted the Transit Oriented Development Design Guidelines to be a community-based effort. A Steering Committee made up of key stakeholders, City staff and the public was formed shortly after the kick off meeting. The Steering Committee, made up of a diverse group of citizens and individuals that have an interest in the development of the downtown, was formed. This group helped to direct staff and the consultant team on issues and opportunities of interest to residents, businesses and the community at large. The Committee held its first meeting on October 2007 for an orientation session and walking tour of the project area. It was decided that they should meet two additional times, the second time in December and the third meeting in February of 2008. A community workshop was scheduled for November 2007.

On November 2007, a community workshop was held at the Loel Center that drew over 75 attendees who discussed the concept of Transit Oriented Development. This public workshop was advertised on the local newspaper and was open to any interested party. The urban planning and design firm, Moore, Iacofano, and Goltsman Inc. (MIG) facilitated the workshop and prepared preliminary guidelines for discussion purposes. At this meeting, participants were divided into groups and instructed to come up with ideas for design guidelines. Each group was led by a member of the consulting team and a Steering Committee member. Additionally, the participants reviewed the project goals and discussed strengths, weaknesses, opportunities and challenges to create a shared understanding of existing conditions in the study area.

At the second Steering Committee meeting, members discussed ways to refine and consolidate the ideas put forward in the community workshop meeting; discussed ways to incorporate subsequent community response and inputs; discussed specific catalytic development sites and the steps needed to create a document that is consistent with the Lodi Municipal Code. The Steering Committees comments were forwarded to the consultant. The last Steering Committee meeting was held on February 2008. At this meeting, the Steering Committee reviewed the general content of the guidelines, enhanced uses in the Downtown district to make it more transit and pedestrian-

friendly, and suggested ways to promote revitalization along Sacramento Street, adjacent to the Multi-Modal Transit Station.

The Transit-Oriented Development Design Guidelines document is the result of a year-long planning collaboration between residents, developers, merchants, community members, and the City of Lodi. The guidelines were developed with considerable public input and participation. Public participation and community involvement played critical role in creating the guidelines (See attachment 1.). Developed through an open and comprehensive community planning process, the Transit-Oriented Development Design Guidelines illustrates various design recommendations for new development and public improvements around the Lodi Multi-Modal Transit Station. These initiatives set the foundation for creating significant transit-oriented development (TOD) in Lodi in the coming years. Recent studies of transit ridership in California (see Gov. Code 65460.1) indicate that persons who live near transit and rail transit stations utilize public transportation in far greater numbers than does the general public living elsewhere. This was reasoning used to formulate the study area within ¼-mile of the Multi-Modal Transit Station. The ¼-mile radius extending from the Multi-Modal Transit Station and parking structure is a crucial component of examining and planning for TOD. The study area extends from School Street on the west to Main Street on the east and from Lockeford Street on the north to Lodi Ave on the south.

The types of mixed use developments envisioned by the Transit-Oriented Development Design Guidelines are located next to transit depots such as Lodi's Multi-Model Transit Station to reduce the dependence of single passenger vehicle trips for commuting and promote the use of mass transit. Staff recommends that the Planning Commission endorse a resolution to the City Council recommending the adoption of the Transit Oriented Development Design Guidelines document.

### **ENVIRONMENTAL ASSESSMENTS**

The project was found to be statutorily exempt according to the California Environmental Quality Act, Article 18 §15262. The project is classified as "Feasibility and Planning Studies" because the project is a planning level document that merely provides general non-binding guidance for future action. No significant impacts are anticipated and no mitigation measures have been required.

### **PUBLIC HEARING NOTICE**

Legal Notice for the Transit Oriented Development Design Guidelines was published on July 30, 2008.

### **ALTERNATIVE PLANNING COMMISSION ACTIONS**

Accept the Report  
Accept the Report, with Comments  
Request Additional Information

Respectfully Submitted,

Concur,

Immanuel Bereket  
Junior Planner

Peter Pirnejad  
Co-Interim Community Development Director

Attachments:           1. Project Schedule and Public Participation  
                              2. Transit Oriented Development Design Guidelines

# Transit-Oriented Development Design Guideline for Downtown Lodi, California

	PHASE I: Inventory & Analysis of Existing Conditions	PHASE II: Vision, Assets, Opportunities & Challenges	PHASE III: Plan Elements & Design Standards	PHASE IV: Design Guideline Documentation & Adoption
PROJECT MANAGEMENT	 Staff Meeting #1: Project Kick-off  Steering Committee Meeting #1 & Walking Tour	 Staff Conference Call #1: Review Emerging Vision & Design Principles  Steering Committee Meeting #2: Review & Refine Vision & Design Principles/Identify Development Opportunities (Conducted by Staff)	 Staff Meeting # 2: MIG and Staff Charrette  Staff Meeting #3: Review Preliminary Design Concepts & Standards  Steering Committee Meeting #3: Review & Refine Design Concepts & Standards	 Staff Conference Call #2: Review Admin Draft
PUBLIC PROCESS		 Community Workshop: Define a Shared Vision		 Shirt Sleeve Meeting  Planning Commission Meeting  City Council Hearing
PLANNING & URBAN DESIGN	 Site Analysis & Data Review: <ul style="list-style-type: none"> <li>• Pedestrian Environment</li> <li>• Land Use &amp; Development</li> <li>• Historic Character</li> <li>• Transportation &amp; Circulation</li> <li>• Natural Resources &amp; Infrastructure</li> </ul> (Produced by Staff and MIG)  Economic Overview	 Design Principles & Development Opportunities	 Preliminary Design Concepts & Standards  Refined Design Concepts & Standards <div style="border: 1px solid black; padding: 5px; text-align: center;">             Pedestrian Environment              Land Use, Development and Site Feasibility              Historic Character              Transportation &amp; Circulation              Natural Resources &amp; Infrastructure           </div>	
PRODUCTS	 Existing Conditions Map Book  Walking Tour Summary Memo	 Workshop Summary Memo (Produced by Staff)  Vision Statement, Design Principles & Development Opportunities Memo	 Visualization Tools: <ul style="list-style-type: none"> <li>• Renderings</li> <li>• Site Plans</li> <li>• Photo Simulations</li> <li>• Sections &amp; Elevations</li> </ul>	 Admin Draft Design Guideline Package  Draft Design Guideline Package  Final Design Guideline Package
	MONTH 1 - 2	MONTH 3 - 4	MONTH 5 - 7	MONTH 8 - 9

-----09/10/07-----10/10/07-----11/13/07----11/30/07-----12/13/07----12/17/07----02/26/08-----03/13/08-----04/08--08/08--08/08--09/08--

# Downtown Lodi

## Transit-Oriented Development Design Guidelines:

### A Tool to Stimulate Downtown Development



CITY OF LODI | DRAFT

Prepared by



In Association with  
Economic & Planning Systems, Inc.



**A VISION FOR LODI TOD**

---

Downtown Lodi Transit-Oriented Development Design Guidelines:  
A Tool to Stimulate Downtown Development

**CITY OF LODI | DRAFT**

PREPARED BY



In Association with  
Economic & Planning Systems, Inc.

# acknowledgements

---

## City of Lodi

Peter Pirnejad, Community Development  
Rick Caguiat, Community Development  
Wally Sandelin, Public Works  
Tiffani Fink, Public Works  
Rebecca Areida, Public Works

## Community Workshop

Eileen St. Yves, Lodi Improvement Committee  
Bob Takeuchi, Lodi Improvement Committee  
Linda Martin, Downtown Lodi Business Partnership  
Chuck Easterling, Steering Committee/Business Owner  
Pat Patrick, Lodi Chamber of Commerce  
Mary and Joseph Mann, Sierra Adventure Outfitters  
Curt Daniger, Daniger Furniture  
Sheila Zwiefel, Hertz Realty Inc.  
Mary Wallace, Frames & Fine Things  
Lynn M. Smith, World of Wonders Science Museum  
Tillie Easterling, Business Owner  
Tom Sisneros, Brodie Jaynes Photography  
Keith Land, F & M Bank  
Roger Stafford, SPARC Committee  
Matt Dobbins, Mokelumne Land and Development Co.  
Ross Farrow, Lodi News Sentinel  
William Maxwell, Steering Committee/Property Owner  
Dale N. Gillespie, Steering Committee/Developer  
Mike Swearingen, Steering Committee/SJCOG  
Tracy Williams, Steering Committee/LOEL Center  
Michael Scanlan  
Virginia Snyder  
Jack Grunsley  
Joe Petersen  
Lana Carouba

## Steering Committee

Doug Kuehne, Planning Commissioner  
Jamie Watts, Downtown Lodi Business Partnership  
Staci Bennet, Downtown Lodi Business Partnership  
Chuck Easterling, Downtown Business/Property Owner  
Brian Schmidt, San Joaquin Regional Rail Commission  
Mike Swearingen, San Joaquin Council of Governments  
Tracy Williams, LOEL Center  
Dale Gillespie, Developer/Chamber of Commerce  
Jon Ibarra, Disabled Citizen Representative  
Roger Kahn, Realtor/Pakistani Representative  
Phil Pennino, Former City Council Member/Consultant  
William Maxwell, Property Owner in Project Area

## MIG, Inc.

Chris Beynon, Principal-in-Charge  
Anchi Mei, Project Manager/Urban Designer  
Julia Abbassi, Design Associate

## Economic & Planning Systems, Inc.

Darin Smith, Principal

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# Preamble

The Downtown Lodi Transit-Oriented Development Design Guidelines (the “Plan”) is the result of a year-long planning collaboration between residents, developers, merchants, community members, and the City of Lodi. Developed through an open and comprehensive community planning process, the Plan illustrates various design recommendations for new development and public improvements around the Lodi Multi-Modal Transit Station. These initiatives set the foundation for creating significant transit-oriented development (TOD) in Lodi in the coming years.

TOD is defined as compact, higher-density mixed-use development in a highly pedestrian-oriented environment that encourages people to walk, ride transit, and bike, in addition to driving cars. Many cities around the country are embracing TOD as a positive model for urban growth as gas prices rise, climate change issues become pressing and people seek alternative living options located closer to a strong hub of social and commercial activities. With a regional rail stop and the entire City’s bus services departing from the Multi-Modal Transit Station, Downtown Lodi is in an excellent position to leverage transit and maximize TOD opportunities in conjunction with an already charming city center.

Context-sensitive approaches that respond to local conditions and residents’ choices are critical to good urban planning. This Plan reflects the contributions of community members, developers, landowners and other stakeholders who worked with the City’s Planning Department and the consultant team to share their knowledge and desires for Lodi. From this basis, a Vision for TOD in Lodi (opposite page) was developed to guide the Plan and its elements. In addition, conceptual designs and recommendations were crafted to respond to the assets, issues and opportunities particular to Lodi, as well as to reflect the Vision.

## A VISION FOR TOD IN LODI

*Downtown Lodi is a bridge to the City's past as well as its future. The brick and marble historic building facades remind residents of their unique heritage, while new mixed-use housing and commercial developments near the Multi-Modal Transit Station reflect the City's thriving Downtown.*

*Commuters working in Stockton and Sacramento step off the train and easily walk home. Residents throughout the City hop on a number of buses that conveniently take them to a myriad of shops, restaurants and offices located Downtown. Visitors of the local wine tour come back to their charming hotel and continue the fun with a night out in Downtown Lodi and its various entertainment venues.*

*There is a variety of people of all ages walking on the lush, tree-lined sidewalks, meeting each other in the artful plazas and relaxing in peaceful parks. With its beautifully restored buildings and exciting, new projects, Downtown is a place where quality development gets done easily. This healthy mix of uses and activities – catalyzed by the Multi-Modal Transit Station – makes Downtown an economically-sustainable and healthy place, and a source of community pride.*

This Plan is a visionary framework that suggests how the area might grow over the long term. It is a living document that will change and evolve as the City considers future market conditions and fosters further community dialogue and collaboration. As the Plan is meant to guide both public and private efforts, the following two overarching intentions clarify the purpose of this document.

*Assist the City in Acquiring TOD Funding*

This Plan provides a vision for public improvements and private developments that need financial resources in order to implement them. By creating a design framework for the project area, the City will be able to better leverage existing resources and as well as seek additional funds to: 1) create the best public environment near the Multi-Modal Transit Station and, 2) help facilitate the development of new, high-quality projects.

*Inspire and Support New Development, Not Inhibit*

With regard to all the recommendations in the Plan, changes in existing land uses will be pursued with full communication and cooperation with willing property owners. The development guidelines described in Chapter Three are meant to guide and provide recommendations for, rather than dictate, the design of new developments. This Plan is intended to be a tool to assist developers, not a burden.

1

introduction





# INTRODUCTION



- in this chapter:
- Project Area Context
- Planning Context
- Project Process
- Document Overview

**IN THE FALL OF 2007**, residents, developers, merchants, community members, and planners worked together in a community planning process to create design guidelines for transit-oriented development (TOD) around the Lodi Multi-Modal Transit Station. The goal of this project was to provide design direction for building and streetscape improvements along Sacramento Street adjacent to the Multi-Modal Transit Station, as well as to enhance uses in the Downtown district to make it more transit- and pedestrian-friendly. TOD focuses on the intersection of transportation and land use, a crucial connection in the transformation of existing areas into sustainable communities. TOD addresses environmental concerns by promoting alternate transportation modes, concentrating development in urbanized areas, discouraging greenfield development, and supporting healthy communities.

The broad Downtown vision and specific design direction outlined in this document **establish a framework for guiding new and infill development**, as well as public improvements, such as streets and open spaces. This approach will result in a Downtown environment that is attractive and vibrant and leverages transit for development and economic benefit.

## PROJECT AREA CONTEXT

Located in the San Joaquin Valley between Stockton (six miles to the south), and Sacramento (35 miles to the north), Lodi is adjacent to U.S. Highway 99 and within five miles of Interstate 5 (see Figure 1.1). Lodi is characterized by an arid climate with dry, hot summers and temperate, wet winters.

The Southern Pacific Railroad and the Amtrak San Joaquin routes run along tracks through the center of the project area. All of the City's bus lines as well as regional buses run through the neighborhood and connect at the Lodi Multi-Modal Transit Station. In the future,

commuter rail to Sacramento and Stockton may be extended, further connecting Lodi with significant regional employment centers.

Lodi has over 65,000 residents (according to the 2005 census) and is contained in an area of 12 square miles.

Downtown Lodi is located in the northeastern corner of Lodi near Highway 99 and Interstate 5. Figure 1.2 shows the project area and the ¼-mile radius around the Multi-Modal Transit Station. It has been shown that people living within the ¼-mile radius of a transit station are more likely to take transit. Therefore,

the ¼-mile radius extending from the Multi-Modal Transit Station and parking structure is a crucial component of examining and planning for TOD. The project area is more focused to extract significant scenarios and recommendations that are able to be implemented and that can serve as a template to be used throughout the Downtown. It extends from School Street on the west to Main Street on the east, and Lockeford Street on the north to Lodi Ave on the south. Various residential neighborhoods adjacent to the project area form an integral part to the area's overall identity and character.

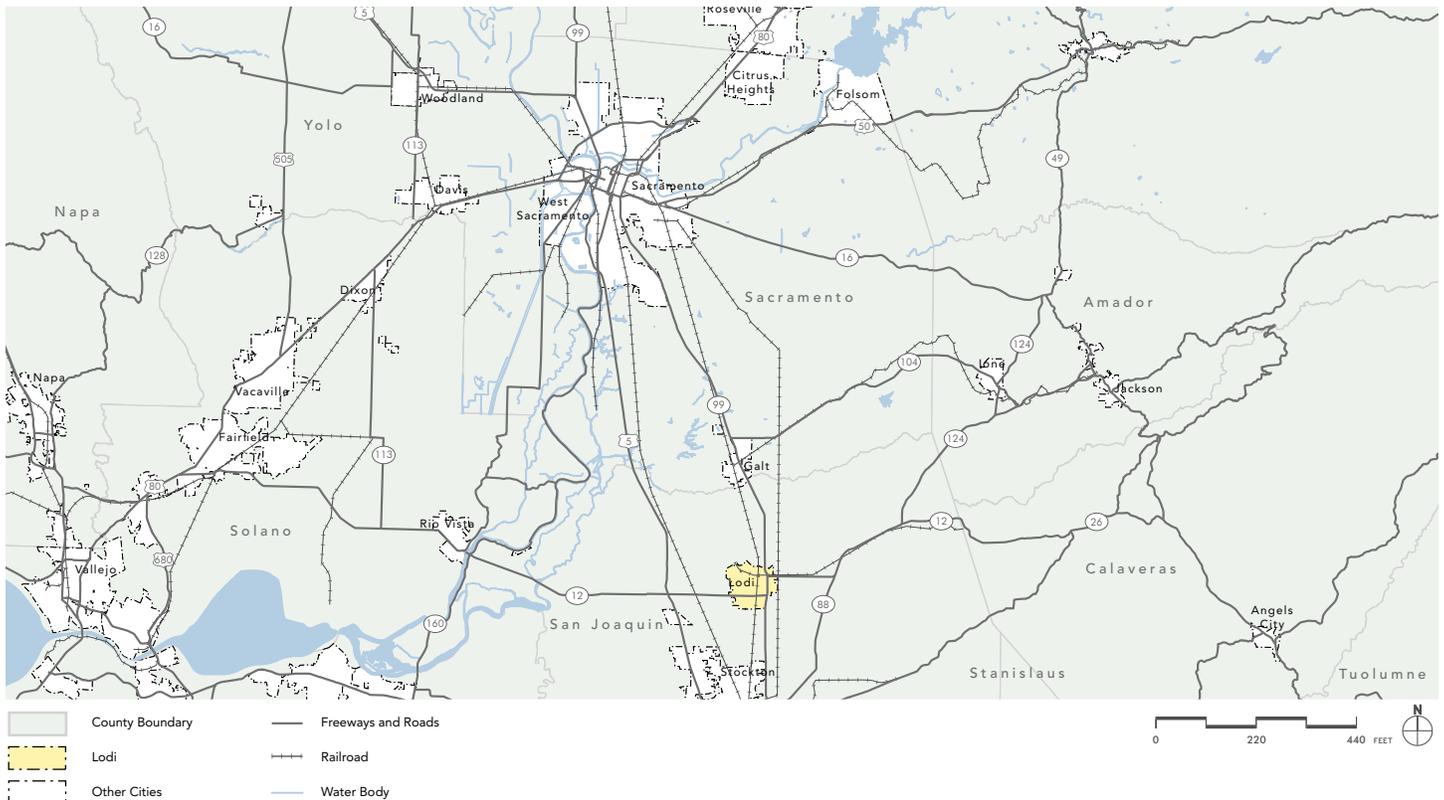


Figure 1.1 Regional Context

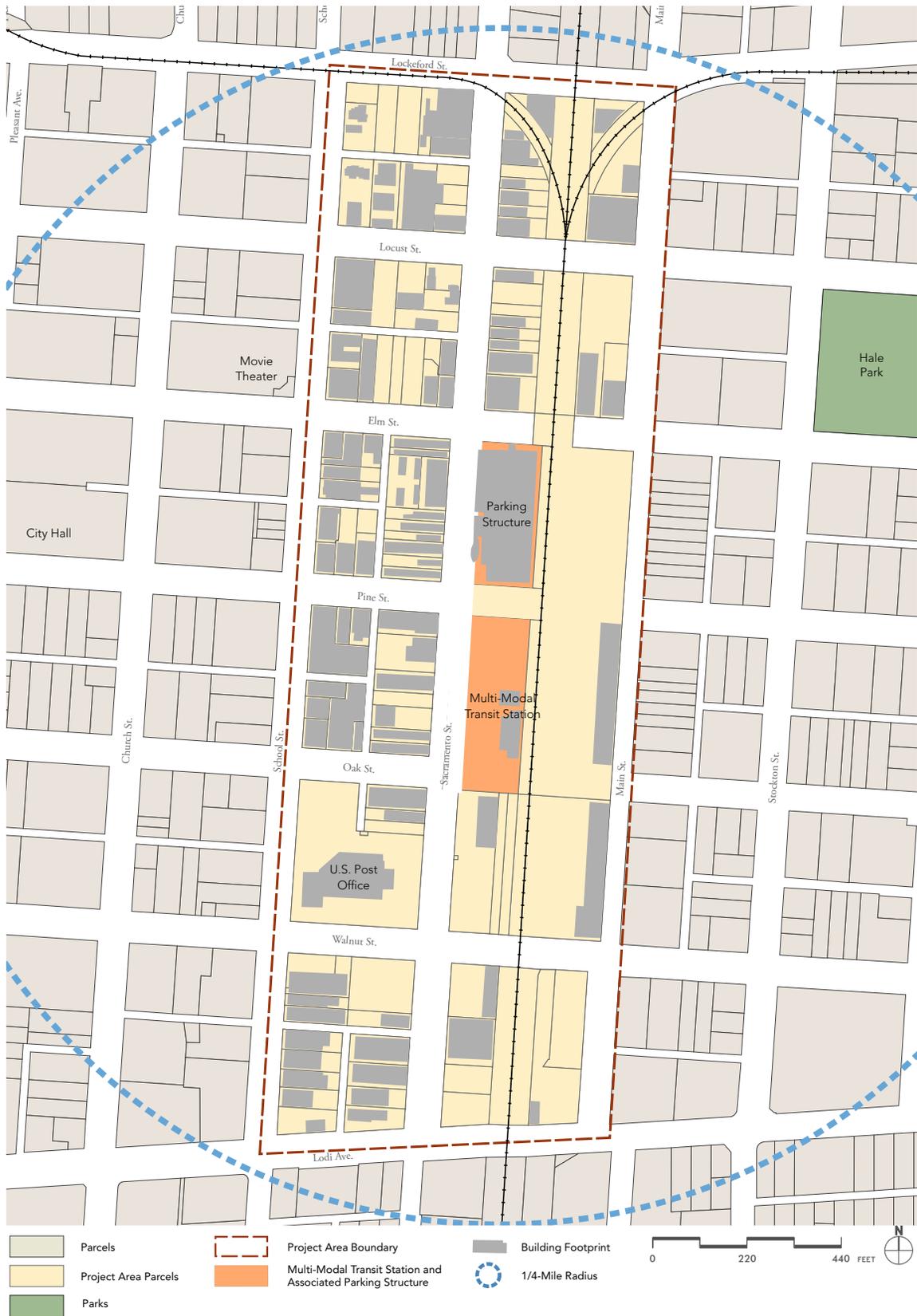
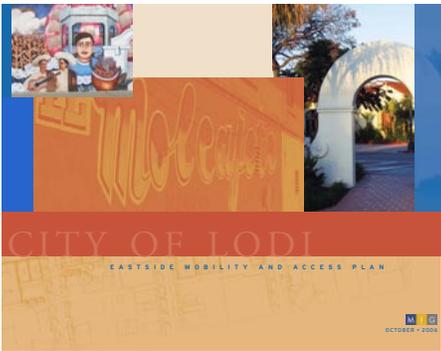


