

<p>CARNEGIE FORUM 305 WEST PINE STREET LODI, CALIFORNIA</p>	<p>AGENDA LODI PLANNING COMMISSION</p>	<p>REGULAR SESSION WEDNESDAY, AUGUST 8, 2012 @ 7:00 PM</p>
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For information regarding this agenda please contact:

Kari Chadwick @ (209) 333-6711
Community Development Secretary

NOTE: All staff reports or other written documentation relating to each item of business referred to on the agenda are on file in the Office of the Community Development Department, located at 221 W. Pine Street, Lodi, and are available for public inspection. If requested, the agenda shall be made available in appropriate alternative formats to persons with a disability, as required by Section 202 of the Americans with Disabilities Act of 1990 (42 U.S.C. Sec. 12132), and the federal rules and regulations adopted in implementation thereof. To make a request for disability-related modification or accommodation contact the Community Development Department as soon as possible and at least 24 hours prior to the meeting date.

1. ROLL CALL
2. MINUTES – “July 11, 2012”
3. PUBLIC HEARINGS
 - a. Request for Planning Commission to Recommend to the City Council to Approve Growth Management Allocations to Permit and Construct 12 Residential Units at 2110 Tienda Drive. (Applicant: John Giannoni; File No: 12-GM-01)
 - b. Request for Planning Commission Approval of a Use Permit and a SPARC Review to Allow Development of a Gas Station with 8-Dispenser Canopy, 3,078 Square Foot Convenience Store with sale of beer and wine (Type-20), and a drive through carwash facility on a .94-acre site located at 255 East Harney Lane. (Applicant: Peter Tobin, on behalf of Hardev Singh Gill; File Number: 12-U-06 and 12-SP-02)
 - c. Request for Planning Commission approval of a Use Permit to allow a Type 2 (Winery) Alcoholic Beverage Control license at 20 West Elm Street. (Applicant: Erin Taylor, on behalf of Riaza Wines, LLC; File Number: 12-U-11)

NOTE: The above item is a quasi-judicial hearing and requires disclosure of ex parte communications as set forth in Resolution No. 2006-31

4. PLANNING MATTERS/FOLLOW-UP ITEMS
5. ANNOUNCEMENTS AND CORRESPONDENCE
6. ACTIONS OF THE CITY COUNCIL
 - a. Council Summary Memo
7. DEVELOPMENT CODE UPDATE
 - a. Staff presentation on the Draft Development Code Section 2, Commercial and Industrial Districts
8. ACTIONS OF THE SITE PLAN AND ARCHITECTURAL REVIEW COMMITTEE
9. ART IN PUBLIC PLACES
10. COMMENTS BY THE PUBLIC (NON-AGENDA ITEMS)
11. COMMENTS BY THE PLANNING COMMISSIONERS & STAFF (NON-AGENDA ITEMS)
12. ADJOURNMENT

Pursuant to Section 54954.2(a) of the Government Code of the State of California, this agenda was posted at least 72 hours in advance of the scheduled meeting at a public place freely accessible to the public 24 hours a day.

****NOTICE:** Pursuant to Government Code §54954.3(a), public comments may be directed to the legislative body concerning any item contained on the agenda for this meeting before (in the case of a Closed Session item) or during consideration of the item.

Right of Appeal:

If you disagree with the decision of the commission, you have a right of appeal. Only persons who participated in the review process by submitting written or oral testimony, or by attending the public hearing, may appeal.

Pursuant to Lodi Municipal Code Section 17.72.110, actions of the Planning Commission may be appealed to the City Council by filing, within ten (10) business days, a written appeal with the City Clerk and payment of \$300.00 appeal fee. The appeal shall be processed in accordance with Chapter 17.88, Appeals, of the Lodi Municipal Code. Contact: City Clerk, City Hall 2nd Floor, 221 West Pine Street, Lodi, California 95240 – Phone: (209) 333-6702.

**LODI PLANNING COMMISSION
REGULAR COMMISSION MEETING
CARNEGIE FORUM, 305 WEST PINE STREET
WEDNESDAY, JULY 11, 2012**

1. CALL TO ORDER / ROLL CALL

The Regular Planning Commission meeting of July 11, 2012, was called to order by Chair Olson at 7:00 p.m.