Figure 1.2 Project Area

## PLANNING CONTEXT



*East Lodi Avenue Design Guidelines*



*Eastside Mobility and Access Plan*

The Lodi TOD Design Guidelines builds on other planning efforts in Lodi to improve mobility, offer alternatives to driving and improve the pedestrian realm for residents. The East Lodi Avenue Design Guidelines and Eastside Mobility and Access Plan projects are two recent efforts that have achieved such goals.

This project also responds to community desires for making Downtown Lodi a more livable place through increased housing options and improved connections to adjacent neighborhoods.

In addition, the project supports the following City goals relating to planning for TOD:

- Identify governmental and economic constraints, and possible solutions;
- Compliment the character of Downtown;
- Create and study four template sites;
- Provide economic analysis for the sites;

- Incorporate public comment and recommendations; and
- Orient guidelines to leverage Proposition 1C Infill Incentive and Measure K Smart Growth funds.

On a broader level, the Lodi TOD Design Guidelines are part of a larger national movement of progressive urban planning practice oriented towards compact urban design solutions that are healthier for people and the environment.

This plan anticipates a current shift in population demographics that fuels support for transit-oriented development. Young single adults, childless couples, “empty nesters” wanting smaller homes, and immigrants who are used to taking transit are emerging as new markets for transit-based housing. Additionally, people are also making the choice to live near transit to reduce their commutes to work, home, and shopping.

Transit-oriented development also has lower public infrastructure costs than dispersed suburban development, by reducing the amount of roads and concentrating facilities such as parking, schools, sewer and water lines, and fire stations.

Taking all these factors into consideration, the Lodi TOD Design Guidelines reflect Lodi’s forethought and commitment to meet the social, environmental and economic challenges of our future with creative, thoughtful action now.



*Mixed-use building with lofts and condominiums above ground floor retail*

## PROJECT PROCESS

The City of Lodi Planning Department initiated the project in the summer of 2006. The Department hired urban planning and design consultants MIG, along with financial consultants Economic & Planning Systems, to lead a community visioning process and create design guidelines for the project area. The planning process involved a year of dedication by City staff, consultants and community members. Throughout the process, the planning team frequently met with a Steering Committee of local developers, residents, business owners and community leaders, as well as regional representatives from the San Joaquin Council of Governments and the Regional Rail Commission to give critical direction and feedback on the design guidelines.

The project team held a public workshop on November 12, 2007 at which approximately 30 community members convened to focus on an existing conditions analysis of and provide feedback and direction for their vision of Downtown Lodi in the future. Community members' perceptions aligned with the initial analysis, and participants contributed additional thoughts on the project area's assets, issues and opportunities. Large and small group discussions were conducted to hear ideas on community preferences for future transit-oriented improvements. The results of the community dialogue and comments created the foundation for a robust, multi-faceted vision and community design principles, as further elaborated in Chapter Three.



Community members shared their visions for Downtown Lodi at the community workshop in November of 2007



Community members drew on maps and engaged with City staff and consultants at the community workshop

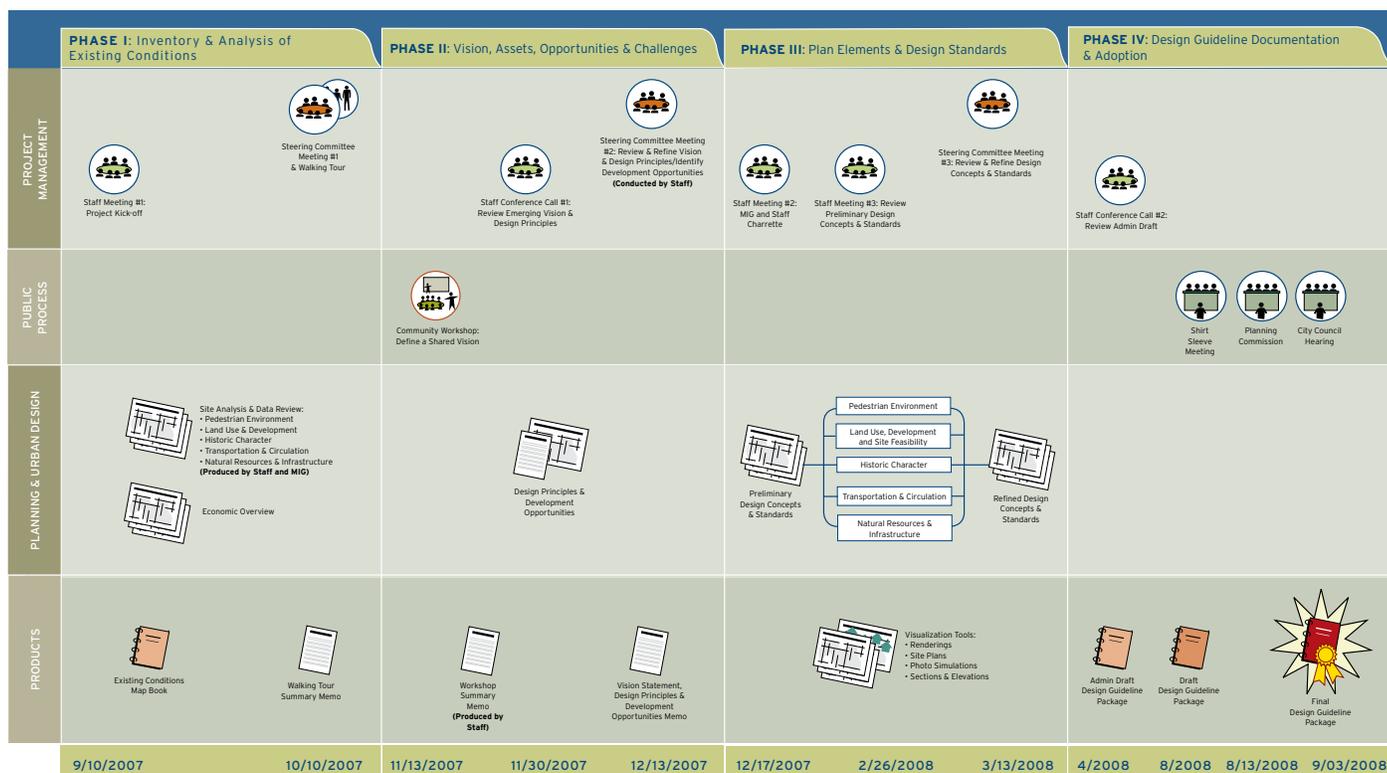


Figure 1.3 Process Graphic

## DOCUMENT OVERVIEW

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The remainder of the Lodi TOD Design Guidelines consists of the following chapters:

### **Chapter Two: Existing Conditions**

Synthesizes existing physical, infrastructure and economic conditions, reviews the area's local, regional and historical context, and identifies the key assets, challenges and opportunities faced in the area's revitalization process. An economic overview of transit-oriented development and the feasibility of TOD in Lodi is also included in this section.

### **Chapter Three: Strategy for Lodi TOD**

Chapter Three presents the vision, design principles and recommendations for shaping a TOD supportive environment. This chapter includes the Vision Statement, Design Principles, Land Use Recommendations, Downtown Lodi TOD Strategy Diagram, streetscape and new

development design guidelines. Four Development Opportunity Projects that epitomize the design guidelines and represent projects that could be prototypes to be used throughout Downtown are also presented. Supporting Strategies are also included, focusing on Transportation Recommendations, Infrastructure Recommendations and Accessibility and Security Strategies.

### **Chapter Four: Implementation**

The implementation chapter outlines strategies to best move the project forward. Recommendations include a list of high priority public improvements, a new development guidelines checklist and funding mechanisms for financing social and public uses in new development. Priority action steps and financing strategies are also covered.





# EXISTING CONDITIONS

# 2

in this chapter:  
Assets  
Issues and Opportunities

**THIS CHAPTER REVIEWS ASSETS OF DOWNTOWN LODI** as they affect the project area, and highlights particular constraints and opportunities that can be addressed to support transit-oriented development. An overview of the economics of transit-oriented development in general and in specific relation to Downtown Lodi is also included in this chapter.

The project area, which covers a block to the east and west of the rail tracks and ¼-mile to the north and south of the Multi-Modal Transit Station, has many strengths, as well as a number of challenges to be addressed and improved upon. Planning efforts must recognize the project area's close link to Downtown Lodi given its location at the eastern edge of the Downtown. Recommendations and strategies should **build upon existing assets, work to counter challenges, and maximize opportunities.**

## ASSETS

*Downtown Lodi and the surrounding area offer a number of strong and unique assets. Leveraging these positive attributes will support a vibrant and successful Downtown and new transit-oriented development.*

### Circulation

Downtown Lodi is laid out on a nearly north-south grid that provides significant vehicular and pedestrian connectivity. The grid is reinforced by a strong alley network, a resource that allows for internal block circulation, loading and service area access and parking reserves.

Local and collector streets run through the Downtown and project area. As such,

the larger traffic volumes that can be deleterious to the pedestrian environment are relegated to the arterials that run outside of the Downtown and project area (see Figure 2.1).

The Downtown is also a multi-modal transit hub for buses and trains. The Multi-Modal Transit Station at Pine and Sacramento streets serves as a conver-

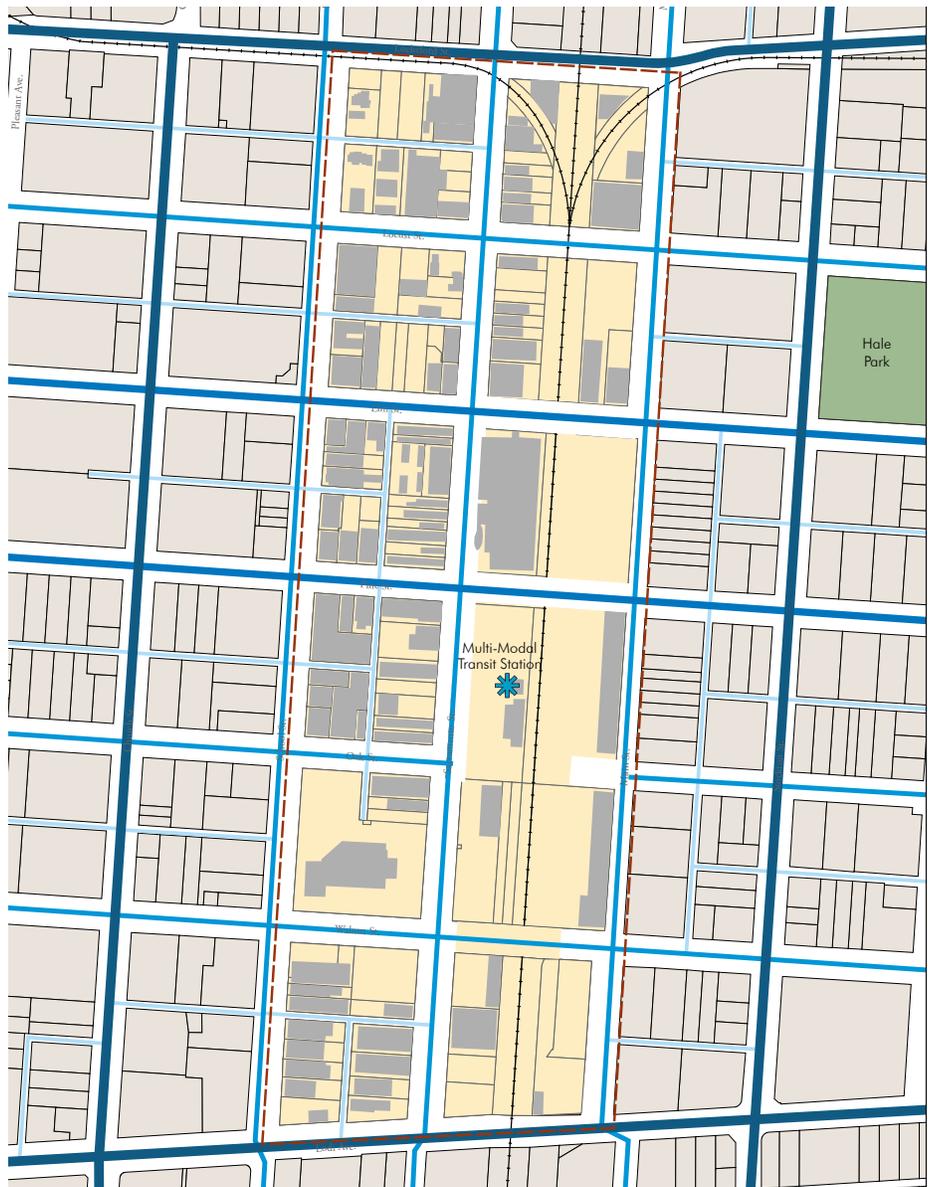


Figure 2.1 Circulation System

gence point for four types of services, including local-serving lines, express routes to Stockton and Sacramento, and links to other transit, such as San Joaquin KDT in Stockton, and SCT/Link in Sacramento.

The Amtrak San Joaquin line stops at the transit station, connecting with Sacramento to the north and the greater Central Valley to the south (see Figure 2.2).

Further, Lodi has the potential to capitalize on its location. A significant Downtown residential population in close proximity to the transit station could utilize the transit options that connect with the larger employment regions in Sacramento and Stockton. A commuter rail connection to Sacramento would greatly support the vitality of Downtown.



Buses at the transit center



A commuter rail line, similar to the Capital Corridor above, would benefit Downtown Lodi



Figure 2.2 Transit System

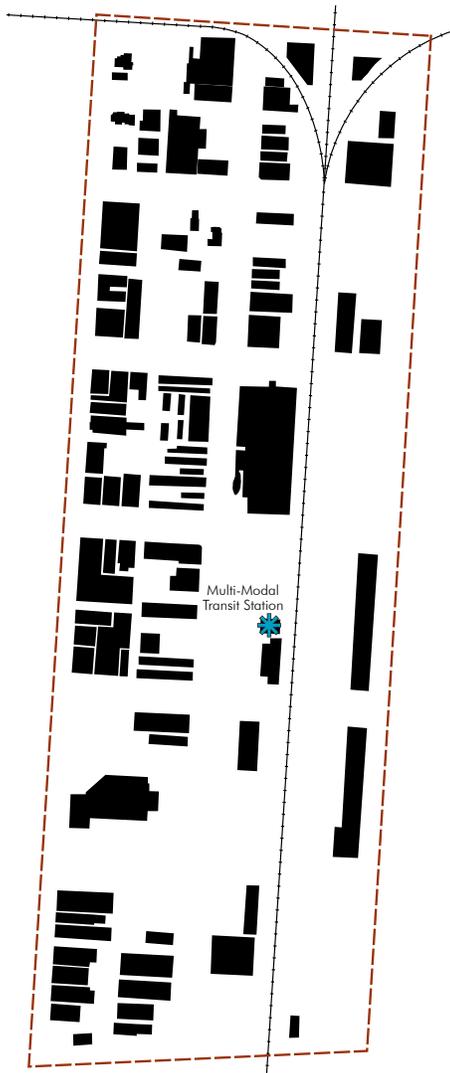


Figure 2.4 Building Figure Ground

### Robust Downtown

Lodi's Downtown is well-established and full of character. The Downtown is organized primarily along School Street between Locust Street to the north and Lodi Avenue to the south. It has many small-scale retail offerings and a recently constructed streetscape design. The buildings and storefronts are fine-grained with a variety of small-scale ground floor uses. The variety of buildings and uses adds interest and character to the Downtown, and serves as a destination for residents of Lodi and tourists.

The building figure ground map for the project area (see Figure 2.4) displays the footprints of all buildings in the project

area in black. The fine-grained building pattern and strong building edge along School Street and many of the cross-streets, such as Pine and Elm streets, can be seen. The break-down in the building edge and the larger footprint buildings that characterize the blocks east of Sacramento Street can also be seen in the figure.