Present: Planning Commissioners – Heinitz, Hennecke, Kirsten, Kiser, and Chair Olson

Absent: Planning Commissioners – Cummins and Jones

Also Present: Community Development Director Konradt Bartlam, Associate Planner Immanuel Bereket, Deputy City Attorney Janice Magdich, and Administrative Secretary Kari Chadwick

2. MINUTES

“June 13, 2012”

MOTION / VOTE:

The Planning Commission, on motion of Commissioner Kiser, Heinitz second, approved the Minutes of June 13, 2012 as written. (Commissioner Kirsten abstain because he was not in attendance of the subject meeting)

3. PUBLIC HEARINGS

- a) Notice thereof having been published according to law, an affidavit of which publication is on file in the Community Development Department, Chair Olson called for the public hearing to consider the request for Planning Commission to amend approved Use Permit application 12-U-07 to allow Type 21 Off-Sale General and Type 42 On-Sale Beer and Wine Alcoholic Beverage Control license at 2418 West Kettleman Lane. (Applicant: Denise Rowman, on behalf of Beverages and More, Inc. File Number: 12-U-07a)

Associate Planner Bereket gave a brief PowerPoint presentation based on the staff report. Staff recommends approval of the project.

Commissioner Heinitz asked if BevMo will have to go through the same ABC training as other types of bars or wine bars. Mr. Bereket stated that they will have to meet all the standards and participate in the necessary training that all other ABC applicants attend.

Hearing Opened to the Public

- Mike Klein, applicant representative, came forward to answer questions.
- Commissioner Heinitz asked if the other stores have the same conditions. Mr. Klein stated that all of the stores offer this service. Heinitz asked what the minimum age of employees and if all the employees attend the training. Mr. Klein stated that all employees must be at least 21 years of age and will attend the ABC training.

Public Portion of Hearing Closed

- MOTION / VOTE:

The Planning Commission, on motion of Commissioner Kiser, Hennecke second, approved the request of the Planning Commission to amend approved Use Permit application 12-U-07 to allow Type 21 Off-Sale General and Type 42 On-Sale Beer and Wine Alcoholic Beverage

Control license at 2418 West Kettleman Lane subject to the conditions in the resolution. The motion carried by the following vote:

Ayes: Commissioners – Heinitz, Hennecke, Kirsten, Kiser and Chair Olson
Noes: Commissioners – None
Absent: Commissioners - Cummins and Jones

- b) Notice thereof having been published according to law, an affidavit of which publication is on file in the Community Development Department, Chair Olson called for the public hearing to consider the request of the Planning Commission for a Use Permit to establish a warehousing and storage use for recycled materials, including California Redemption Value (CRV), cardboard, scrap metal and electronic waste at 235 North Washington Street (Applicant: Fawad Ebrahimi, on behalf of Go Green Recycling Inc: File Number: 12-U-09)

Associate Planner Bereket gave a brief PowerPoint presentation based on the staff report. Staff recommends approval of the project.

Hearing Opened to the Public

- Fawad Eberhimi, applicant, came forward to answer questions.
- Commissioner Hennecke asked if there was any electronic waste breakdown on the site. Mr. Abrahami stated that the Lodi sites do not accept e-waste at this time.
- Commissioner Heinitz stated his appreciation that the current site is very well maintained.
- Bill Anderson, resident of Lodi, came forward to support the project.
- Shelly Stagho, resident of Lodi, came forward to support the project.

Public Portion of Hearing Closed

- MOTION / VOTE:
The Planning Commission, on motion of Commissioner Kiser, Heinitz second, approved the request for a Use Permit to establish a warehousing and storage use for recycled materials, including California Redemption Value (CRV), cardboard, scrap metal and electronic waste at 235 North Washington Street subject to the conditions in the resolution. The motion carried by the following vote:

Ayes: Commissioners – Heinitz, Hennecke, Kirsten, Kiser and Chair Olson
Noes: Commissioners – None
Absent: Commissioners - Cummins and Jones

- c) Notice thereof having been published according to law, an affidavit of which publication is on file in the Community Development Department, Chair Olson called for the public hearing to consider the request of the Planning Commission for a Use Permit to allow establishment of an automotive body/fender repair and paint shop at 21 North Stockton Street. (Applicant: Delia Rodriguez. File Number: 12-U-10)

Associate Planner Bereket gave a brief PowerPoint presentation based on the staff report. Staff recommends approval of the project.