Building entries can serve as a rough measure of activation as entrances to residential and commercial uses off of sidewalks provide opportunities for interaction and spill-over between the public and private realms. They also provide architectural breaks in long facades and a



Restaurant at Elm and School streets

more interesting edge to the pedestrian environment. Predictably, in Downtown Lodi, the greatest number of building entrances is along School and Pine streets. Sacramento Street between Elm and Pine streets has many building entrances (see Figure 2.3). and as such, they constitute the “bones” to create an activated pe-

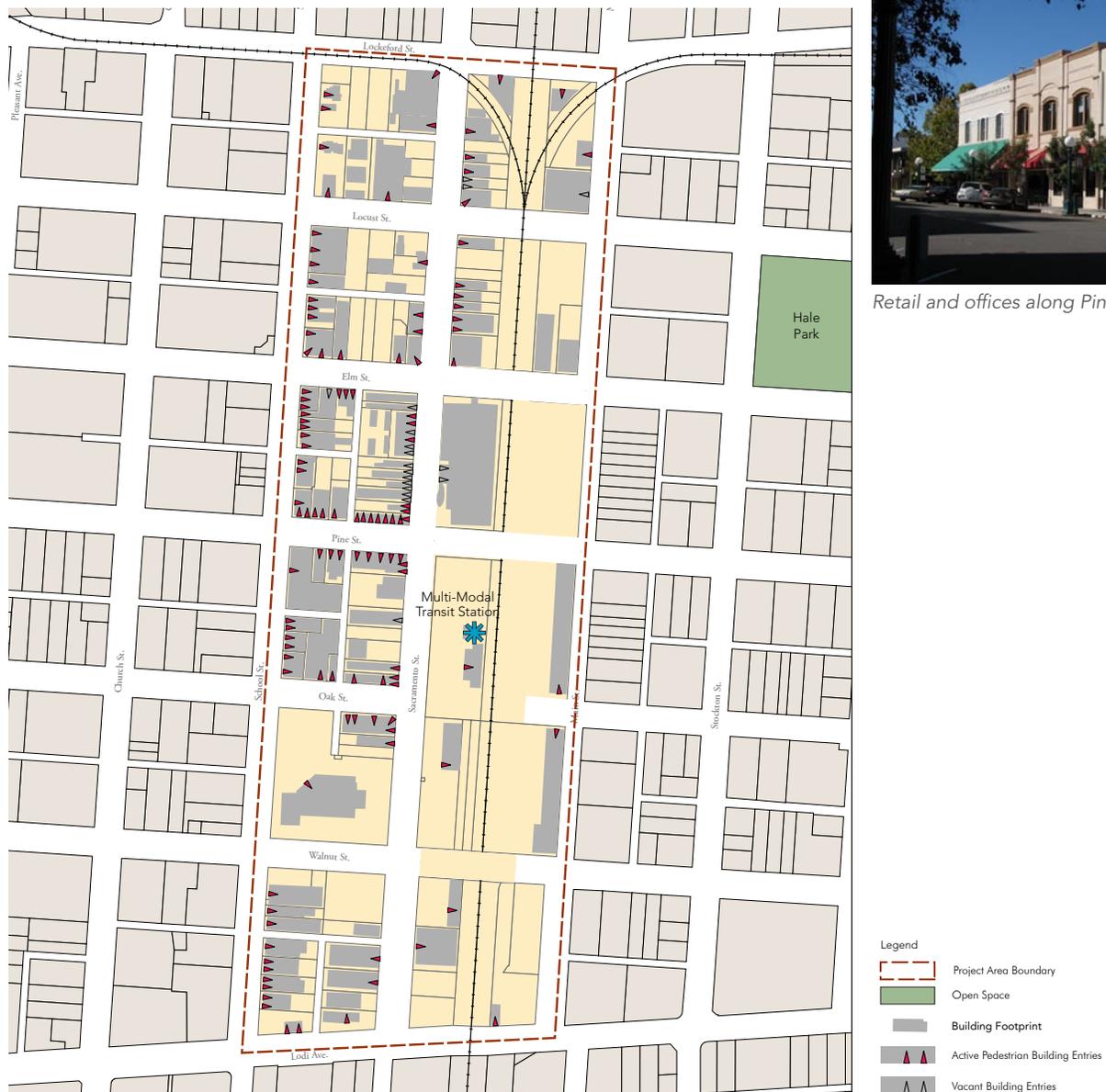
destrian environment along that stretch of Sacramento Street. Several of these buildings have historic design elements, however many are currently vacant or underutilized. These buildings need renovation and new uses, rather than demolition.



*Shops along School Street*



*Retail and offices along Pine Street*



**Figure 2.3 Building Entries**

## Character and Identity

Downtown Lodi has a strong and identifiable character supported by the scale of development, the pedestrian environment, building uses and architectural heritage.

Most buildings are two to three stories, providing a low-rise and human-scaled pattern of development. The buildings also are consistently built to the property line, which results in a strong edge to the pedestrian environment. This supports a sense of street enclosure that is beneficial to a robust public realm. As mentioned above, the buildings are fine-grained and have multiple building entries. The storefronts have a significant amount of

transparency with large windows, supporting visual connections between the public and private realms.

Streetscape improvements have been made along a number of Downtown streets with new paving treatments, bulbouts, street trees, pedestrian-scaled streetlights and amenities such as benches, signage, kiosks and trash cans. The designs for School Street and Elm Street are particularly distinctive. School Street's design includes special paving for the entire roadway, distinctive yellow colored concrete for the wide sidewalks and mature trees in tree wells between parking spaces. Elm Street between School



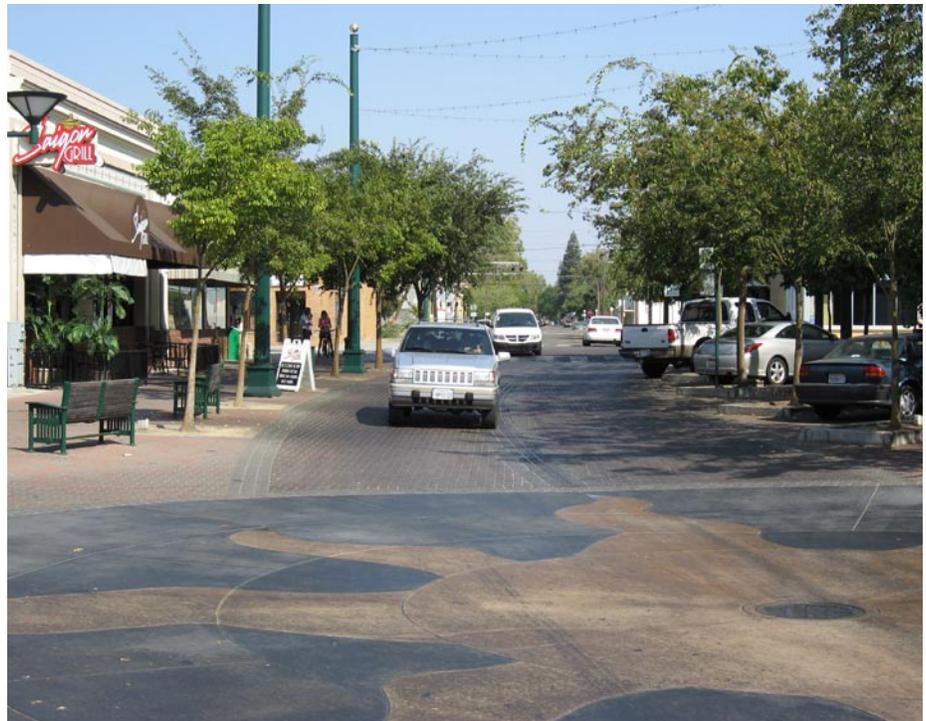
*Buildings to property line*



*Streetscape improvements along School Street*



*Example of a shared-mode roadway*



*Elm Street is shared equally by pedestrians, bicyclists and vehicles in Downtown Lodi*

and Sacramento streets is a “woonerf,” a condition in which the street prioritizes pedestrians and bicyclists over vehicles, and is often designed with additional pedestrian amenities such as special paving, trees and benches. In many cases, a “woonerf” is a flush roadway, as is the case with Elm Street, to further equalize the various transportation modes. The unique and pedestrian-oriented character of School and Elm Streets greatly support an identifiable Downtown environment.

In addition to local landmarks, gateway elements and murals add distinctive elements to the Downtown. The gateway arches at Pine and Sacramento streets

and at School Street and Lodi Avenue are unique elements that celebrate the history of the area. The murals throughout the Downtown have been successful at telling the story of Lodi, as well as mitigating long walls along streets and buildings.



Murals celebrating grape-growing in Lodi



Murals highlighting Lodi's history



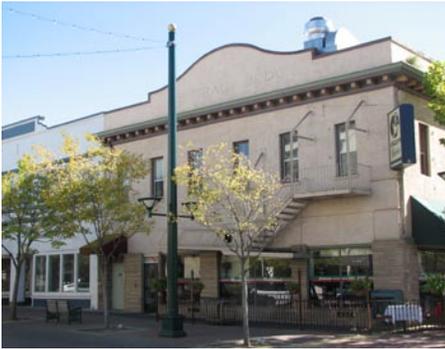
Historic Lodi gateway arch over Pine Street

## Historic Buildings

The historic nature of much of the building stock as well as the architectural style, materials and scale of the historic buildings contribute to the look and feel of Downtown Lodi. Dominant materials of the historic buildings include brick, marble, stone and plaster.

A row of vacant historic buildings along Sacramento Street between Pine and Elm streets provide color, identity and a unique character. Historic buildings also can be found along Main Street in what used to be the Japanese-American neighborhood in Lodi. The opportunity exists to redevelop or rehabilitate these historic facades and buildings to maintain much of the historic character while allowing for new uses.

Other historic buildings along School, Elm and Pine streets have been adaptively reused and converted into salons, restaurants and have a variety of uses at the ground floor. This has resulted in viable commercial spaces in buildings that maintain this historic character and charm of Lodi.



*Historic building on Elm Street*



*Brick and stone are common building materials*



*Vacant, historic buildings along Sacramento Street*

## Mix of Uses

Downtown Lodi has a wide variety of uses that attract different types of users. A diverse set of uses characterizes the Downtown with business hours that support activity throughout the day and destinations that appeal to different demographics.

For the most part, small-scale retail and mom-and-pop stores populate the main stretches of the Downtown. Local serving uses, such as banks and health food stores ensure that Downtown Lodi remains functional for the residents.

Restaurants, cafes and entertainment uses, such as the new movie theater, help activate Downtown in the evenings

and provide destinations for residents and visitors. Civic and institutional uses are also well-represented in Downtown, including City Hall and the Library.

Lodi is also increasingly becoming a tourist destination as its reputation as a wine-producing region grows. A number of businesses within the Downtown are capitalizing on the wine industry and the visitors to the area. Wine-tastings, boutiques and antique stores that cater to tourists have been established along School Street.



Entertainment uses in the Downtown



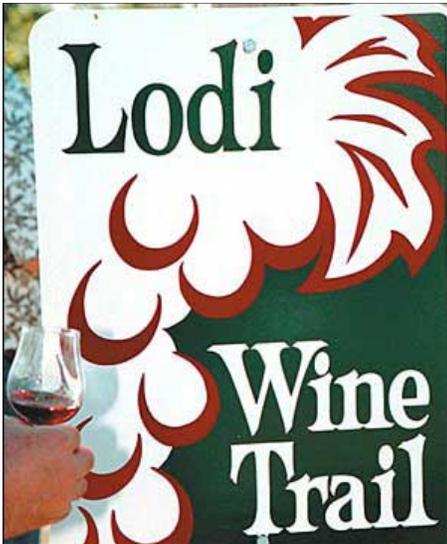
Wine tastings in Downtown Lodi



Outdoor dining



Winery outside Lodi



Signage for the Lodi Wine Trail



Downtown Lodi Business Partnership banners

## Special Events

The City of Lodi has been working diligently on attracting new visitors, residents and uses. Lodi's profusion of special events and attractions for visitors are an opportunity to support better and more transit options. The Farmer's Market is a weekly event from mid-June to October and is located on School Street. The market is on Thursday evenings, which attracts people and activity to the Downtown on a less popular night. At the Lodi School Street Wine Stroll, over 25 local wineries set up in participating businesses along School Street. Visitors are encouraged to stroll through Downtown Lodi, do wine tastings and visit the local businesses.

The Lodi Street Faire is a biannual event often attended by over 30,000 people and 700 vendors. It is often the most profitable days for local merchants and brings many new visitors to Lodi. The Lodi Parade of Lights is sponsored by the Downtown Lodi Business Partnership and is a holiday event with floats, marching bands and dance troupes. The event often attracts up to 50,000 people. Additional events such as Oktoberfest and Winterfest serve to boost recognition of the Downtown as a cultural destination and attract visitors to support local businesses.

## Active and Invested Stakeholders

A key asset in Lodi is the level of commitment and interest in revitalization of the Downtown from elected officials, Downtown merchants and residents. The Chamber of Commerce and Downtown Lodi Business Partnership are two active organizations committed to ongoing support of local businesses in the community. In addition, longtime residents and property owners are open to new ideas such as more transit options and residential, mixed-use and live-work options in Downtown that can support the vitality of their city.

Stakeholders also recognize that Lodi has many opportunities that can be built upon, such as the Multi-Modal Transit Station and the charm and unique identity of Downtown. In addition, Lodi is able to capitalize on trends such as the reurbanization of downtowns and transit-oriented development to support its successful revitalization.

## ISSUES AND OPPORTUNITIES

Downtown Lodi and the project area are also faced with issues and constraints that will need to be overcome or mitigated in order to create a vibrant downtown and transit-oriented development. These issues are simultaneously opportunities that can be addressed and built upon.

### Inconsistent Pedestrian Environment

While some Downtown streets are pedestrian-friendly, others within the project area have an inconsistent pedestrian environment (see Figure 2.4). Sacramento Street is lined with vacant buildings and lots, auto repair shops and parking lots. The result is a predominantly undesirable pedestrian environment with a poor sense of street enclosure, an auto-oriented character and a lack of amenities.

In addition, many buildings along Sacramento Street have long stretches of blank walls and boarded-up windows. Main Street also lacks basic pedestrian amenities such as streetlights, sidewalks and gutters. Chain link fences, vacant lots and vacant buildings compromise the pedestrian edge along Main Street.



Figure 2.4 Pedestrian Unfriendly Edges



Vacant lot on Sacramento Street

### Vacant and Underutilized Land

The project area has a number of gaps in the building fabric, which is an issue for the quality of the pedestrian environment but also serves as an opportunity for infill development. Significant lots along Sacramento, Elm and Main streets are currently vacant or underutilized. Surface parking lots, such as the City-owned lot on Elm Street, could be more efficiently

used to bring people and activity to the Downtown (see Figure 2.5).

Property within the project area that is currently being used for light industry, such as the seasonal cherry packing warehouses, and auto-oriented businesses, such as auto repair and car sales, are not taking full advantage of the proximity to the Multi-Modal Transit Station. A key



Figure 2.5 Vacant/Underutilized Land And Buildings

tenet of transit-oriented development is to locate the highest intensity uses that would benefit from access to transit closest to the station. While auto-oriented uses are a necessary part of a functioning city, they are not utilizing land within a ¼-mile of the transit center to its fullest and can be viewed as opportunity sites for higher intensity uses.

Many of the buildings along Sacramento and Main streets are currently vacant and represent opportunities for adaptive reuse or façade rehabilitation. In particular, Joe Hassan’s Warehouse and the row of historic buildings along Sacramento Street between Elm and Pine streets are significant opportunities to maintain the

character and identity of the area while allowing for new development. In addition, many of the second floors of the historic building throughout the Downtown are vacant and could be reconfigured as office or residential units.

Many of these sites are opportunities to help transform the east side of the project area with higher intensity uses. Uses that support transit should be encouraged, such as market-rate housing for empty-nesters and young professionals, inclusive housing for seniors and economically disadvantaged populations, neighborhood-serving retail, and live-work units.



*Used car dealership near transit center*



*Vacant lot on Main Street*



*Opportunities for adaptive reuse along Main Street*



*Boarded up building east of the tracks*

## Image and Perception

Some parts of the project area stand in contrast with the charm and quaint character of Downtown Lodi. In particular, the buildings and streets along Sacramento and Main Streets needs some extra repair and maintenance. A number of bars, mini-marts and auto-related services located in the north of the project area contribute to a lack of activity on the streets and in the buildings. The north and south ends of the project area have a more industrial character and appear to be underutilized from a land use perspective. In areas that contrast with more pleasant parts of the project area, there is generally a lesser number of residences and a lack of active building uses. This results in low pedestrian activity and the perception of a need for greater safety.

Through reinvestment and creative context-sensitive design that builds off of the existing edgy and industrial character on the eastside of the project area, the opportunity exists to begin to increase residential units in the project area, generate more activity on the streets, add more people in the neighborhood and shape the public realm into a more walkable, vibrant and livable place.



*An SRO on Main Street*

## Need for Community Amenities In the Center of Downtown

While there are many community amenities adjacent to the project area, there is a need for more in the center of Downtown. There are many City services, such as City Hall, the Police Department, the Fire Department and the Library, are located towards the west of the project. (see Figure 2.6). Emerson Park is a few blocks away from Downtown, and Hutchins Street Square is a significant

community facility that hosts theater troupes, dances, community events and is also a senior center. Hale Park is a full-block park on the east side of the project area, complete with picnic tables, benches and a tot-lot. In addition to these amenities, the opportunity exists to increase open space and community facilities within the project area.



Bulbout plaza on School Street



Hale Park east of the tracks

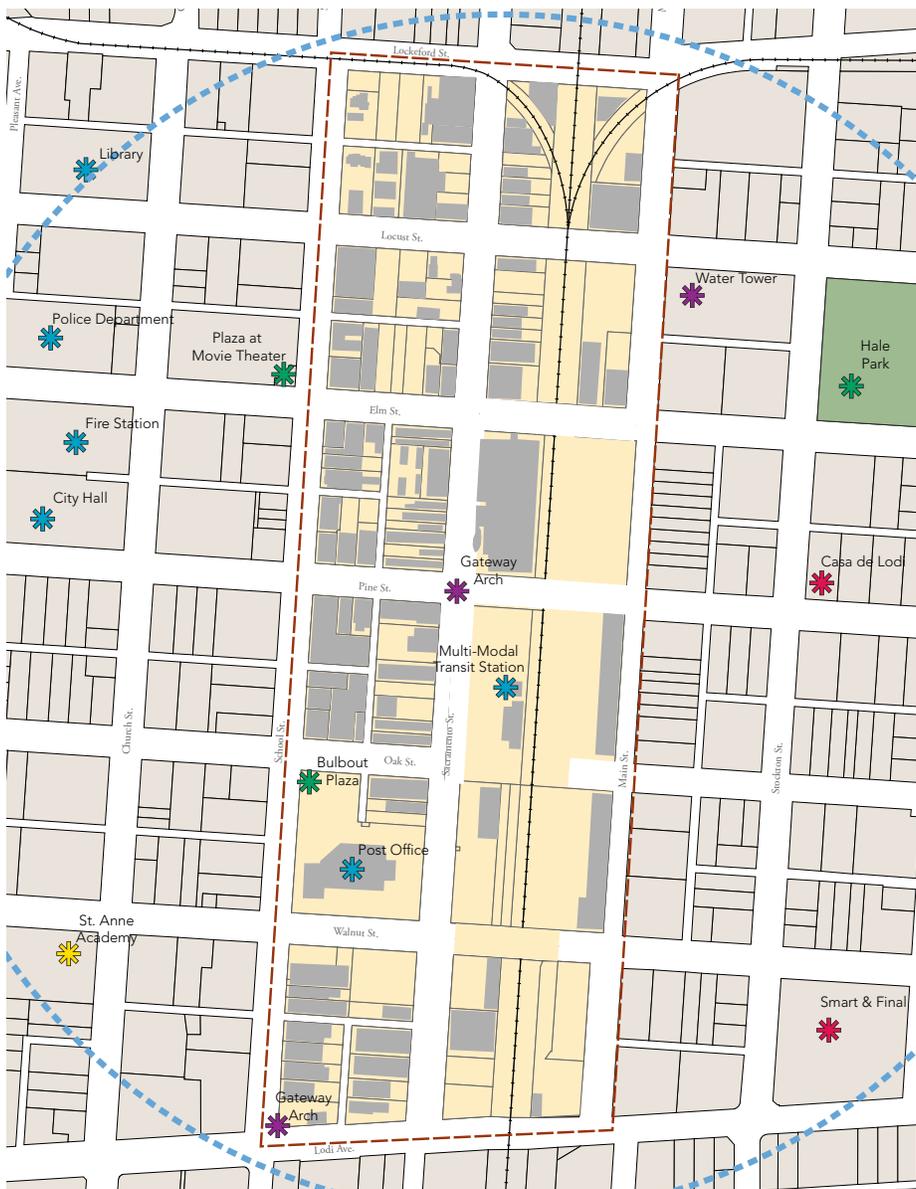


Figure 2.6 Community Amenities

## Current Land Use and Zoning

To encourage change to occur in an area, the correct institutional and regulatory frameworks must be in place.