Commissioner Heinitz asked why it is necessary for this to come before the Commission. Mr. Bartlam stated that the painting of the automobiles is what is triggering the Use Permit.

Commissioner Hennecke asked where the paint booth is going to be located on the floor plan. Mr. Bartlam stated that the paint booth will be one of the drop-in types and the exact location

hasn't been determined. The applicant will have to go through the permitting process before installing the booth.

Chair Olson asked if the applicant has started the Air Resources Board application or Building permit process. Mr. Bartlam stated that Staff has suggested not starting that process since the Planning Commission is the first hurdle and if the Commission doesn't support the project then the process can't continue.

Commissioner Kiser asked if the building is going to have to be brought up to current code to accommodate the addition of the paint booth. Mr. Bartlam stated that the Building Department Plans Examiner will make that determination when reviewing the Tenant Improvement Plans.

Hearing Opened to the Public

- Florentino Melendrez, owner of the building, came forward to answer questions. Mr. Melendrez stated that there was a booth there when he bought the building, but it was not done with the proper permits, so the City made him take it out.
- Commissioner Heinitz asked if this is something that should be grandfathered-in. Mr. Bartlam stated that the booth was not registered previously and would not have passed code.
- Chair Olson asked if Mr. Melendrez was the owner of the building or the owner of the business. Mr. Melendrez stated that he is the owner of the building not the business.
- Commissioner Kirsten asked if he could see the letter from the San Joaquin County Air Quality Board that Mr. Melendrez referred to.

Public Portion of Hearing Closed

The Commission took a moment to look at the letter. A copy was not supplied to staff for the record.

- Commissioner Hennecke asked if the paint booth was required to be removed by the City. Mr. Bartlam stated that there are several staff members involved and which one initiated the removal conversation.

MOTION / VOTE:

The Planning Commission, on motion of Commissioner Heinitz, Kiser second, approved the request of the Planning Commission for a Use Permit to allow establishment of an automotive body/fender repair and paint shop at 21 North Stockton Street subject to the conditions in the resolution. The motion carried by the following vote:

Ayes: Commissioners – Heinitz, Hennecke, Kirsten, Kiser and Chair Olson
 Noes: Commissioners – None
 Absent: Commissioners - Cummins and Jones

4. PLANNING MATTERS/FOLLOW-UP ITEMS

None

5. ANNOUNCEMENTS AND CORRESPONDENCE

None

6. ACTIONS OF THE CITY COUNCIL

Director Bartlam stated that there has been a memo provided in the packet and staff is available to answer any questions.

7. DEVELOPMENT CODE UPDATE

- a. Staff presentation on the Draft Development Code Section 2, Residential and Mixed Use Districts.

Director Bartlam gave a PowerPoint presentation based on the staff report. The current code was adopted back in 1954 and has been amended several times throughout the years. In 1999 the process was started to update the code, but it was put on hold due to staffing and other higher priorities. Then in 2006/7 the process was started again and put on hold once again because the General Plan took priority. Now the General Plan is done and it is necessary to update the code to fit with the general plan land uses that have been created. The Commission will be asked to adopt a new Zoning Map at the end of this process also.

Commissioner Heinitz asked about the parks and basin style parks and the plan to develop a series of waterways, bridges, and fountains along the west side of Lodi. Should this be something that is included in this plan or does the Commission have to wait for a developer to bring it back. Mr. Bartlam stated that the General Plan addresses that on a broad policy position. The City is moving away from the combined basins and parks because of the State filtration regulations. The DeBenedetti Park is an example of the type of basin/park that the city will be moving toward, with a lower basin area that will have water for longer periods of time and a higher area that will remain relatively dry and usable for park purposes. The Westside and Southwest Gateway proposed plans have a series of neighborhood parks as part of their development plans.

Commissioner Kiser asked if the filtration will be with the use of purple pipe and recirculation of the water for other uses. Mr. Bartlam stated that isn't the intent and isn't a requirement with this code.

Chair Olson asked about the ratio relating to floor area. Mr. Bartlam stated that the ratio should be considered in the Commercial and Industrial areas and will depend on the site and use. It needs to be shown in the Mixed Use areas, but isn't really meant for Residential uses.

Commissioner Hennecke asked if street sizes are being addressed in this code. Mr. Bartlam stated that the size of the streets will be discussed in the Public Works Standards. It is the intent of Staff to bring those Standards back once the Development Code Update is complete.