Within the project area land use designations change at the railroad tracks. To the west is the Downtown Commercial land use designation and to the east is primarily Light and Heavy Industrial (see Figure 2.7). The uses on the ground reflect

this bifurcation of the land use designations, such that the commercial uses are focused along School Street and a few of the cross-streets and more industrial and industrial uses are to the east of the project area.

The zoning designations which implement the land use designations further isolate the project area from the com-

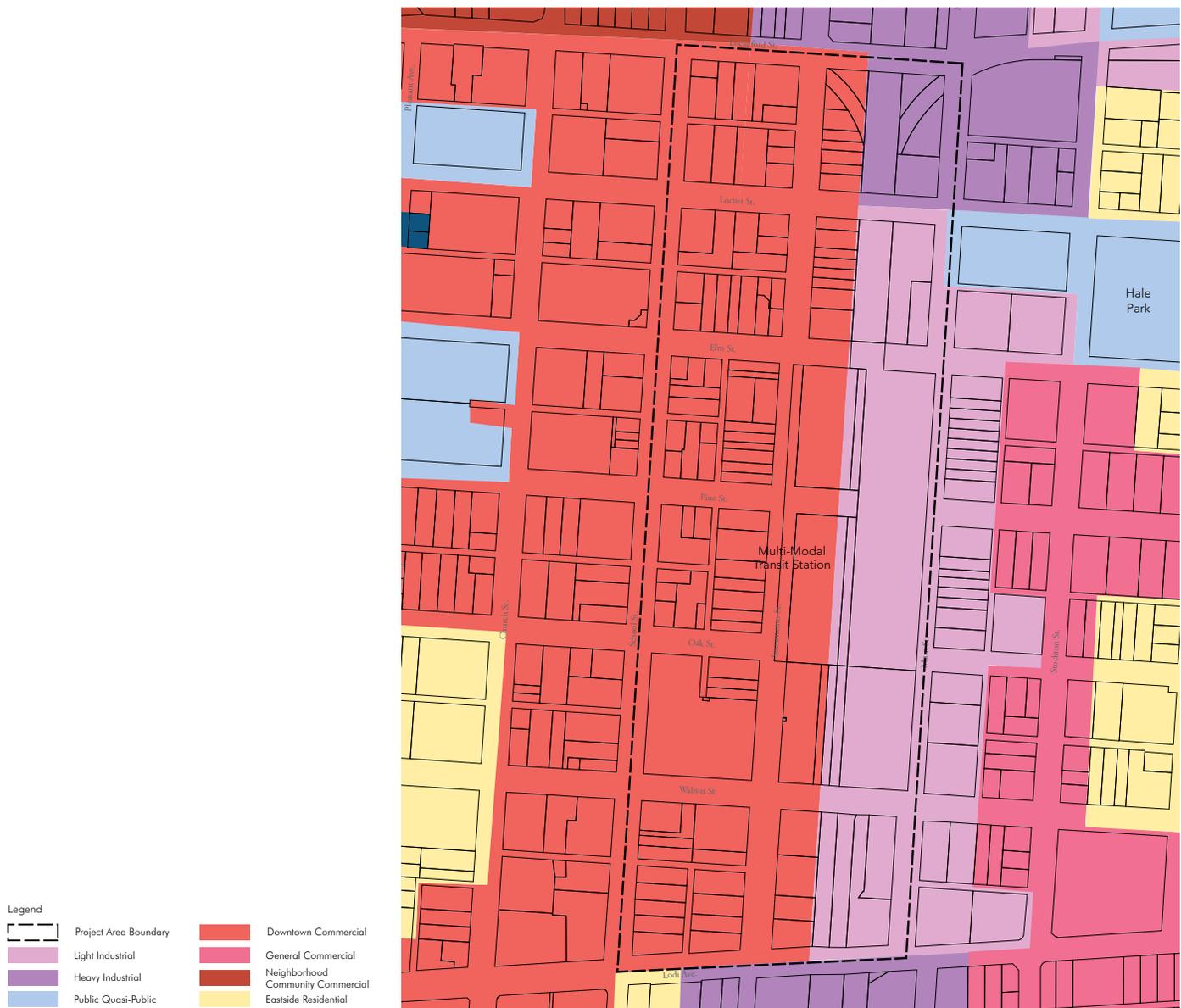


Figure 2.7 General Plan Land Use

mercial-oriented Downtown by zoning the parcels on either side of Sacramento Street as Commercial Light Industrial (C-M) (see Figure 2.8). The parcels on the east side of the tracks are Light Industrial (M-1) and Heavy Industrial (M-2).



Figure 2.8 Zoning

## Connectivity to Adjacent Neighborhoods



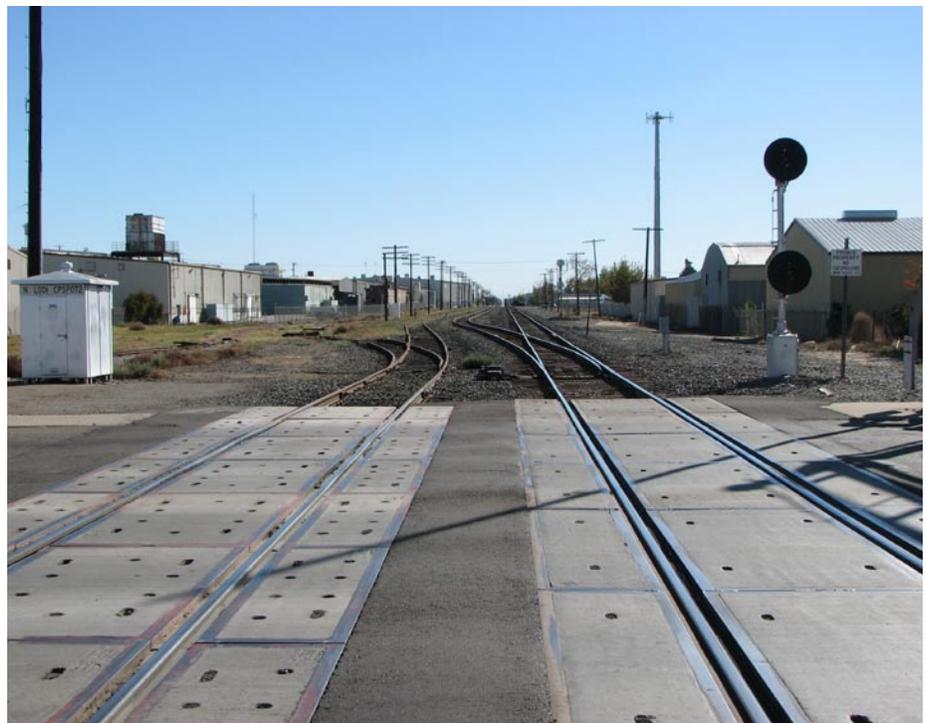
*Blank walls*



*Lack of sidewalks, curb and gutter along Main Street*

While the grid street pattern is an asset for vehicles and pedestrian within Downtown Lodi, the Downtown itself is disconnected from adjacent neighborhoods by the railroad tracks. The tracks are a visual and physical divide between the Downtown and the neighborhoods to the east. Lots along the east-west oriented streets that cross the tracks remain undeveloped since much of the land is in Union Pacific ownership and also due to regulations regarding how close development can be built to the tracks. The result is a stark pedestrian environment along the cross-streets and a visual disruption in the building fabric, contributing to a visual divide between the east and the west sides of the tracks.

The trains that run along the tracks also present a physical barrier as they periodically block vehicular and pedestrian access. Walnut and Oak streets do not connect across the tracks but dead-end on either side further disrupting the physical connectivity. However, it should be noted that Lodi does have far better connectivity across its railroad tracks than many other cities where cross streets have been interrupted for blocks to minimize conflict points between vehicles, pedestrians and trains.

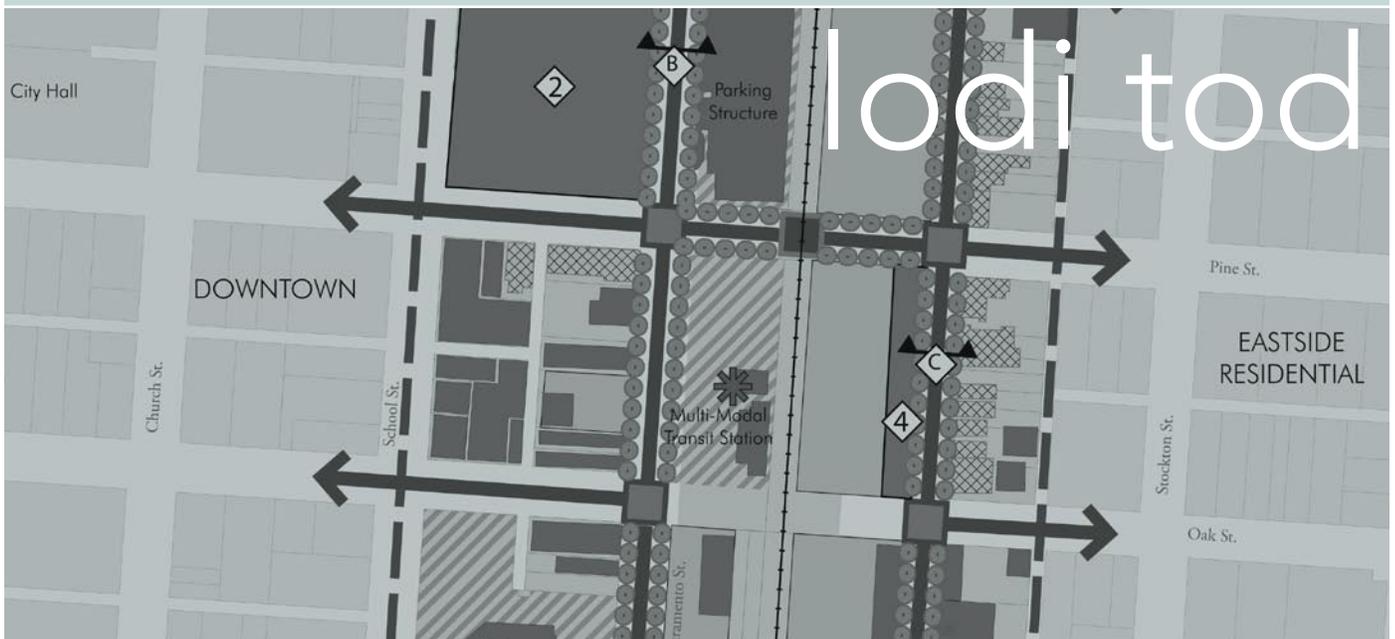


*Railroad tracks bisect the project area*

# 3

## strategy for

## lodi tod





# STRATEGY FOR LODI TOD

# 3

## in this chapter:

Design Principles

Strategy Diagram

Land Use  
Recommendations

Design Guidelines

Development Opportunity  
Sites

**THE STRATEGY FOR TRANSIT-ORIENTED DEVELOPMENT IN LODI** begins with the design principles that resulted from the community workshop and steering committee meetings. The strategy adds specificity and direction to the vision; as such, it is composed of the strategy diagram and specific recommendations for development standards, land use recommendations, design guidelines, and potential designs for development opportunity sites.

The strategy for transit-oriented development is meant to **guide and streamline the implementation of new infill transit-oriented developments** while ensuring a high-quality product that supports the broader goals of the City.

## DESIGN PRINCIPLES

---

On November 13, 2007, approximately 30 community members convened for an interactive community visioning workshop. Participants identified assets, issues and opportunities in Downtown Lodi, discussed the community vision for the Downtown and transit-oriented development, and brainstormed preliminary design strategies to achieve the community vision. This vision is presented on page ii.

The list of design principles below were identified by the community during the planning process. These principles will provide specific direction for future development and work towards achieving the community's vision. As the plan is developed, the community goals will also provide a mechanism to evaluate alternatives and trade-offs.



### Create a Vibrant Pedestrian Environment that is Attractive and Safe

Community members suggest improving the safety and pedestrian environment of the streets in the project area, particularly Sacramento, Elm and Pine streets.



### Leverage the Historic Architecture and Traditional Charm

The Lodi community values the historic architecture and old-town appeal of Downtown Lodi and suggest that future improvements should build on Downtown's historic character and unique identity.



### Incentivize Development and Break Through Development Barriers

Aware of economic realities, community members want the City and its public and private partners to pursue creative funding and marketing strategies for infill development and redevelopment Downtown.



### Cultivate a Mix of Uses and Activities Downtown

Community members desire a variety of housing opportunities and encourage a diverse mix of land uses and activities to sustain a vibrant Downtown economy.



### Create Great Open Spaces

Residents support the creation of additional parks and plaza spaces for recreation, entertainment and relaxation Downtown.



### Provide Multi-Modal Connections and Adequate Parking Access

Lodi residents support transit-oriented developments Downtown that integrate into all modes of transportation – auto, bus, rail, bike and pedestrian – and encourage new development that will increase transit activity while also accommodating the needs of drivers.

## STRATEGY DIAGRAM

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*The strategy diagram is composed of recommended improvements to both the public and private realms. The diagram is the visual blueprint and “roadmap” for future growth and development in Downtown Lodi.*

**Public realm improvements** fall into three categories: streetscape improvements, pedestrian linkages and intersection improvements.

Streetscape improvements along Sacramento and Main streets have been identified by community members and are identified on the strategy diagram. In addition, streetscape improvements across the tracks at Pine, Elm and Locust streets are recommended to better connect Downtown with the neighborhoods east of the project area.

Better pedestrian linkages across the tracks and between the neighborhoods around Downtown are also called out in the strategy diagram. These include the residential neighborhoods to the east and west as well as the commercial corridor along Lodi Avenue, the neighborhoods south of Lodi Avenue, and the more industrial area north of Lockford Street. Intersection improvements are crucial to a safe and well-connected area. In particular, the intersections from Locust to Walnut streets along Sacramento and Main streets should be improved. Intersections of the railroad tracks and the east-west streets of Locust, Elm and Pine streets should be given special attention to support safe pedestrian, bicycle and vehicular crossings. Improvements to the intersections with the railroad tracks also serve to better connect the east and west sides of the tracks.

**Private realm improvements** focus on vacant and underutilized parcels, opportunities for adaptive reuse and catalytic opportunity sites.

The vacant and underutilized parcels have been identified in pink as being the strongest candidates for infill development. They include parking lots, auto-oriented uses, vacant lots and parcels with vacant buildings of no historic significance.

The adaptive reuse opportunities and the historically significant buildings are designated in blue and crosshatch. Such buildings should be targeted for rehabilitation, renovation or at the very least façade restoration. They are of value to the community and add character and identity to the Downtown area.

The four opportunity sites that were chosen for further design study are identified in yellow. The sites designs, detailed in the development opportunity section, show prototypical projects that are representative of the types of development that could occur throughout the Downtown. Streetscape improvements have been identified on Sacramento and Main Street to support new development. Downtown alleys have also been identified as an important urban design asset to be built incorporated and built upon in new developments.

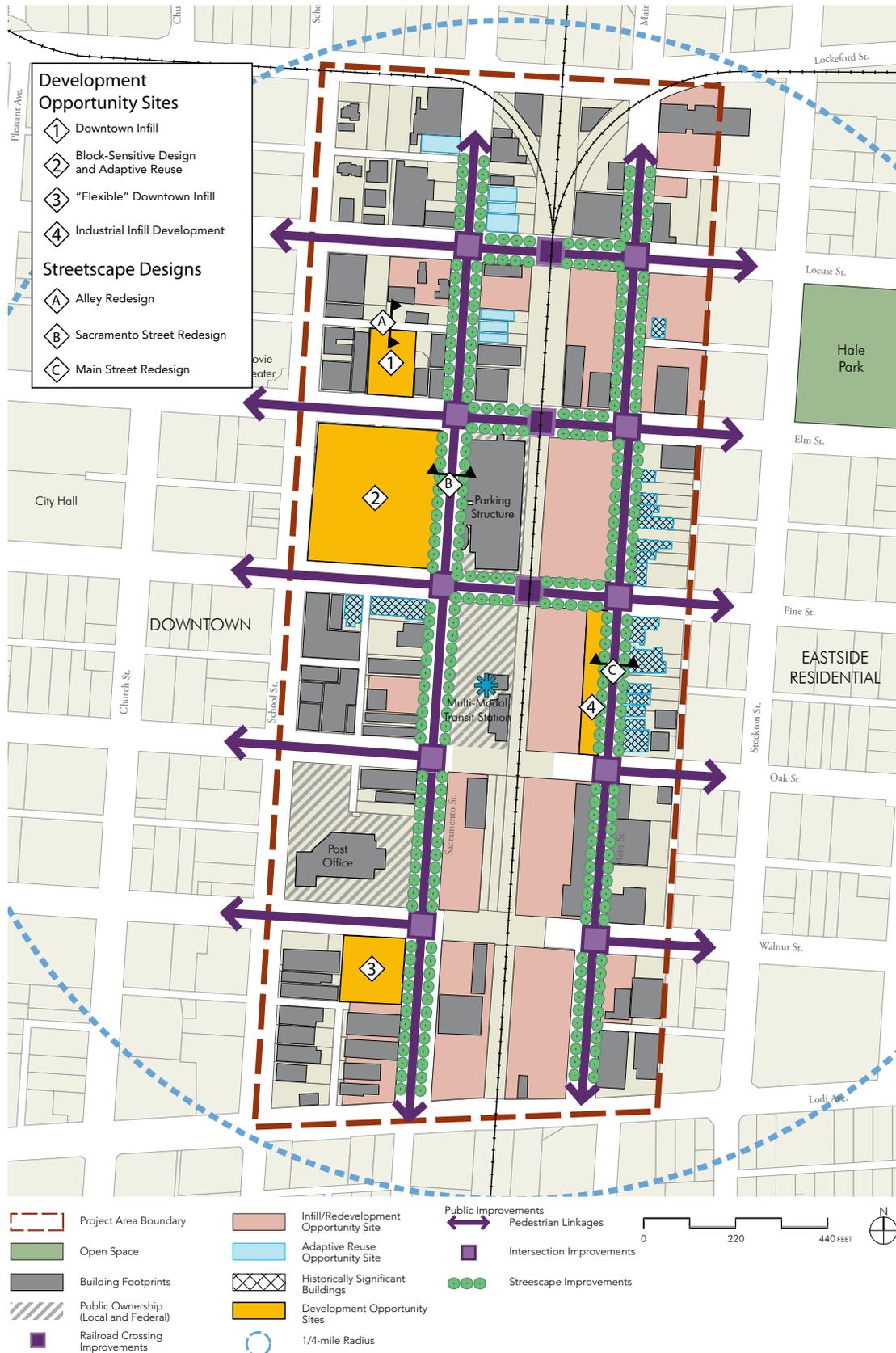


Figure 4.1 Strategy Diagram

## LAND USE RECOMMENDATIONS

As a part of the City's General Plan Update process, changes to the land use and zoning designations for the project area should be made.

As noted in Chapter 2, the existing land use and zoning designations within the project area do not support the goals of capitalizing on transit and revitalizing the Downtown. The parcels within the project area, the 1/4-mile radius from the transit center, should be rezoned to allow for a range of transit-oriented product types and uses. In addition, the parcels within the 1/8-mile radius from the transit center should be zoned to require an even greater level of development intensity to maximize their proximity to transit option.

This plan recommends commercial ground floor uses within an 1/8 of a mile of the Multimodal Transit Station and Parking Structure. Given market demand and existing conditions, residential ground floor uses should be allowed outside of an 1/8 of a mile of the Multimodal Transit Center and Parking Structure.

Zoning standards for the two zoning designations should maintain the current height limit of 65 feet for the Downtown. For the parcels within the 1/8-mile radius of the transit center, four stories are encouraged. For parcels between the 1/8- and 1/4-mile radii, three stories are encouraged.

New infill developments should be built to the building edge of adjacent buildings to maintain the character of Downtown and to create a strong edge to the public realm.

Residential parking standards should be lowered to one space per unit or removed to support new development and respond to the increase in residents using transit as a result of their proximity to the transit center. On-street parking spaces and shared parking facilities should be explored as ways to reduce the parking requirements for office and retail uses.