Opened for Public Comment

- Steve Hailey, representative for Farmers and Merchants Bank, came forward to ask what will be the process to present ideas or concerns. Mr. Bartlam stated that viable options for getting your comments or concerns heard by staff are by making comments to the Commission here, submitting comments in writing to the Commission and/or to the Planning Division, and then there is the opportunity to make an appointment to speak with Immanuel Bereket or himself in person.
- Delores Androsky, property owner on Forest Avenue, came forward to ask if some of the older structures would be exempt from the new rules if they can't meet the requirements. Mr. Bartlam stated that non-conforming uses are not being created with this code change, but will more than likely offer more opportunities rather hindrances' for Ms. Androsky's properties.
- Chair Olson asked how long this code is intended to last. Mr. Bartlam stated that 25 to 30 years or at least as long as the General Plan.
- Commissioner Heinitz asked if the old drafts are being dusted off and used or completely put aside. Mr. Bartlam stated that design guidelines will be a part of this development code. It isn't the intent of staff to change the work that has already been done, but to make sure that the current code does not conflict with the current General Plan.
- Commissioner Kiser asked if there was going to be a historic area created. Mr. Bartlam stated there will not.

Closed to Public Comment

8. ACTIONS OF THE SITE PLAN AND ARCHITECTURAL REVIEW COMMITTEE

None

9. ART IN PUBLIC PLACES

Vice Chair Kirsten gave a brief report regarding the last meeting. The Taco Truck Cook-Off was a success.

10. COMMENTS BY THE PUBLIC

None

11. COMMENTS BY STAFF AND COMMISSIONERS

Commissioner Hennecke asked if there has been any word from the Wal Mart group. Mr. Bartlam stated that he is meeting with Mr. Browman and the reps from Wal Mart next week. The Supreme Court has extended the appeal date to August 23, 2012.

Commissioner Kiser asked if a tenant has been found for the current store. Mr. Bartlam stated that he isn't aware, but will be asking Mr. Browman when they meet.

Vice Chair Kirsten asked if the legal issue can be discussed. Deputy City Attorney Janice Magdich stated that the issue is a procedural issue of when the Attorney/Client privilege should have been raised to the court by the opponent.

Director Bartlam stated that the winning truck from the Taco Truck Cook Off will be at the Farmers Market tomorrow night.

12. ADJOURNMENT

There being no further business to come before the Planning Commission, the meeting was adjourned at 8:10 p.m.

ATTEST:

Konradt Bartlam
Planning Commission Secretary

Item 3a

**CITY OF LODI
PLANNING COMMISSION
Staff Report**

MEETING DATE: August 8, 2012

APPLICATION NOS: 12-GM-01

REQUEST: Request for Planning Commission to Recommend to the City Council to approve Growth Management Allocations to permit and construct 12 residential units at 2110 Tienda Drive. (Applicant: John Giannoni; File No: 12-GM-01)

LOCATION: 2110 Tienda Drive
(APN: 027-410-19)
Lodi, CA 95242

APPLICANT: John Giannoni
2111 W. Kettleman Lane, Suite D
Lodi, CA 95242

PROPERTY OWNER: John and Kerry Giannoni
2960 Applewood Drive
Lodi, CA 95242

RECOMMENDATION

Staff recommends that the Planning Commission approve the request of John Giannoni recommending that the City Council award 12 medium density growth management allocation units (12-GM-01), subject to the conditions in the attached resolution.

PROJECT/AREA DESCRIPTION

General Plan Designation Mixed Use Corridor
Zoning Designation R-C-P, Residential, Commercial and Professional
Project Size 0.81 acre (35,284 sq. ft)

	ADJACENT ZONING DESIGNATIONS AND LAND USES		
	GENERAL PLAN	ZONING CLASSIFICATION	EXISTING LAND USE
North	Low Density Residential	R-2, Single Family Res.	Residential uses
South	Mixed Use Corridor	R-C-P, Residential, Commercial, Professional	Office Use
East	Mixed Use Corridor	R-C-P, Residential Commercial, Professional	Church
West	Mixed Use Corridor	R-C-P, Residential, Commercial, Professional	Recently constructed duplexes

SUMMARY

The proposed project would permit the construction of 12 medium density residential units on .81-acre parcel located on Tienda Drive, one block north of W. Kettleman Lane. Per the City's Growth Management Ordinance, residential construction development of 5 or more units must secure Growth Management allocations. The property is a fully improved vacant lot and allows residential development units up to 35 dwelling units per acre. The area surrounding the project site is fully developed with a variety of single and multi-family residences and office/institutional uses.