Low to medium-intensity mixed-use development



Higher-intensity mixed-use development



Ground floor retail



Live-work units



Neighborhood grocery store

Mixed-use development with ground floor retail and upper floors of residential is a hallmark of transit-oriented development and, in Lodi, could help revitalize the Downtown and attract further transit options. Live-work units that combine office, light industrial and residential could also bring jobs and residents to the Downtown. A small, local-serving grocery would be a strong asset for attracting new Downtown residents and reducing their carbon footprint.



Figure 4.2 Proposed TOD Zoning Overlay

## DESIGN GUIDELINES

The following are design guidelines that provide further direction for private and public realm improvements.

They include specific guidelines for building height and massing, ground floor and upper floor design features, architectural style, parking and green design. They also address overall streetscape design, and provide guidelines for the alleys, Sacramento Street and Main Street.

### A. Building Height and Placement

*New development should increase residential density to achieve higher levels of transit ridership and a vibrant, active Downtown. Good building placement is defined by creating continuity between existing and new development and creating a flush or nearly flush building edge.*

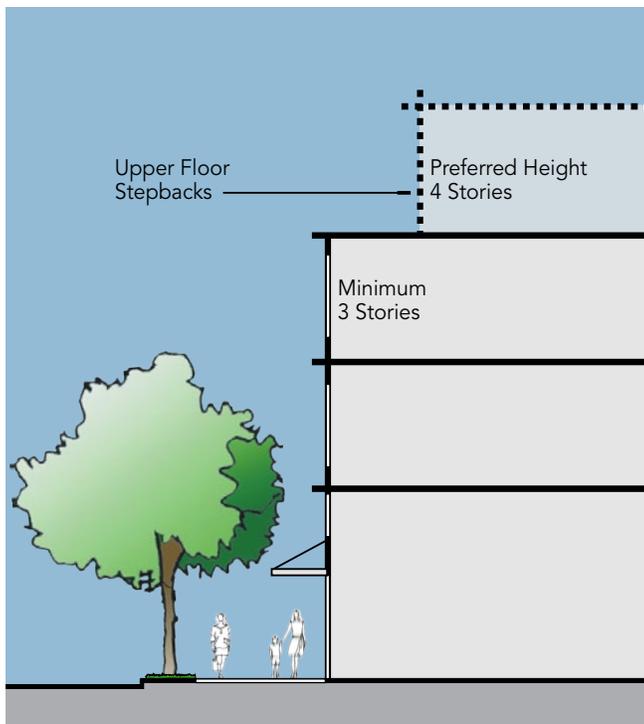
**A1.** New buildings should be a minimum of three stories and encouraged to be four stories within 1/8-mile of the Multi-Modal Transit Station.

**A2.** New buildings should respect the narrow widths of existing development in Downtown (about 40 feet wide). New construction of buildings on lots wider than 40 feet should utilize architectural design elements that underscore the fine-grain character of Downtown through the following techniques:

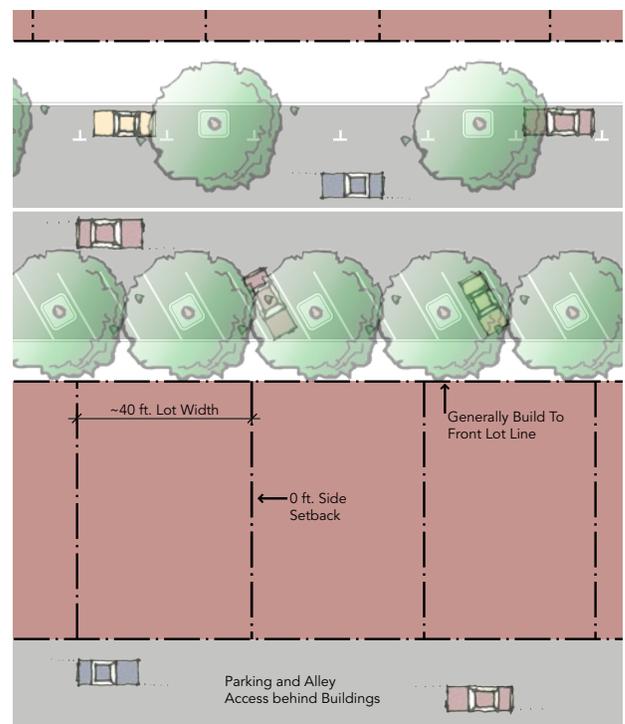
- Incorporate vertical and horizontal architectural elements to mitigate long unbroken building facades,
- Break up large expanses of wall surface by varying building planes and grouping windows.

**A3.** New buildings should have a zero front setback unless adjacent buildings have ground floor setbacks. If a front setback is created, the area should be designed with active or attractive uses (outdoor seating, display of goods, plantings).

**A4.** Allow zero side setbacks between new development and existing development in order to provide a continuous building edge that improves the pedestrian realm.



A1, B3 Minimum height and upper story stepbacks for new buildings



A2, A3, A4 Build-to line and zero side setbacks

## B. Upper Floor Design Features

*Upper story design features should help create a safer and more interesting street environment by adding building elements (such as generous windows and balconies) that enhance the appearance of taller buildings and offer residents a connection to the public realm.*

**B1.** Provide balconies or faux balconies off of upper floor residential units along streets, alleys and open spaces to provide “eyes on the street”, facade articulation along blank walls, and potential useable space for residents. Discourage residents from placing unsightly storage on balconies.

**B2.** Allow residential balconies to protrude four to six feet from the building edge into the sidewalk realm.

**B3.** Encourage setbacks on upper floors to mitigate the height of new buildings and allow more sunlight to reach pedestrians on the sidewalk.

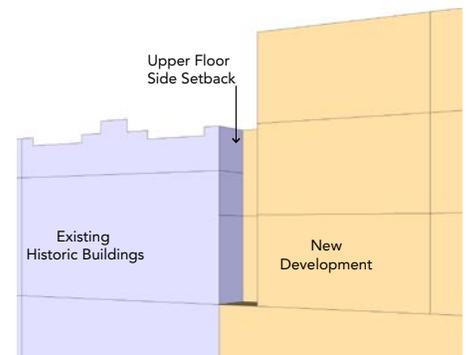
**B4.** Encourage the adaptive reuse of upper floors of existing buildings in Downtown.

**B5.** Provide upper story side setbacks in new development when built adjacent to historic buildings of lower height to respect the scale and distinctive architectural features.

**B6.** Encourage buildings, especially individual residential units, to have access to sun and air and to provide adequate light and ventilation through windows, balconies, and/or a courtyard configuration.



*B4 Successful adaptive reuse of upper floors*



*B5 Side setbacks on upper floors in new buildings adjacent to historic buildings of lower height*



*B1 Balconies and building entries along alleys*

### C. Ground Floor Design Features

*Ground floor design should contribute to an attractive pedestrian experience through transparent and engaging building design details that reflect the historic character of the area's ground floor design features.*



C1 Distinctive articulation and colors

**C1.** Encourage distinction in new buildings by varying roof heights, setbacks, building articulation, landscaping treatment, etc to provide a richer pedestrian experience.

**C4.** Ensure that the ground floor is as transparent as possible to connect the pedestrians and the building users and uses.

**C2.** Orient buildings such that the primary active facades and key pedestrian entrances of the buildings face the street.

**C5.** Require all ground floor commercial uses to have non-reflective glass windows fronting onto sidewalks. When windows face southwest and west, frame them with protruding vertical and horizontal shading elements such as lintels, sills and awnings to provide adequate protection from glare.

**C3.** Encourage corner buildings to actively face onto both streets with pedestrian-friendly entries. Encourage creative corner treatments such as rounded or cut corners, corner roof features and distinctive shop windows.



C4, C5 A high level of transparency along the ground floor with awnings to shade the windows



C1, C2, C3 Distinctive building articulation and primary active facades along streets

**C6.** Allow privacy for ground floor residential and office uses by elevating the first floor two feet above grade and allowing windows to be two to three feet above floor level.

**C7.** Encourage the use of awnings on buildings to provide shade and comfort to pedestrians.

- Where possible, make awning height match awning heights of adjacent buildings.

**C8.** Where possible, locate pedestrian-oriented entries to upper floor residential units along the street-facing façade to encourage interaction and activation on the ground floor.

**C9.** Prohibit blank walls along sidewalks.

**C10.** Mitigate blank walls with murals, faux façade elements or other elements that provide interest to the pedestrian experience.



C8 Pedestrian-oriented entries along street-facing facade



C9, C10 Prohibit blank walls and mitigate with murals



C6 Elevated 1st floor provides privacy for users



C7 Match awning heights of adjacent buildings

## D. Architectural Style and Context-Sensitive Design

*Downtown Lodi has a strong sense of character and a traditional small town quality that the local community embraces as an asset. New developments and building retrofits should respect this existing character and design in an architectural context that complements adjacent structures of significance.*



*D2, D7 Brick and marble facade on a historic building should be rehabilitated.*

**D1.** New buildings should be consistent with adjacent building facades. Front setbacks should only be allowed for outdoor dining and entrances. It is important to maintain a strong building edge in Downtown.

**D2.** Encourage the use of materials that are already prevalent in Downtown, such as brick, stone and marble. However, new building materials and details that complement the overall traditional character of Downtown are encouraged.

**D4.** Provide continuity between existing and new development by approximately matching floor heights, continuing patterns of windows and entries, and incorporating similar building elements, such as awnings.

**D5.** Utilize building elements such as cornices, lintels, sills, balconies, awnings and porches to enhance building facades. Strengthen visual linkages to adjacent building facades by extending existing corner or floor lines, repeating proportions of windows or roof features.

**D6.** Reflect the fine-grained building fabric of the existing Downtown by utilizing ground floor elements, such as windows and entries, that visually subdivide the facade with a more varied pattern.

**D7.** Prohibit the painting over of marble and other original building materials, and encourage maintaining them as a part of facade rehabilitation.



*D4 Encourage continuity in materials, building articulation and height in the Downtown*

**D8.** Utilize programs and construction methods to address train vibrations and noise, such as establishing a quiet zone, constructing sound walls, and building to higher construction standards.

**Live-Work Units**

**D9.** Orient the flexible space component of the unit towards the public realm of streets to optimize business viability.

**D10.** Design the front façade to reflect the simple yet distinctive character of industrial buildings in the area.

**D11.** Articulate the front facades with big double-height windows, awnings, saw-toothed roofs, etc.

**D12.** Encourage the use of roll-up doors along streets to support the interactive qualities of the live-work units and the viability of the businesses.



*D9 Live-work units in Emeryville*



*D10 Simple and functional, yet edgy front facades*



*D11, D12 Live-work units with roll-up doors and double-height windows*

## E. Green Design

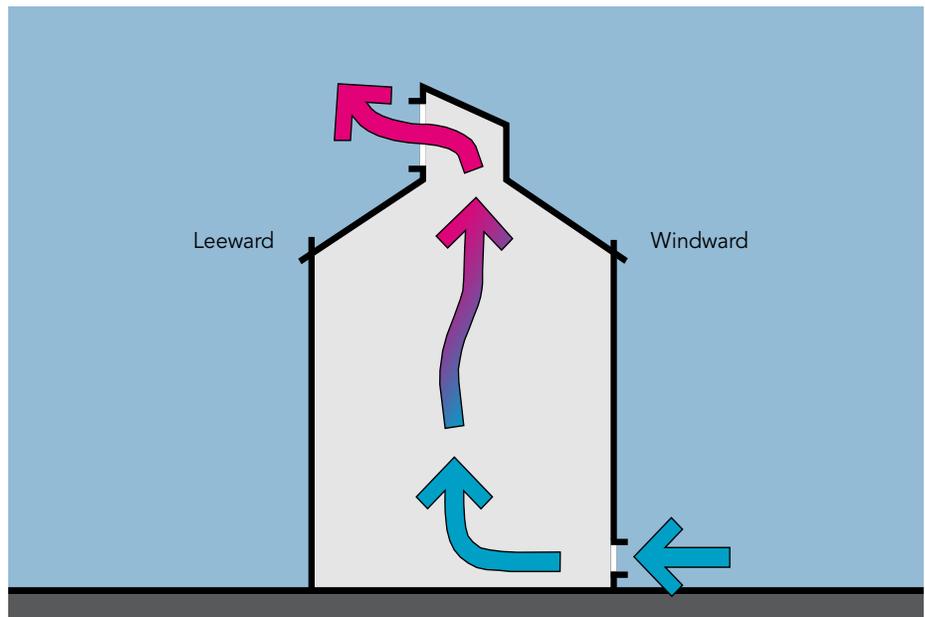
*With increasing energy costs and natural resource demands, ecologically-sustainable design is an increasingly important responsibility for planning and design. TOD is a large-scale approach to addressing environmental concerns that integrates transportation and land use decisions, reduces vehicle trips, and concentrates development in already urbanized areas. Green building design is a more site- and building-specific approach to addressing water and energy conservation. Refer to the US Green Building Council's LEED Standards for more specific green building strategies.*



E1, E3, E4, E5, E6, E7 Ecologically-sensitive building design and materials

- E1.** Encourage the use of building materials and colors that minimize heat absorption and maximize heat reflection to reduce the urban heat island effect.
- E2.** For buildings with southfacing sides, explore using vegetation along walls as it is the most effective way of minimizing heat gain.
- E3.** Provide awnings, canopies and deep-set windows on south-facing windows and entries to minimize heat gain.
- E4.** Explore using exterior shades and shade screens on east, west and south-facing windows as alternate methods for blocking sunlight.

- E5.** Encourage using horizontal overhangs, awnings or shade shelters above south windows to block summer sun but allow winter sun. Encourage overhang width to equal half the window height to shade the window completely from early May to mid-August yet allow for winter sun.
- E6.** For buildings with exposed east and west sides, provide vertical shading.
- E7.** Maximize natural cooling by installing high vents or open windows on the leeward side of the building to let the hottest air, near the ceiling, escape. In addition, create low open vents or windows on the windward side that accepts cooler air to replace the hotter air.



E7 Maximize natural cooling with high and low vents

**E8.** Encourage the use of solar panels on the roofs new development.

**E9.** Encourage roof articulation that maximizes the effectiveness of catching the sun with solar panels.

**E10.** Explore establishing a hierarchy for stormwater run-off managements, beginning at the building, then the lot, open spaces and the roadway. Maximize run-off management at each of these levels to minimize run-off into the existing stormwater system.

**E11.** Encourage the use of intensive and extensive green roofs and water collection devices, such as cisterns and rain barrels, to capture rainwater from the building for reuse.

**E12.** Explore ways to minimize on-site impermeable surfaces, such as concrete, asphalt and hardscaping.

**E13.** Encourage the use of permeable pavers, porous concrete, porous asphalt, reinforced grass pavement (turf-crete), cobblestone block pavement, etc to detain and infiltrate run-off on-site.

**E14.** Explore configuring buildings in such a way as to create internal courtyards to trap cool air while still encouraging interaction with streets.

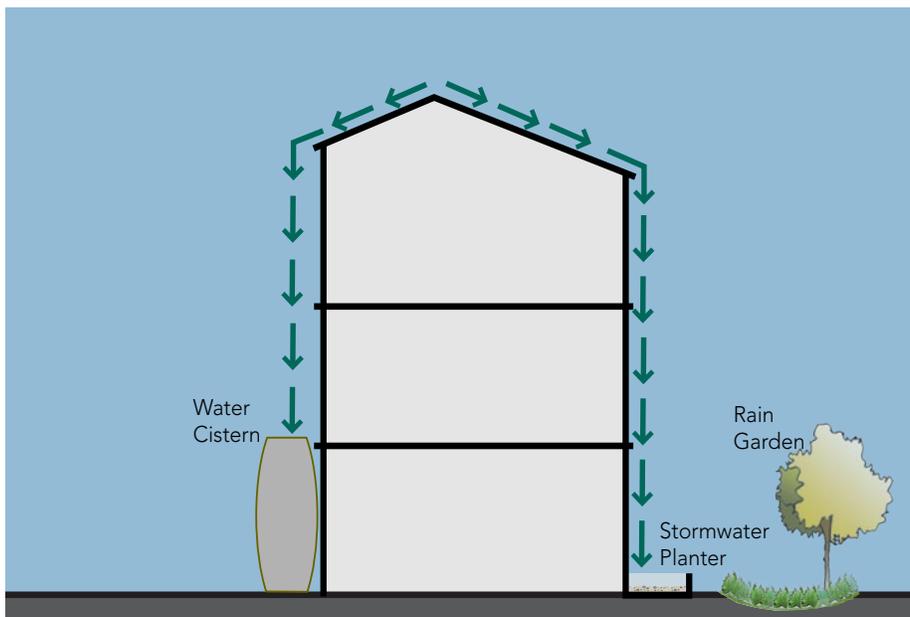
**E15.** Encourage the planting of deciduous trees on the south side of buildings to shade the south face and roof during the summer while allowing sunlight to penetrate buildings in the winter.



*E11 Green roofs help stabilize building temperatures and reduce stormwater runoff*



*E13 Permeable pavers absorb runoff and provide alternate paving materials that can add identity to a streetscape*



*E11 A variety of methods for managing stormwater runoff on-site*

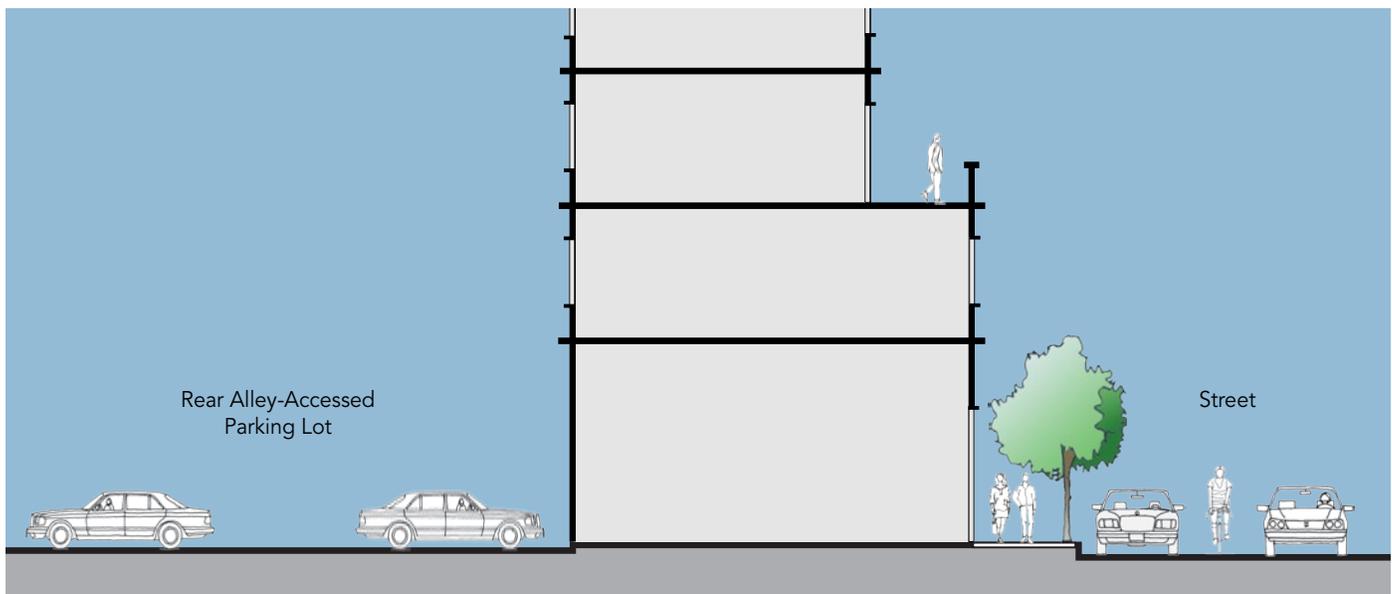
## F. Parking

*The primary goal of TOD is to reduce auto dependency through offering attractive alternatives to driving. Consequently, transit-oriented developments are expected to have less auto-related infrastructure. Transit-oriented developments should have an appropriate amount of parking - not too high so as to discourage higher density projects yet sufficient enough to accommodate realistic driving needs. Parking entrances, garages and lots should be visually minimized and located to the rear of buildings so as to take into consideration a pedestrian-oriented streetscape.*



F6 Swale in parking lots to attenuate run-off

- F1.** Reduce residential parking requirements to one space per residential unit.
- F2.** Ensure parking is located on the interior of the block behind new development.
- F3.** Encourage the use of shared parking facilities to optimize use of parking lots.
- F4.** Provide access to residential units, commercial loading areas and parking off of existing or new alleys.
- F5.** Explore the possibility for a portion of the parking requirements of individual projects to be satisfied by on-street parking.
- F6.** Explore ways to reduce run-off from existing and planned parking lots with options such as permeable paving and swales.
- F7.** Explore utilizing part of the parking structure to offset parking requirements.