BACKGROUND

The Planning Department received one residential growth management application for the year 2012. This is the first growth management application received since 2006. There are three other separate projects that were granted allocations in 2006 by the City Council through the use of Development Agreements that grant multi-year allocations that began in 2007. These projects are Reynolds Ranch, Southwest Gateway and Westside developments. The latter two projects are seeking to dissolve their Development Agreements. In the event the Development Agreements are dissolved, their growth allocations would be void and each project would have to submit a growth management application going forward.

The proposed development, known as The Villas at Sunwest, is consistent with the General Plan land use designation for the project site. The project includes a total of 12 attached single-family residential lots ranging from 3,092 to over 4,392 square feet in size. The project is located on approximately 0.81 acre (35,284 sq. ft) bounded generally by Tienda Drive on the north and an office development on the south, with Kettleman Lane located just south of the project site. The site is vacant lot with most off-site improvements, including utility lines, have been installed.

ANALYSIS

The City Council adopted the Growth Management Allocation Ordinance (GMAO) in 1991 to regulate the growth, location, amount and timing of residential developments in the City. The GMAO applies to any new residential development project dwelling that adds five (5) or more new housing stock to the City. The GMAO does not regulate non-residential development, senior citizen housing, residential remodels or additions, or demolition and construction of new homes on the same site. Second residential units, condominium conversions and special care/senior facilities are also exempt. The GMAO allocation award is based on a competitive permit allocation system with points given based on site specific resource protection measures.

The allocation system gives priority through point assignments to projects that reduce impacts on services, infrastructure, and resources. The ordinance sets an annual growth limit of two percent of the City's population, compounded annually. Once the amount of allocation units is figured, the City requires that the allocation units be distributed among housing types as follows; 65 percent low density, 10 percent medium density and 25 percent high density. For example, the following explains the 447 units available for 2012:

1. Calculate two percent of the City's current population: $62,825 \times 2\% = 1,257$.
2. Divide 1,257 by the average number of persons per household $1,257/2.812 = 447$
3. Divide the 447 units into the 3 housing types:
 - 65% low density = 290 units
 - 10% medium density = 45 units
 - 25% high density = 112 units

As indicated above in the background discussion, the present project is being reviewed for growth management allocations for 2012. There are also three projects that received allocations through Development Agreements (Southwest Gateway, Westside and Reynolds Ranch development). Southwest Gateway and Westside are entirely residential development of various densities and types. The Reynolds Ranch project is a mixed-use development with various types and densities of residential development. Southwest Gateway and Westside projects are on-hold due to the economy and no allocation has been utilized. Extensive commercial development activities have occurred at the Reynolds Ranch project. The residential component of the project has not begun and no allocation has been utilized.

The applicant, Mr. Giannoni, has submitted application for a total of 12 medium density growth management allocation units (8-20 units/acre). There are 45 medium density allocation units allocated for 2012; however, the City also has medium density growth management units that were not issued in

prior years as shown in Table A below, which leaves a “bank of units” from previous years. The 45 medium density allocations available for 2012 will be sufficient to provide for the proposed project.

Table A: Growth Management Allocation History

Density	Available Allocations				
	Scheduled from 1989-2012	Granted from 1989-2011	Remaining from 1989-2011	2% Allocations for 2012	Total Available for 2012
Low (0.1-7)	6,648	2,893	3,482	290	3,772
Medium (7.1-20)	1,023	438	615	45	660
High (20.1-30)	2,557	0 ^a	2,452	112	2,564
TOTAL	10,228	3,331	6,549	447	4,278

^a There have been high density allocations granted over the past 20 years; however, they have expired or withdrawn prior to issuance of building permits.