F1 Parking lots located behind new development in the interior of the blocks

## G. Streetscape Design

*TOD needs a pedestrian- and biking-oriented streetscape to provide an attractive alternative to driving. People are generally willing to walk ¼ to a ½ mile to a transit station. A safe and pleasant route will encourage more people to walk or bike to the station.*

### Overarching Guidelines

**G1.** Ensure continuous ADA accessible five foot wide pathways along all streets.

**G2.** Provide clearly marked minimum 10 foot wide crosswalks at all controlled intersections and at intersections of key streets. Ensure all sidewalks have ramps for ADA access.

**G3.** Provide pedestrian-oriented streetscape amenities, including lighting, seating, trash cans and public art, at key nodes and bulbouts.

**G6.** Provide bicycle racks and/or lockers at the transit center, and explore opportunities for artistic design of bicycle racks.

**G7.** Explore using special paving material for crosswalks to heighten visibility and lend identity to the area.

**G8.** Provide a railroad crossing treatment across the entire right of way that is flush with the tracks and articulates the pedestrian zone through the use of material and color. Coordinate signage and traffic lights to make crossing safe and navigable.

**G9.** Ensure that sufficient lighting is on the streets.

### Alleys

**G10.** Ensure alleys are a minimum of 20 feet wide to allow for emergency access and possible landscaping opportunities.

**G11.** Minimize alley and service access driveway curb cuts along streets.

**G12.** Discourage the loading of commercial space off of existing streets. If loading from streets is unavoidable, restrict hours of loading to late evening and early morning.

**G13.** Where possible, create new mid-block alleys to access new development.

**G14.** Allow setbacks on new development along alleys to provide space for narrow trees and landscaping opportunities.

**G15.** Discourage the placement of dumpsters and trash receptacles in alleys.

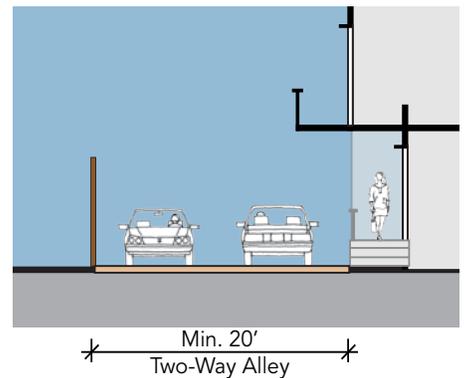
**G16.** Enforce code violations for dumping in alleys.



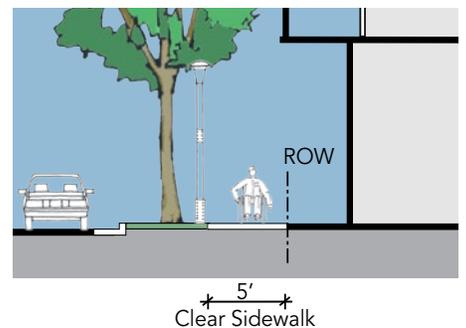
G6 Artistic design of bicycle racks



G7 Example of special paving crosswalk design in Sacramento



G8 Minimum 20' alleys



G1 Five feet wide ADA pathways on sidewalks

## G. Streetscape Design (cont.)



G14 Flush alley in Pasadena

**G17.** Allow alleys to remain flush, shared-mode surfaces for use by pedestrians, automobiles and bicycles.

**G18.** Encourage the use of distinctive paving patterns and special paving materials, including stamped concrete, colored concrete, permeable pavers, etc., in alleys to add uniqueness and character.

**G19.** Install sufficient lighting in alleys to promote safety and cleanliness.

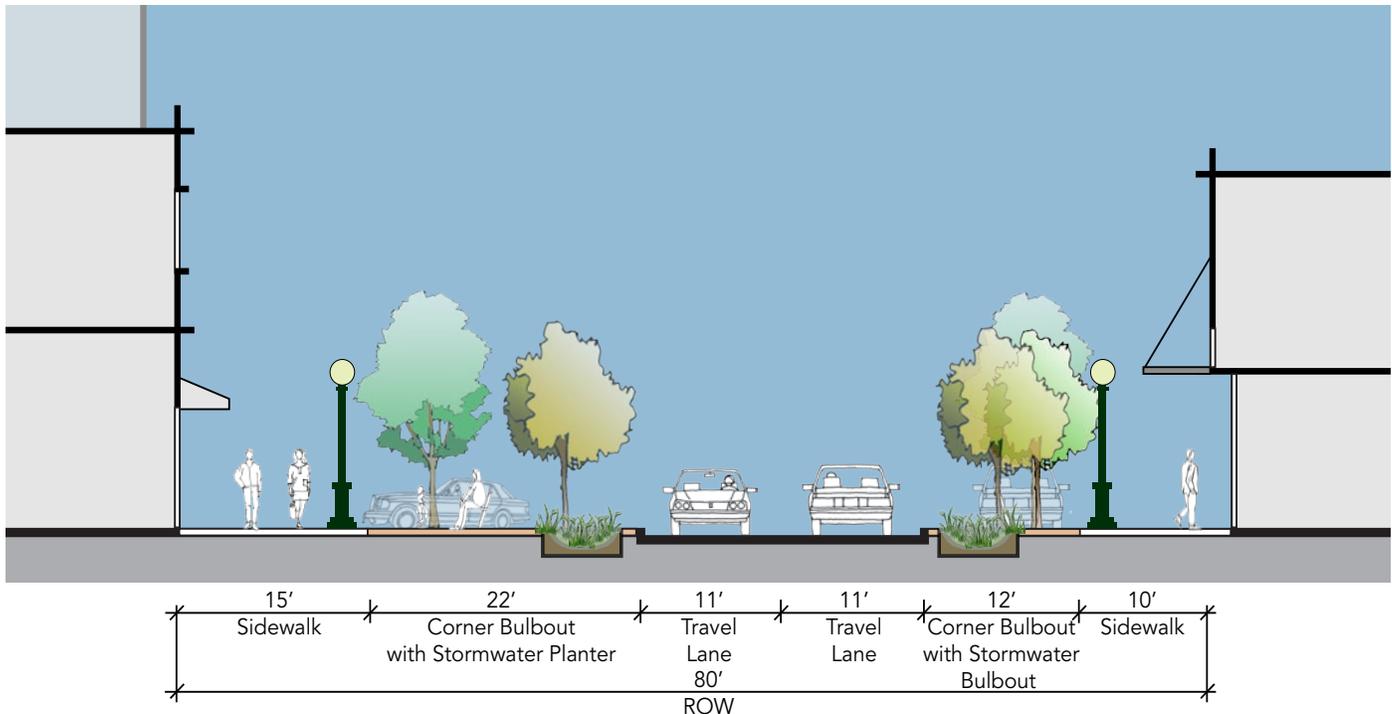
### Sacramento Street

**G20.** Create small plazas at intersection corners with bulbouts, or extensions of the sidewalk into parking spaces closest to the intersection.

**G21.** Maintain the existing configuration of Sacramento Street, except at intersections.

**G22.** At intersections, reduce lane widths to 11 ft. in each direction and extend bulbouts to minimize crossing distance and increase pedestrian visibility.

**G23.** Add landscaping planters and new trees at corner plazas to reduce impermeable surfaces.



G16, G18, G19 Proposed section of Sacramento Street at corner intersection

**Main Street**

**G24.** Encourage the use of stormwater planters at the corner intersections and along Sacramento Street to help manage stormwater runoff. Ensure that tree branches at intersections have a vertical clearance of 12 feet for safe sight lines between automobile drivers and pedestrians.

**G25.** Incorporate special paving, benches and other amenities at corner plazas to enrich the pedestrian experience.

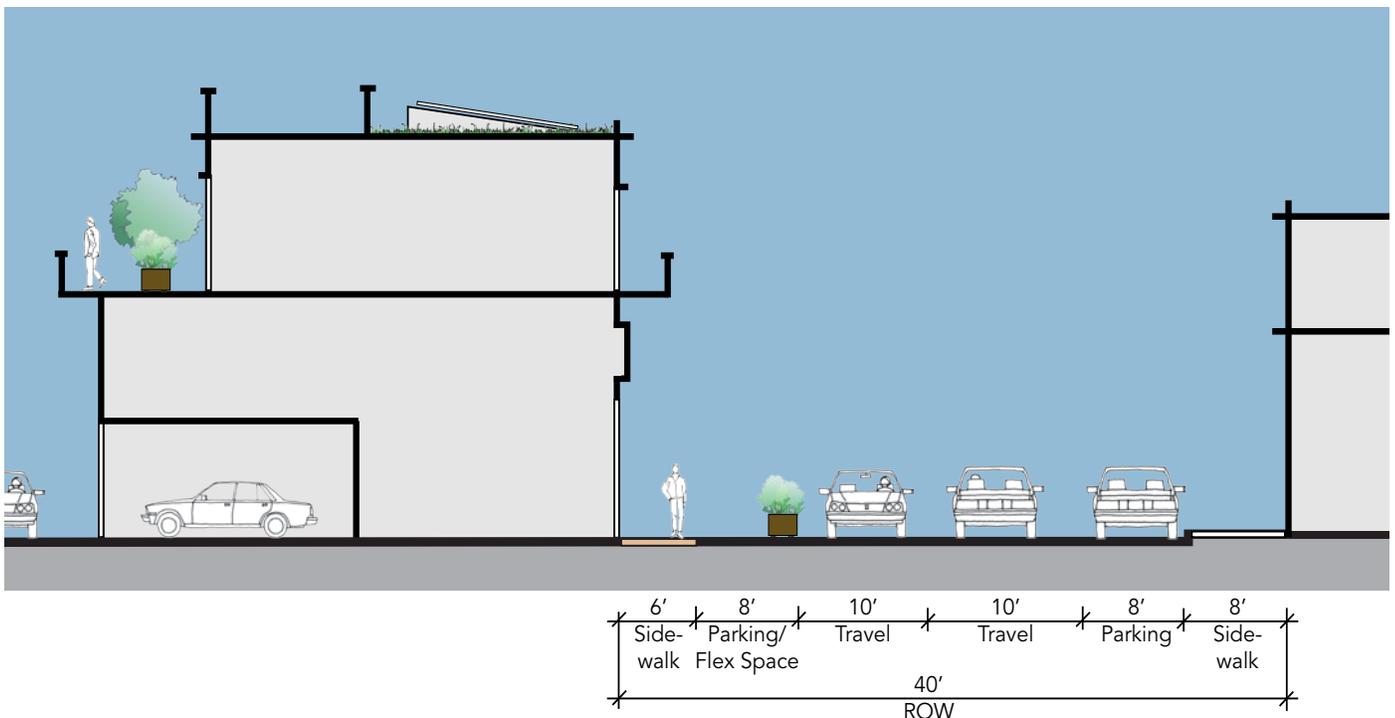
**G26.** Maintain the existing street cross-section with a sidewalk on the east side and a flush environment on the west side.

**G27.** Provide parking on both sides of the street.

**G28.** Allow the parking spaces on the west side to operate as “flex space”, in which adjacent uses are allowed to encroach into them for use such as a market.



G20 Use stormwater planters at corner intersections along Sacramento Street



G22, G23, G24 Proposed section of Main Street

## DEVELOPMENT OPPORTUNITY SITES

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The following section describes four site designs for various development opportunity sites throughout the project area. These site designs are meant to illustrate how the preceding design guidelines could be applied to specific locations and contexts.

Many development opportunity sites in the project area exist to which these site design approaches can be applied. These site designs demonstrate a high quality yet feasible approach to various design conditions in the project area. While each site is different, all the sites seek to increase housing opportunities close to the multi-modal transit station and Downtown services.

For each of four different opportunity sites in Downtown Lodi, the development scenarios were designed to be sensitive to the context of the historic downtown yet support transit-oriented and sustainable development. In each case, new housing is assumed to be a significant part of the new development, but each site and scenario also includes other uses, such as ground floor retail and alternative commercial space (such as live/work lofts).

The four sites were chosen to represent a cross-section of property types, development scenarios and circumstances. The first of the four development opportunity sites is a currently vacant parking lot owned by the City. The site is located mid-block between built-out parcels, and is accessed from the rear by a mid-block alley. The second of the sites is an entire block in the project area that was chosen because of its variety of existing uses and buildings. A design for an entire block facilitated understanding how new development could respect the existing fabric and character of Downtown Lodi. The third site is a corner parking lot in the south of the project area. It was chosen because the owner is interested in developing the site, and to explore site design and uses that would be appropriate away from the commercial core of Downtown Lodi. The fourth was chosen because of its location on the east side of the project area, which has a unique character and could support alternative uses and development.

Using data provided by local developers as well as published construction cost estimates and market data, the feasibility of each development scenario was assessed by comparing the estimated costs of each type of development (excluding land costs) to the estimated values of that development.

In cases where the values exceed the costs, a “residual land value” has been calculated, which estimates the amount that a developer could pay to acquire and prepare the site for the new construction. Whether the sites could actually be acquired for those estimated prices is uncertain, and will depend on the considerations of the current property owner.

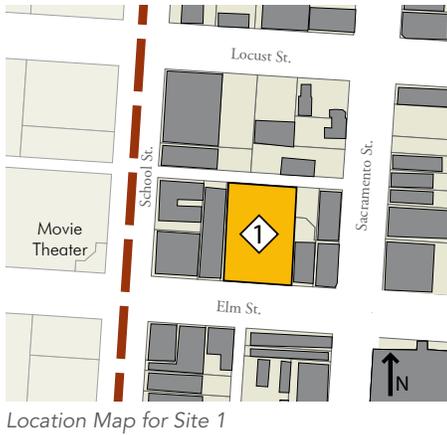
In general, the findings indicate that the most feasible developments are those that minimize parking, utilize lower-cost surface parking rather than higher-cost structured parking, and offer residential units for-sale rather than for-rent.



Figure 4.3 Development Opportunity Sites

## Site 1: Downtown Infill on Elm Street

**Design Goal:** *The design of Site 1 demonstrates infill development on a built-out Downtown block. It represents a design on a City-owned parcel, whose dimensions are typical of the small-scale, parcel sizes and depths of many Downtown lots.*



Location Map for Site 1

### Site Description

Site 1 is located on the north side of Elm Street between School and Sacramento Streets. The lot is 120 feet by 170 feet (20,400 square feet or 0.5 acres.) The site is owned by the City and currently used as a public parking lot located between two small-scale commercial buildings. A rear alley runs behind the parcel and is currently underutilized as an access and service route. The site's location in the heart of Downtown Lodi and in close proximity to the Multi-Modal Transit Station presents an opportunity to increase services and residential intensity. Elm Street runs along the south side of the site, and its innovative design as a pedestrian-oriented street suggests the design of a highly engaging building facade for any new development.

### Design Approach

- Locate service and residential parking access from alley.
- Design a building façade that integrates with adjacent buildings.
- Increase residential density in the project area.
- Create an engaging pedestrian-oriented building façade.
- Mitigate building height with architectural elements, such as stepbacks and other features that visually connect with adjacent buildings.



Rooftop garden



Photorealistic rendering of Site 1 - Before

## Development Program

The development program for Site 1 is mixed-use commercial and residential development. Three designs (titled Options 1A, 1B and 1C) were developed for Site 1 based on various parking solutions (structured, surface and a combination of the two).

In all three options, the proposed development scenario assumes commercial activity is accessed by pedestrians walking around Downtown and customer parking is assumed to be supplied by existing on-street.

The program of the building fronting Elm is mixed-use residential over ground-floor commercial. This building remains identical in all three site design options.

The diagram below is a photo simulation of this new mixed use building as seen from Elm Street. It is one large building designed to look like three smaller buildings to fit into the fine-grain context and traditional character of Downtown Lodi.

A common element in the Site 1 design options is the prevalence of rooftop gardens and generous balconies. These features break up and articulate the massing of a building to make it more dynamic and interesting. Rooftop gardens are also a terrific way to offer residents an intimate “backyard” in the city and easy connection to the natural elements.

## Feasibility Analysis

The three site designs created for Site 1 not only illustrate a range of design approaches that differ in the ratio of residential density to surface parking but also show different development feasibility levels for each design scenario.

Option A shows a design approach that maximizes residential density and square footage (32 residential units; 30,950 square feet of total residential space) by building residences over a parking podium. However, given the higher cost of podium parking (\$16,000 per parking space) compared to surface parking (\$3,000 per parking space), Option A resulted in a negative residual land value for both for-rent (-\$2,484,510) and for-sale scenarios (-\$698,097).



Photosimulation of Site 1 - After

### Feasibility Analysis (cont.)

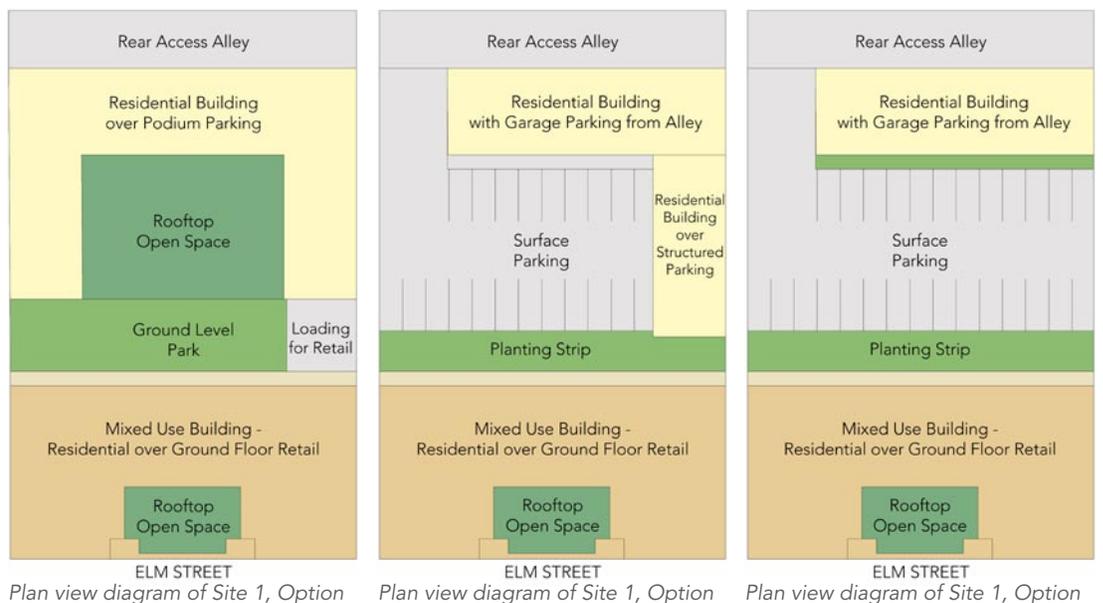
Option B strikes a moderate compromise between residential density and square footage (30 residential units; 32,910 square feet of total residential space) and a combination of surface and structured parking. By relying more heavily on surface parking, Option B has much lower total development costs than Option A however, is still unable to break even between development costs versus development value. The residual land value for both for-rent (-\$1,782,346) and for-sale (-\$55,264) scenarios are negative.

Option C locates all the required parking on surface parking spaces and has the least amount of residential units and square footage (26 residential units; 29,510 square feet of total residential space) compared to Options A and B. As a result, this design scenario has the lowest development costs compared to the other design options. While the for-rent scenario in Option C results in a negative residual land value (-\$1,420,565), the for-sale scenario results in a positive residual land value of \$68,957.