Priority Location Map and Point System

The Growth Management Ordinance includes a priority location map and a point system to assist the City with prioritizing issuance of growth management allocations. The priority location map designates lands available for development and provides development categories of one, two or three, with Priority Area 1 being the first priority area for development. The priority areas are based on availability of city services (e.g., water, wastewater, storm drains, streets, police, fire and parks). The proposed project site is classified as an in-fill project and is not included in the Priority Area map. However, for scoring purposes in-fill projects are considered Priority Area 1 projects. The point system was established to rate projects based on various project merits in order to determine if one project should be approved before another, particularly if there are more allocation requests than there are available allocations. Since this is the only allocation request submitted, and there is surplus of inventory accumulated over the years, scoring methodology wasn't performed as it was unnecessary.

Growth Management Allocation Recommendation

The proposed project site is located in In-fill location. The project is in an area that is fully developed with residential and office/commercial uses. The surrounding uses are suitable and consistent with the type of development proposed by the applicant. For these reasons, staff recommends approval of the request for 12 medium density growth management allocation units for the project subject to the conditions outlined in the attached resolution. It should be noted that securing the 12 growth management allocations does not guarantee that this number of units can be built. The applicant must still demonstrate, via SPARC review process, they can build a 12-unit project that meets all City development and design requirements. Pursuant to City Code § 17.81.030, residential building proposed to be erected in the R-C-P Zoning District are subject to SPARC review and approval. Staff has proposed a condition requiring the applicant submit a detailed development plan showing exact dimensions and building details.

General Plan and Zoning Conformance

The request of 12-units does not require any zoning change. The property is zone R-C-P, Residential, Commercial and Professional, which allows multi-family residences. The project site has a General Plan land use designation of Mixed Use Corridor (MUC). The proposed project is consistent with the existing General Plan designation of Mixed Use Corridor (MUC) and the proposed density of 20-units per acre is within the MUC density range of 7.1-20 dwelling units per acre.

Discussion of Proposed Development Plan

Prior to the approval of the project, a development plan must be reviewed and recommended for approval by the Planning Commission. Once approved, the project site must be developed in accordance with the approved development plan. The applicant has submitted a conceptual development plan depicting the proposed layout and design for the 12 unit project. The Planning Commission can approve GM allocations based on this plan if they feel the proposal is generally

acceptable. However, prior to final approval of an actual construction permit, the applicant must submit a final development plan (SPARC application) that details all the required components of the project with exact dimensions, architectural details and landscape plans. This plan must meet all requirements of the zoning ordinance, including setbacks, parking area dimensions, lot coverage, etc.

The development plan shows 12 townhouse units arranged in two rows facing each other. Each unit will have 1,710 square feet of living space on two floors with half the units having an attached 400 square foot two-car garage and the rest having a one-car garage and one-car uncovered parking space adjacent to the units. Access to the property will be from a driveway from Tienda Drive connected to a central driveway providing access to the individual garages. The driveway is a dead-end access so vehicles will have to turn around to exit the property. The plan illustrates a property lines separating each unit and also forming a common facilities such as roofs, driveway, etc. Detailed site analyses and review will be performed when the applicant submits a SPARC/Development plan review application.

The City Council has final action on the requests for Growth Management Allocations; however, all growth management applications requests must first be reviewed by the Planning Commission with a recommendation forwarded to the City Council. Therefore, staff recommends that unless additional or contrary information is received during the public hearing the Planning Commission recommend approval of the Growth Management Allocation application (12-GM-01) to permit 12 growth management allocation units.. Because the City hasn't had growth management allocation requests since 2006, surplus allocation has been accumulated in the amount of 660 medium density available units. The applicant's request for 12 medium density units can be accommodated.

ENVIRONMENTAL ASSESMENT

The project qualifies for a CEQA Categorical Exemption, Section 15332, In-Fill Development Project, Class 32. The project is consistent with the General Plan, is located in the City limits, is less than 5-acres in size and is surrounded by existing urban uses. The project site is not a habitat for any rare or endangered species of plant or wildlife, and the project will not create a significant environmental impact.

PUBLIC HEARING NOTICE

A legal notice for the Growth Management Allocation Application was published on July 27, 2012 in the Lodi News Sentinel. Twenty eight (28) public hearing notices were sent to all property owners of record within a 300-foot radius of the subject property.