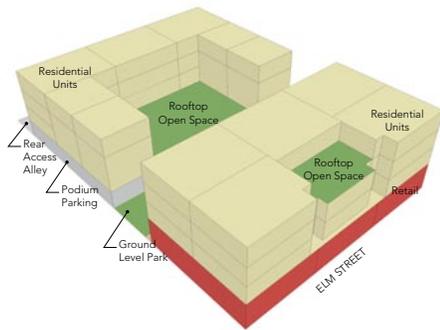
This design exercise demonstrates that 1) for-sale residential units contribute greatly to a development’s feasibility and 2) surface parking is far more feasible than structured parking. However, several public funding sources are currently available to municipalities precisely to bridge the funding gap and increase residential densities higher than market realities would allow.

	Site 1A		Site 1B		Site 1C	
	For-Rent	For-Sale	For-Rent	For-Sale	For-Rent	For-Sale
Housing Units	32	32	30	3-	26	26
Retail SF	6,456	6,456	6,456	6,456	6,456	6,456
Parking						
Surface Parking	0	0	27	27	27	27
Structured	36	36	6	6	0	0
Street	0	0	0	0	0	0
Total Development Costs	\$7,974,104	\$9,319,360	\$7,780,820	\$9,137,127	\$7,009,556	\$8,213,906
Total Development Value*	\$5,489,594	\$8,621,263	\$5,998,474	\$9,081,863	\$5,578,991	\$8,282,863
Residual Land Value	-\$2,484,510	-\$698,097	-\$1,782,346	-\$55,264	-\$1,430,565	\$68,957

\* Assumes 15% affordable housing Sources: MIG, Inc.; Economic & Planning Systems, Inc.



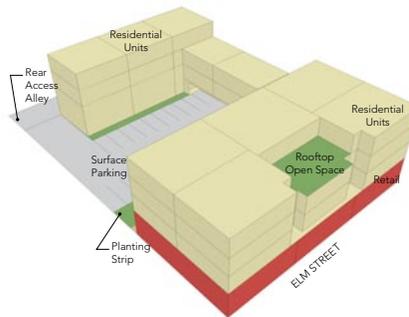
## Site 1A



Land use axonometric of Site 1, Option A

Site 1A has a total of 32 residential units, approximately 6,400 square feet of ground floor commercial development along Elm Street and 36 parking spaces. The residential parking for this option is ground floor podium parking with residential units on top. The unit breakdown for Site 1A is as follows: four studio units at 625 square feet, three one-bedroom units at 850 square feet, 20 two-bedroom units ranging between 925 and 1,200 square feet, four lofts with mezzanine level at 950 square feet, and one large loft with mezzanine level at 1,600 square feet. There is a 2,400 square foot ground-level garden between the two buildings to provide an open space amenity and privacy. This design has the highest residential density, at 68 dwelling units per acre. Parking and loading for the buildings is accessed off of the rear alley to the north.

## Site 1B

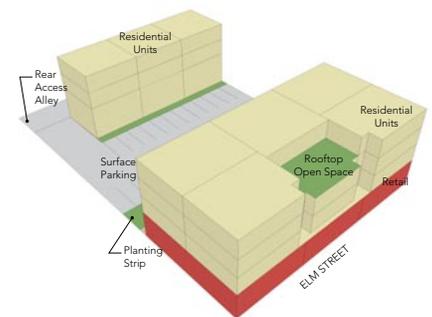


Land use axonometric of Site 1, Option B

Site 1B has 30 residential units, approximately 6,400 square feet of ground floor commercial development along Elm Street, 27 on-site surface parking spaces, six covered spaces and three garage spaces. There are six one-bedroom units at 850 square feet, 21 two-bedroom units between 930 and 1,200 square feet, and three lofts at 1,410 square feet included in the design for Site 1B.

This design has a slightly lower density 64 dwelling units per acre. Access to parking and loading is still accessed off of the rear alley. The three ground level units along the alley are served by three garages.

## Site 1C



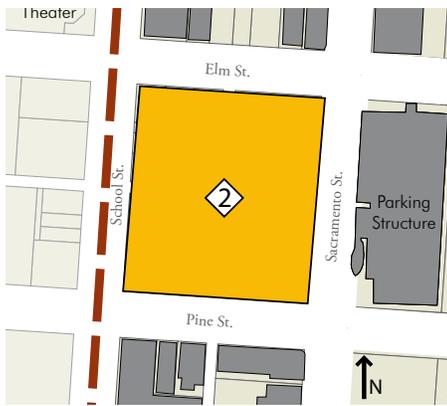
Land use axonometric of Site 1, Option C

Site 1C has 26 residential units, approximately 6,400 square feet of ground floor commercial development along Elm Street, 27 on-site surface parking spaces and three garage spaces. There are two one-bedroom units at 850 square feet, 21 two-bedroom units from 930 to 1,200 square feet, and three lofts at 1,410 square feet.

Site 1C contains all surface parking and the lowest residential density of the three options but still relatively dense at 55 dwelling units per acre.

## Site 2: Block-Sensitive Design and Adaptive Reuse

**Design Goal:** *Site 2 demonstrates a context-sensitive approach to the design of a prototypical city block. Key characteristics of this block are representative of many Downtown blocks, including vacant buildings, viable existing uses, vacant upper floors, historic buildings and facades, and mid-block alleys, and these characteristics provided the parameters for the site design.*



Location Map for Site 2



Facade restoration along historic buildings on Sacramento Street

### Site Description

The site is bounded by Sacramento, Elm, Pine and School Streets. The lot is 320 feet by 360 feet (115,200 square feet or 2.6 acres.) Many of the buildings along School and Pine Streets are historic and contribute to the main commercial corridor of Downtown Lodi. Most are occupied by ground floor retail, though their second floors are vacant. Vacant and nearly vacant buildings on the southern half of Sacramento Street have striking historic building facades. Joe Hassan's brick warehouse at the northeast corner the block further adds to the historic character of Sacramento and Elm Streets. Two mid-block alleys, one oriented north-south and the other east-west, provide access to the interior of the block. The interior of the block is primarily used for parking and loading.

### Design Approach

- Design in a context-sensitive way to maintain integrity of Downtown commercial activity.
- Judiciously use incremental development, strategic infill and adaptive reuse.
- Adaptively reuse Joe Hassan's Warehouse and and rehabilitate historic facades along Sacramento Street.
- Reuse upper floors of existing buildings for office and residential uses to maintain fabric of Downtown.
- Build on local businesses and do not make large-scale redevelopment moves that would jeopardize the health of the Downtown economy.
- Create interior block public open space and connect to adjacent streets and destinations between buildings with pedestrian mews.



Perspective rendering of new development along Sacramento Street in Site 2

## Development Program

Initially, a whole scale redesign of the block was envisioned, but the existing historic buildings, viable retail establishments, and grain of development suggested that a more surgical and nuanced approach to the site design was appropriate. Further any large-scale block redevelopment would disrupt the successful commercial activities on School Street and possibly displace small businesses.

The design that respected the assets of the existing block and built upon them focused on a three prong approach: adaptive reuse, facade rehabilitation with new construction and complete new construction.

The first was to evaluate the second floors of existing buildings along Elm, School and Pine Streets to determine the potential for reuse of the vacant upper floors. This involves the conversion of the second floors of the buildings into viable spaces for residential and office uses.

Many of the buildings are deep and only have access to sunlight on two sides; these were deemed most appropriate for commercial or office reuse. Corner buildings that have more access to sunlight and ventilation can subdivide their upper floors into residential units. Adaptive reuse is also appropriate for Joe Hassan's Warehouse at the corner of Sacramento and Elm Streets. The warehouse is made

of very distinctive brick and provides half of the street façade along Elm Street, also providing historic identity to the street. It is envisioned as adaptively reused lofts by perforating the brick walls with more windows and separating the warehouse into multiple live-work units.

The second prong was facade rehabilitation for the buildings on the south side of Sacramento Street. It is likely economically infeasible to reuse the building structure, but the facades could be saved and become the "face" of new development that could be built behind them. By rehabilitating the facades, the character and charm of the buildings can be retained while also allowing for new mixed-use development.



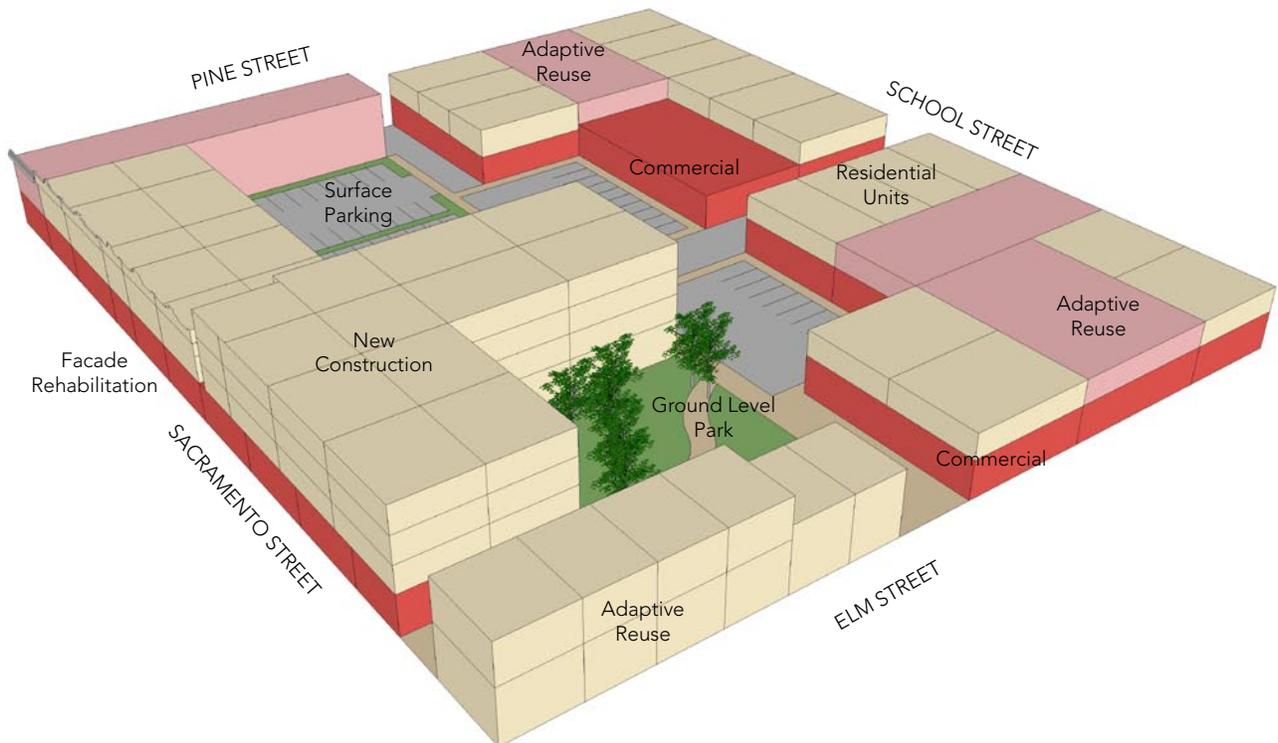
Site plan for Site 2

Some new construction is proposed along Sacramento Street in place of the existing buildings between the historic facades and Joe Hassan's warehouse. This new construction is mixed-use with residential units over commercial space. The ground floor is ample enough to support a small-scale grocery store that could serve new residents of the area.

The development program for all new construction (including new construction behind the historic facades along Sacramento Street) consists of mixed-use development with 51 residential units over 16,200 square feet of ground-floor retail. The residential unit breakdown is as follows: six 1,200 square feet lofts in

Joe Hassan's warehouse, six studios at 625 square feet, nine one-bedroom units at 750 square feet, and 30 two-bedroom units ranging between 930 and 1,000 square feet. The residential density for this development is approximately 40 dwelling units per acre.

A new public park is possible within the interior of the block connected to Sacramento and Elm streets by a pedestrian corridor. The existing alley network is used for accessing the park and parking on the interior of the lot. The alley off of Elm St has been cut off to through-traffic and serves as a pedestrian connector and emergency vehicle access.



Land use axonometric for Site 2

## Feasibility Analysis

The feasibility analysis shown below consists only of newly constructed developments on Sacramento Street. The analysis does not include buildings identified for adaptive reuse nor does the construction cost include historic facade rehabilitation costs.

In order to create a feasible development scenario, all the parking required for the new housing is located on lower-cost surface parking. The proposed development feasibility is greatly helped by the availability of 41 already existing on-street parking spots to service the 16,200

square feet of new commercial space on the ground floor.

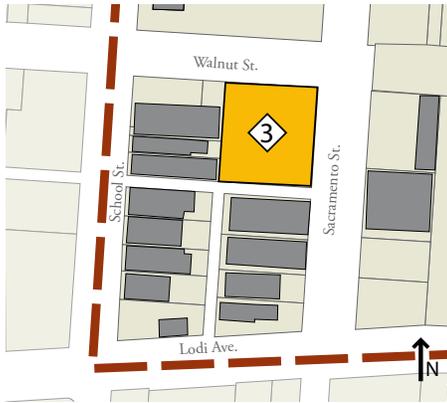
As with Site One, a for-sale scenario (\$6,867) for the residential units is far more feasible than offering rental units (-\$2,976,719).

	Site 2	
	For-Rent	For-Sale
Housing Units	51	51
Retail SF	16,200	16,200
Parking		
Surface Parking	58	58
Structured	0	0
Street	41	41
Total Development Costs	\$12,497,447	\$14,437,143
Total Development Value*	\$9,520,731	\$14,444,010
Residual Land Value	-\$2,976,719	\$6,867

\* Assumes 15% affordable housing

Sources: MIG, Inc.; Economic & Planning Systems, Inc.

## Site 3: “Flexible” Downtown Infill



Location Map for Site 3

**Design Goal:** *Site 3 is representative of medium-scaled lots that are not located in the commercial core of Downtown Lodi. This site incorporates land uses and programs that allow for flexibility in the design depending on how this part of the project area evolves in the future.*

### Site Description

Site 3 is at the corner of Walnut and Sacramento Streets and is currently a parking lot. The parcel is 160 feet by 130 feet (20,800 square feet or 0.5 acres.) The owner of the property is interested in developing the site, and the site design is meant to aid him and suggest types of development that would benefit him as well as the community at large.

The alleys that run through most of the blocks in Downtown Lodi do not run along the western edge of Site 3; as a result, the site is accessed completely from either Walnut or Sacramento Streets.

The site is just north of Lodi Avenue, at which point the retail corridor of School Street has transitioned to more auto-oriented, office and industrial uses. The site presents the opportunity to incorporate land uses such as office and live-work that are more compatible with the context.



Perspective rendering of new development at Walnut and Sacramento streets in Site 3

## Design Approach

- Incorporate flexible office live/work lofts that add housing and ground floor commercial activity.
- Continue ground floor retail and office on Walnut Street.
- Incorporate green building design features solar panels.
- Continue Downtown alley network.
- Ensure consistent scale and design with neighboring architecture.
- Promote flexibility in ground floor uses.

## Development Program

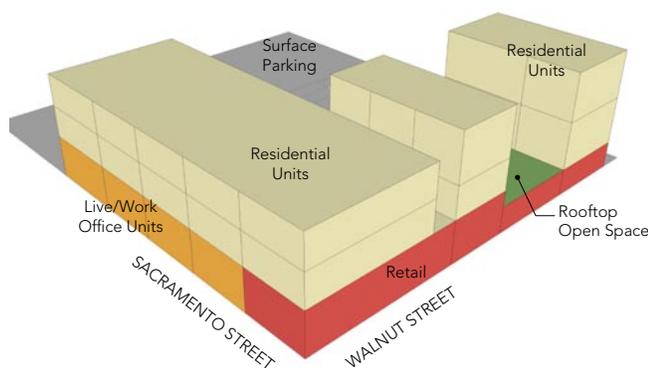
For both options, the program along Sacramento Street remains the same. There are four live-work office units on the ground floor of Sacramento Street with a small amount of retail at the corner and extending back along Walnut Street. Above the live-work office units are two stories of loft-like condominiums.

## Site 3A

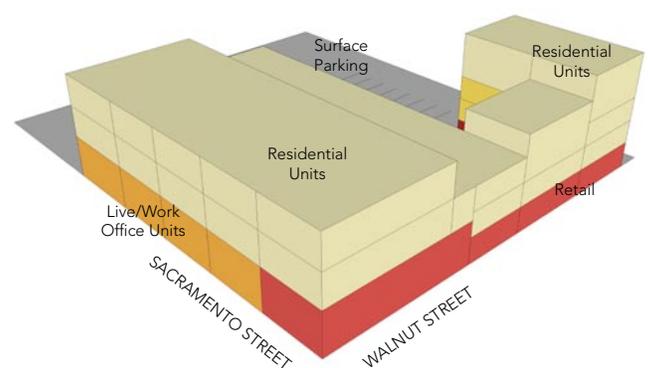
Option 3A has 28 residential units and parking tucked under the rear units on the interior of the lot. The units include 10 one-story lofts along Sacramento Street at 1,250 square feet, three two-story lofts along Walnut Street at 1,150 square feet, eleven studio, one-bedroom and two-bedroom units ranging from 750 to 900 square feet, and four office live-work units at 1,250 square feet. Residential units over the carport parking on the ground level can be accessed from the interior of the lot. There are also residential units above the retail along Walnut Street. The residential units are parked at a 0.8 parking ratio.

## Site 3B

Option 3B has 26 units and surface parking. There are 10 one-story lofts along Sacramento Street at 1,250 square feet, 12 two-story lofts over retail along Walnut Street at 900 square feet, and four office live-work units at 1,875 square feet at the ground floor along Sacramento Street. The residential units are parked at a 1:1 parking ratio. All the residential parking is parked on-site as surface parking. There are additional residential lofts that front onto a rooftop open space along Walnut St. In this configuration, a small open space is created on the lot as an amenity for the residents.



Land use axonometric of Site 3, Option A



Land use axonometric of Site 3, Option B

## Feasibility Analysis

Similar to the range of design options in Site One, Site Three has two design options that differ in the amount of residential density and surface parking.

Option A has a higher number of residential units (28) and a total of 32,600 square feet of residential living space.

To maximize the amount of residential space, apartments are built over 9 struc-

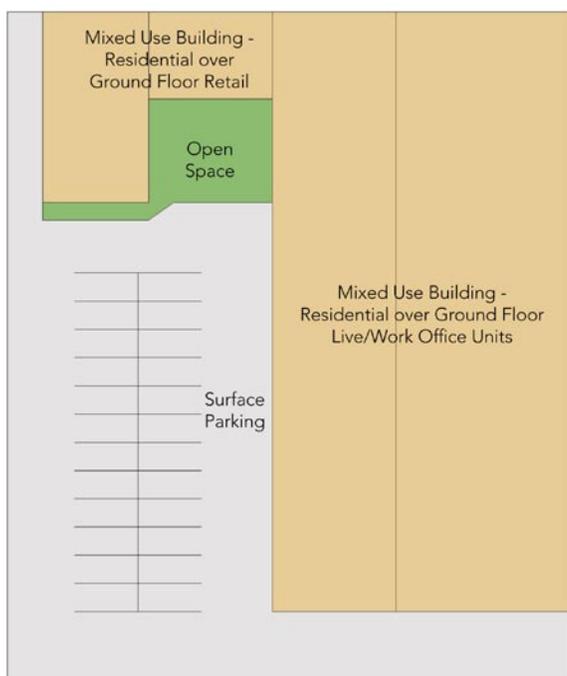
tures parking spaces. Option B utilizes all surface parking and has a few number of residential units and has a total of 30,800 square feet of residential living space total.

The for-sale scenario of Option B is the only one of the four that has a positive residual land value (\$99,075); however, several public funding sources are cur-

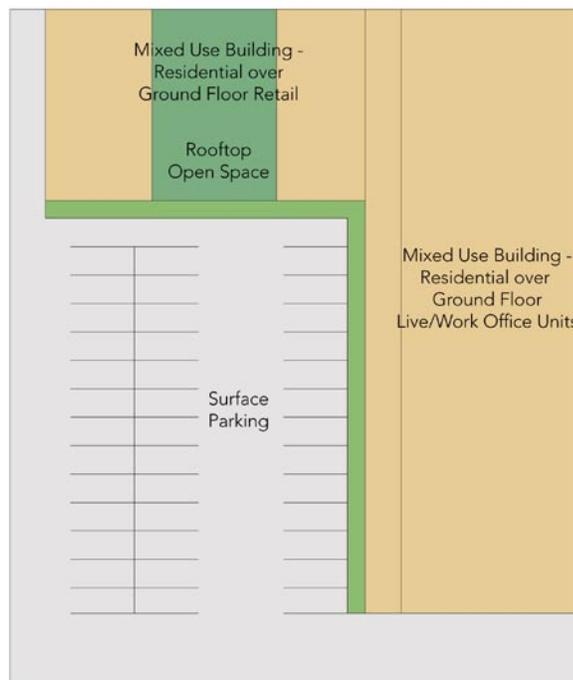
rently available to municipalities precisely to bridge the funding gap and increase residential densities higher than market realities would allow. This Plan recommends seeking these sources of additional funding and subsidizing development projects in the project area so as to maximize the amount of residential units constructed.

	Site 3A		Site 3B	
	For-Rent	For-Sale	For-Rent	For-Sale
Housing Units	28	28	26	26
Retail SF	3,120	3,120	3,700	3,700
Parking				
Surface Parking	14	14	26	26
Structured	9	9	0	0
Street	16	16	16	16
Total Development Costs	\$7,026,700	\$8,365,843	\$6,661,327	\$7,911,485
Total Development Value*	\$5,428,570	\$8,312,456	\$5,316,639	\$8,010,560
Residual Land Value	-\$1,598,129	-\$53,387	-\$1,344,688	\$99,075

\* Assumes 15% affordable housing Sources: MIG, Inc.; Economic & Planning Systems, Inc.



Plan view diagram of Site 3, Option A



Plan view diagram of Site 3, Option B

## Site 4: Industrial Infill Development

**Design Goal:** *The design for Site 4 demonstrates opportunities for development on the east side of the project area. The site was chosen because it is representative of the east side's more constrained parcel dimensions and industrial character.*

### Site Description

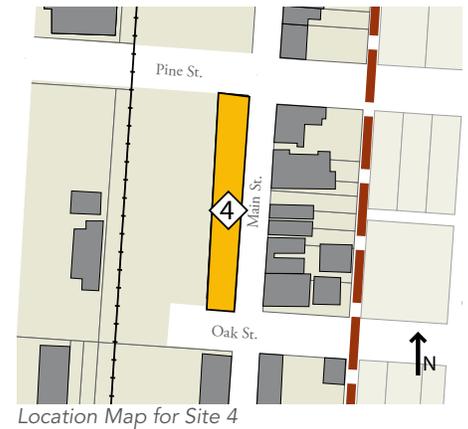
The site is bounded by Main, Pine and Oak Streets and the railroad tracks.

Currently, the site is used as a seasonal vegetable and fruit packing warehouse.

The surrounding character is more industrial and gritty in nature than the west side of the project area. Main Street has sidewalks on the east side, but not on the west side of the street. The lot is 60 feet by 380 feet (22,800 square feet or 0.5 acres.)

### Design Approach

- Design live-work lofts that respond to existing industrial character.
- Incorporate green design features.
- Utilize green roofs and rooftop gardens to provide private open space opportunities and to help regulate cooling and heating of the residential units.
- Integrate units with unique design for Main Street to encourage an activated street design.
- Inspired by "Active Space".



Active Space in Berkeley



Perspective view of new live/work lofts on Main Street in Site 4



"Flex" parking spaces in Mountain View



"Flex" spaces in San Francisco on Park-ing Day



Interior view of "green" live/work units

## Development Program

The development program is 16 live/work loft units fronting Main Street with alley access in the rear for garages. The front of the unit has a large sliding garage door opening that serves as a large, transparent feature that allows light into the ground floor work space.

The lofts are all three story lofts with ground-floor work space. 11 of the lofts are 20 feet wide with a one-car garage and 370 square feet of ground-floor work space. Five of the lofts are 25 feet wide with a two-car garage and 467 square feet of work space.

As discussed earlier in the design guidelines for Main Street, the parking lane in front of the lofts is envisioned as a "flex" space. The spaces could be converted regularly for a market for the worker/residents of the lofts to sell their wares, or could be converted on a more informal basis. Example of such "flex" spaces have been successful in Downtown Mountain View where parking spaces have been converted into outdoor seating for restaurants. On Park-ing Day in San Francisco, parking spaces around the city are temporarily converted into parks.



Plan view diagram of Site 4

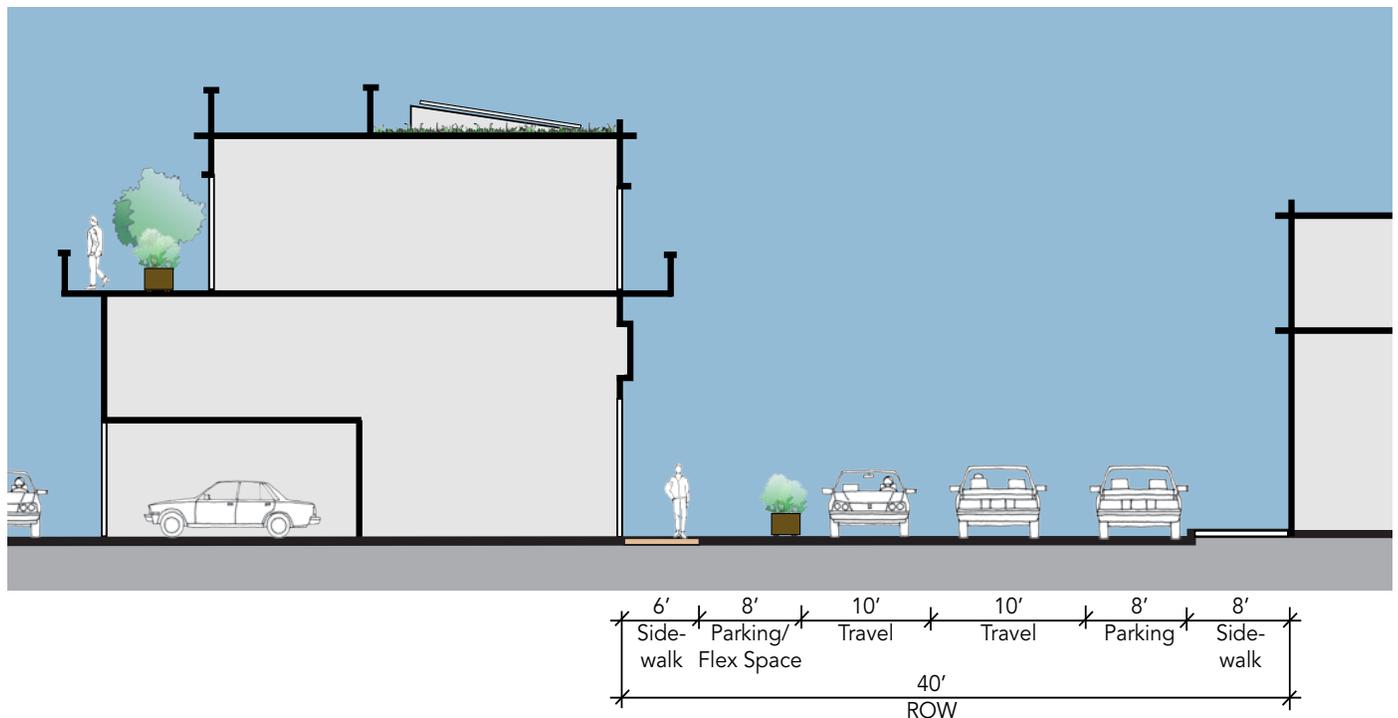
### Feasibility Analysis

The design for Site Four consists entirely of 16 new townhomes on the east side of the project area. As with all the test designs on each site, the for-sale scenario results in a positive residual land value (\$410,006) as compared to the for-rent scenario (-\$490,910).

	Site 4	
	For-Rent	For-Sale
Housing Units	16	16
Retail SF	0	0
Parking		
Surface Parking	0	0
Structured	21	21
Street	18	18
Total Development Costs	\$4,018,248	\$4,950,482
Total Development Value*	\$3,527,338	\$5,360,487
Residual Land Value	-\$490,910	\$410,006

\* Assumes 15% affordable housing

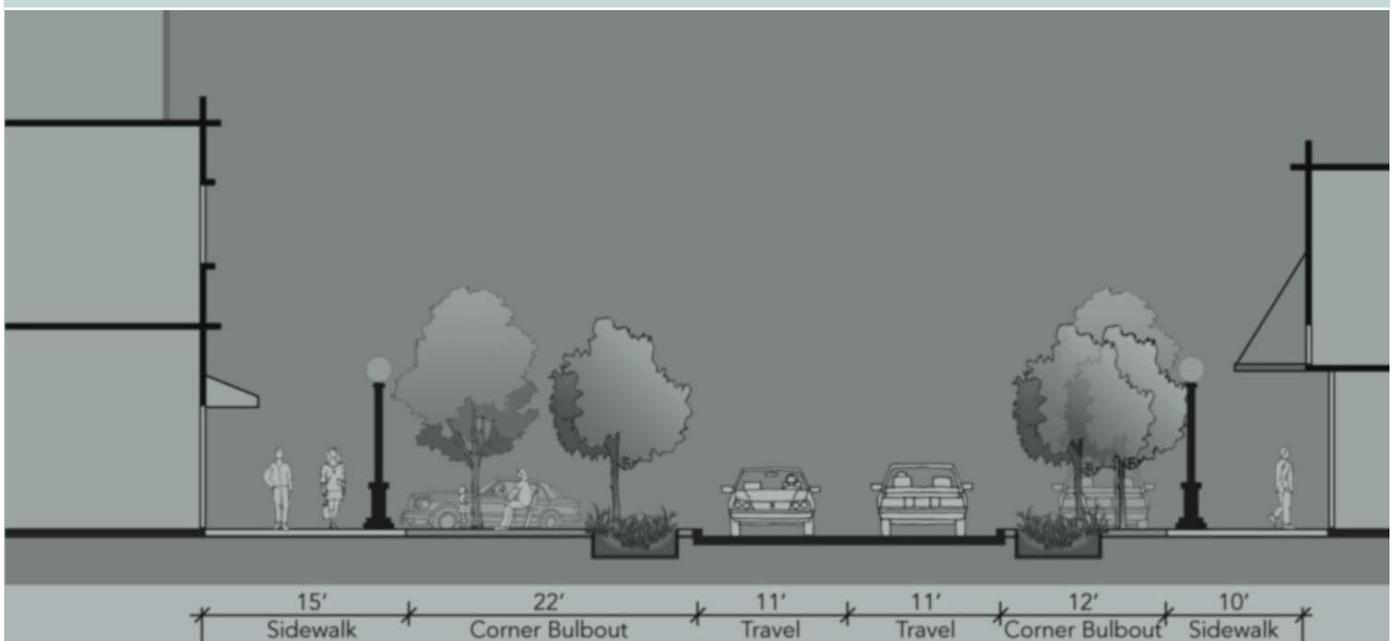
Sources: MIG, Inc.; Economic & Planning Systems, Inc.



Proposed section of Main Street



# 4 implementation





# IMPLEMENTATION 4

in this chapter:  
Priority Action Steps  
Financing Strategies  
Priority Action Matrix

**TO ENSURE SUCCESSFUL IMPLEMENTATION** of the Lodi TOD Design Guidelines, this chapter provides a road map for carrying out the strategies recommended in the plan.

The strategy includes a **refined set of priority improvements and an inventory of potential funding mechanisms**. Lodi stakeholders, City staff, Lodi TOD steering committee members, local residents and property owners must work together on these action steps to maintain momentum and realize the Lodi TOD vision.

In addition, this chapter also includes a list of funding mechanisms that the City could pursue to fund Lodi improvement efforts. This list includes local financing tools as well as state and federal programs.

## PRIORITY ACTION STEPS

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*The Lodi TOD Design Guidelines provides clear, specific direction for rejuvenating the city center while allowing for flexibility and input as the area grows and changes. Project timelines, in particular, may need to be modified as market demands change, behavioral patterns shift, and momentum builds around Lodi TOD development.*

*While this plan provides the short-and long-term road map for success, work must be done on the ground every day to ensure that its objectives are met and tangible change occurs. Lodi TOD stakeholders must work together to continue to build public and private support for the Lodi TOD visions and actions, and to ensure that every project is leveraged to its maximum extent.*

### 1. Lower Parking Standards

Adopt a parking management program for the project area that reduces parking requirements, utilizes shared parking, and integrates parking structures that have retail and/or office uses wrapped around it.

### 2. Modify Land Use Regulations

Land use regulations should be tailored to promote TOD designs. For example, Transit Overlay zoning or the establishment of TOD Districts could be done to favor TOD implementation. The City of Sacramento working with the Sacramento Regional Transit District (RT) created a Transit Overlay zoning for the light rail stations within the City in order to attract quality TOD projects. Such amendments will encourage housing in the TOD area.

### 3. Acquire TOD Funding

Coordinate among the necessary stakeholders such as the City of Lodi, GrapeLine Bus, Amtrak, Chamber of Commerce, Downtown Partnership, San Joaquin Council of Governments, private developers to position itself for the Prop 1C funds.

### 4. Improve Pedestrian and Transit Facilities

Make sure new streetscape improvements and Multi-Modal Center upgrades provide increased pedestrian and bike amenities, such as sufficient sidewalk shade coverage and safe bike storage facilities.

### 5. Bring Commuter Rail to Downtown Lodi

Incorporate transit service into future development/redevelopment projects is also critical to the success of TODs. New development site plans could be required to incorporate strategies that improve transit service and make people drive less.

## FINANCING STRATEGIES

Various financing methods are available for implementing transit-oriented development. Such mechanisms include local improvement districts, tax increment financing, sales tax increases, public/private partnerships and grants (federal, state and local). In order to be able to attract the right type of developer and project, Lodi should demonstrate support for TOD by providing incentives to entice developers to engage in TOD.

Incentives such as tax exemptions, an expedited permit review process, density bonuses, or a reduction or waiver of certain development fees may help “bridge the gap” for an interested developer. Additionally, reducing or waving certain development fees is another incentive technique. For example, traffic impact fees could be reduced where there is a higher level of transit service.

## RECOMMENDED ACTION MATRIX

The recommended action matrix describes important steps the City can take to encourage transit-oriented development for the developers as well as create a supportive physical environment for transit-oriented living.

Public Realm Improvements	Recommended Action Steps	Key Implementer	Funding Source (if necessary)
PRIORITY #1: Parking Management Program	Reduce parking standard requirements.	City of Lodi	
	Offer shared parking arrangements.		
	Offer street parking to offset on-site parking requirements.		
PRIORITY #2: Work with Developers	Facilitate parcel assembly through a cooperative sharing of ideas and vision.		
	Offer design guidelines and templates.		
PRIORITY #3: Acquire TOD Funding	Create a transit overlay zone or TOD zoning designation to implement new standards for the project area.		Prop 1C
PRIORITY #4: Improve Pedestrian and Transit Facilities	Upgrade pedestrian and bike amenities along key streets.		Prop 1C
PRIORITY #5: Bring Commuter Rail to Downtown Lodi	Pursue the opportunity for commuter rail with the Regional Rail Commission.		







**RESOLUTION NO. P.C. 08-21**

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LODI ENDORSING THE TRANSIT ORIENTED DEVELOPMENT DESIGN GUIDELINES.AND RECOMMENDING THAT THE LODI CITY COUNCIL ADOPT TRANSIT ORIENTED DEVELOPMENT DESIGN GUIDELINES.**

**WHEREAS**, the Planning Commission of the City of Lodi has held a duly noticed meeting to discuss this matter; and

**WHEREAS**, the project proponent is the City of Lodi Community Development and Public Works Departments; and

**WHEREAS**, the proponent has held a series of public workshops to obtain input and ideas from residents of Lodi; and

**WHEREAS**, the workshops were well attended and residents have provided valuable ideas on how to encourage developments within ¼ mile of the Multi Modal Transit Station and downtown Lodi; and

**WHEREAS**, based on the ideas gathered at these workshops, the City has developed a written set of guidelines that can be used by the Public Works and Community Development Departments; and

**WHEREAS**, the plan includes ideas on how to improve accessibility and mobility for pedestrians, bicyclists, mass transit users, and motorists along these corridors as well as ways to improve the general appearance of the streets and adjoining properties while reflecting the cultural heritage and community pride that distinguished the areas within the study area; and

**WHEREAS**, the Community Development Department recommends the City of Lodi adopt and implement the plan; and

**WHEREAS**, all legal prerequisites to the adoption of this Resolution have occurred; and

**WHEREAS**, the proposed plan is consistent with all zoning and general plan standards.

**NOW, THEREFORE, BE IT DETERMINED AND RESOLVED** by the Planning Commission that the proposed Transit Oriented Development Design Guidelines will be a valuable planning tool for the City of Lodi and is recommended to be adopted and implemented by the City Council.

Dated: August 13, 2008.

I hereby certify that Resolution 08-21 was passed and adopted by the Planning Commission of the City of Lodi at a regular meeting held on August 13, 2008, by the following vote:

- AYES:           Commissioners:
- NOES:           Commissioners:
- ABSENT       Commissioners:
- ABSTAIN:      Commissioners:

ATTEST:

\_\_\_\_\_  
**Secretary, Planning Commission**

# Item 4a.

# Item 6a.



**MEMORANDUM, City of Lodi, Community Development Department**

**To:** City of Lodi Planning Commissioners  
**From:** Peter Pirnejad, Planning Manager  
**Date:** Planning Commission Meeting of 8/13/08  
**Subject:** Past meetings of the City Council and other meetings pertinent to the Planning Commission

In an effort to inform the Planning Commissioners of past meetings of the Council and other pertinent items staff has prepared the following list of titles.

If you have any questions, please feel free to contact the Planning Department or visit the City of Lodi website at: <http://www.lodi.gov/city-council/AgendaPage.html> to view Staff Reports and Minutes from the corresponding meeting date.

Date	Meeting	Title
July 16, 2008	REGULAR	Provide Direction with Regard to Potential Revisions and Administrative Interpretations to Mobile Food Vending Ordinance, Lodi Municipal Code Section 9.18, "Vending on Streets, sidewalks and Private Property"
		Adopt Resolution Approving a Five Percent Pay Increase for the Community Development Director Position
July 29, 2008	SHIRTSLEEVE	Receive Information Regarding Leadership in Energy and Environmental Design- Neighborhood Development (LEED-ND).
August 5, 2008	SHIRTSLEEVE	Receive Information Regarding Amending the Section of the Lodi Zoning Ordinance Dealing with Site Plan and Architectural Review.
August 6, 2008	REGULAR	Authorize the City Manager to Enter into a Contract with: 1) PBS&J to Provide Planning Services and Prepare an Environmental Impact Report; and 2) PMC, Inc., to Provide Project Management Services for a Proposed Sutter Gould Medical Facility at West Lane and Harney Lane to be Reimbursed by Applicant.
		Receive Information Regarding New Meeting Day for the Site Plan & Architectural Review Committee.
		Adopt Resolution Opposing State Budget Decisions that Would "Borrow" Local

**Continued on other side**

<p>August 6, 2008 – continued</p>		<p>Government and Transportation Funds.</p> <p>Adopt Resolution to Implement the Storm Water Development Standard Plans as Required by the State Water Resources Control Board and Set Inspection Fee for Post Construction Best Management Practices as Required in the Standards.</p>
<p>August 12, 2008</p>	<p>SHIRTSLEEVE</p>	<p>Review Transit Oriented Development Design Guidelines.</p>