ALTERNATIVE PLANNING COMMISSION ACTIONS

- Recommend Approval of the Request with Alternate Conditions
- Recommend Denial of the Request
- Continue the Request

Respectfully Submitted,

Concurred by:

Immanuel Bereket
Associate Planner

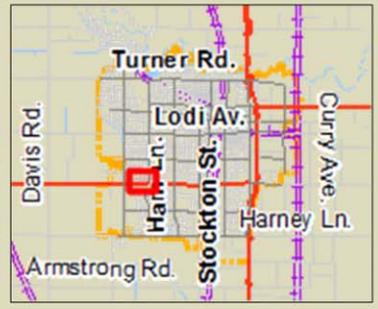
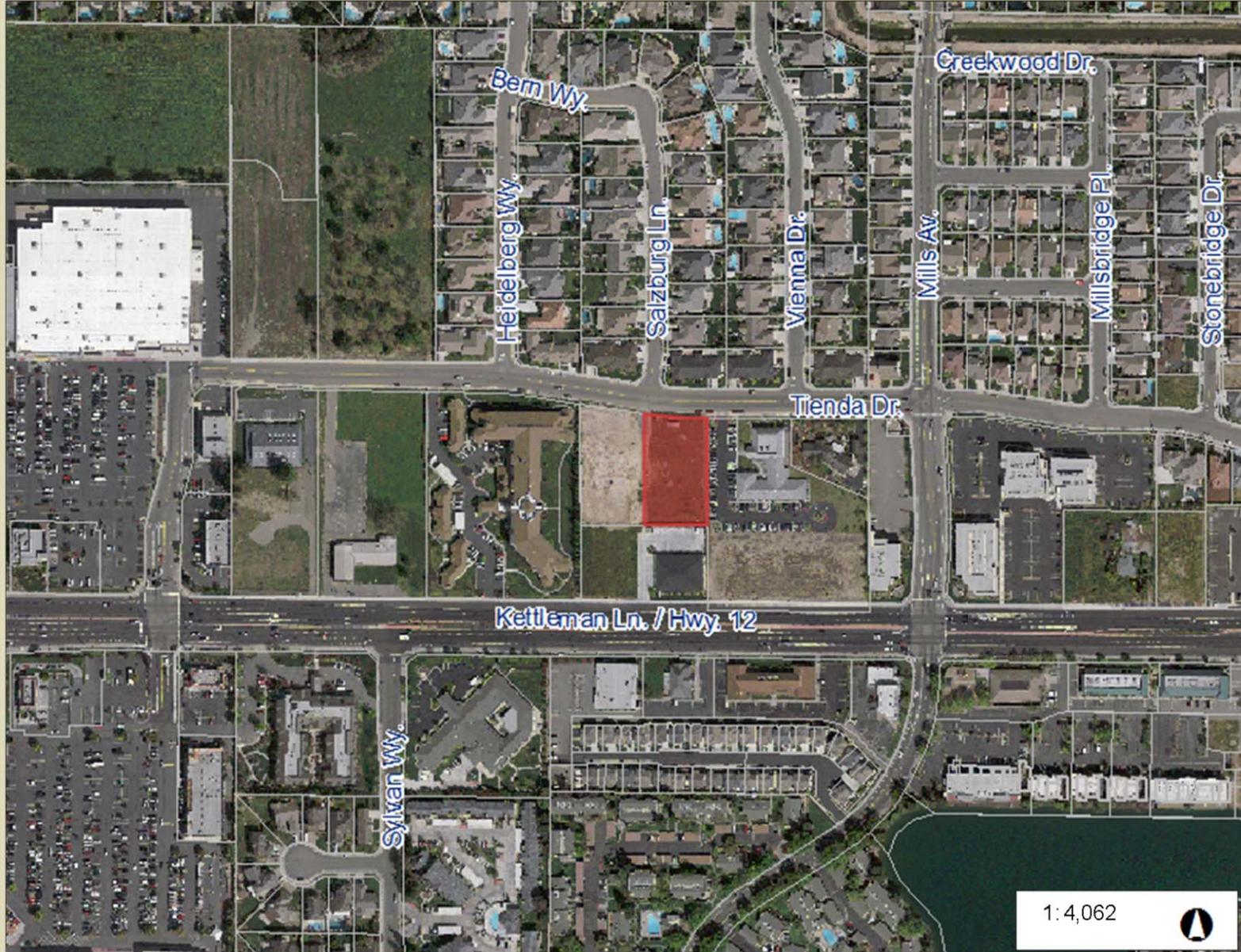
Konradt Bartlam
Community Development Director

ATTACHMENTS:

1. Vicinity Map
2. Aerial Photo
3. Site Plan
4. Growth Management Table
5. Draft Resolution for Growth Management Allocations



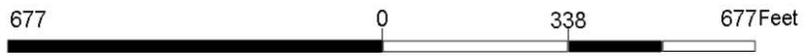
Aerial Map



Legend

- Project Site
- Parks
- City Limit

1: 4,062



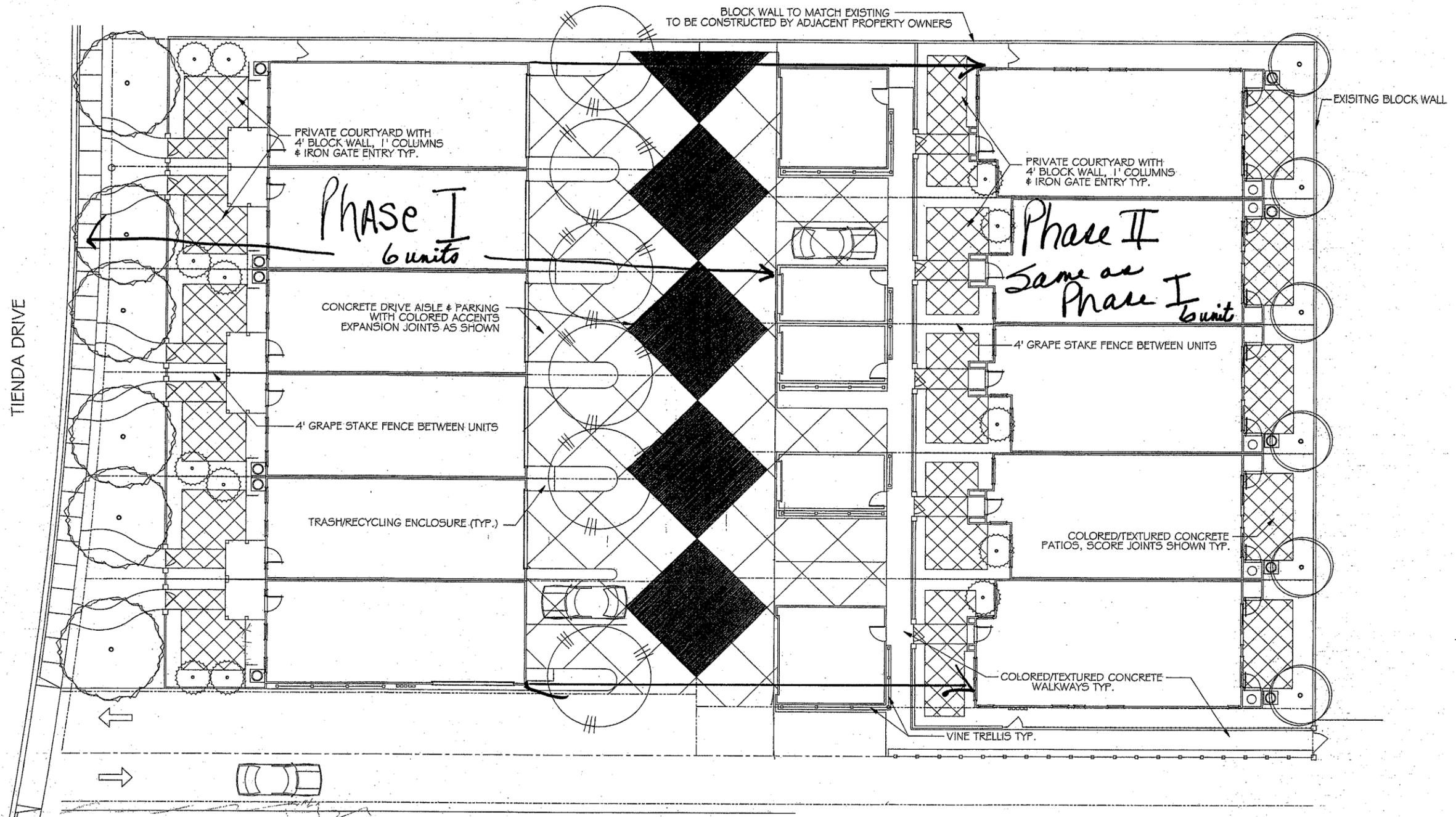
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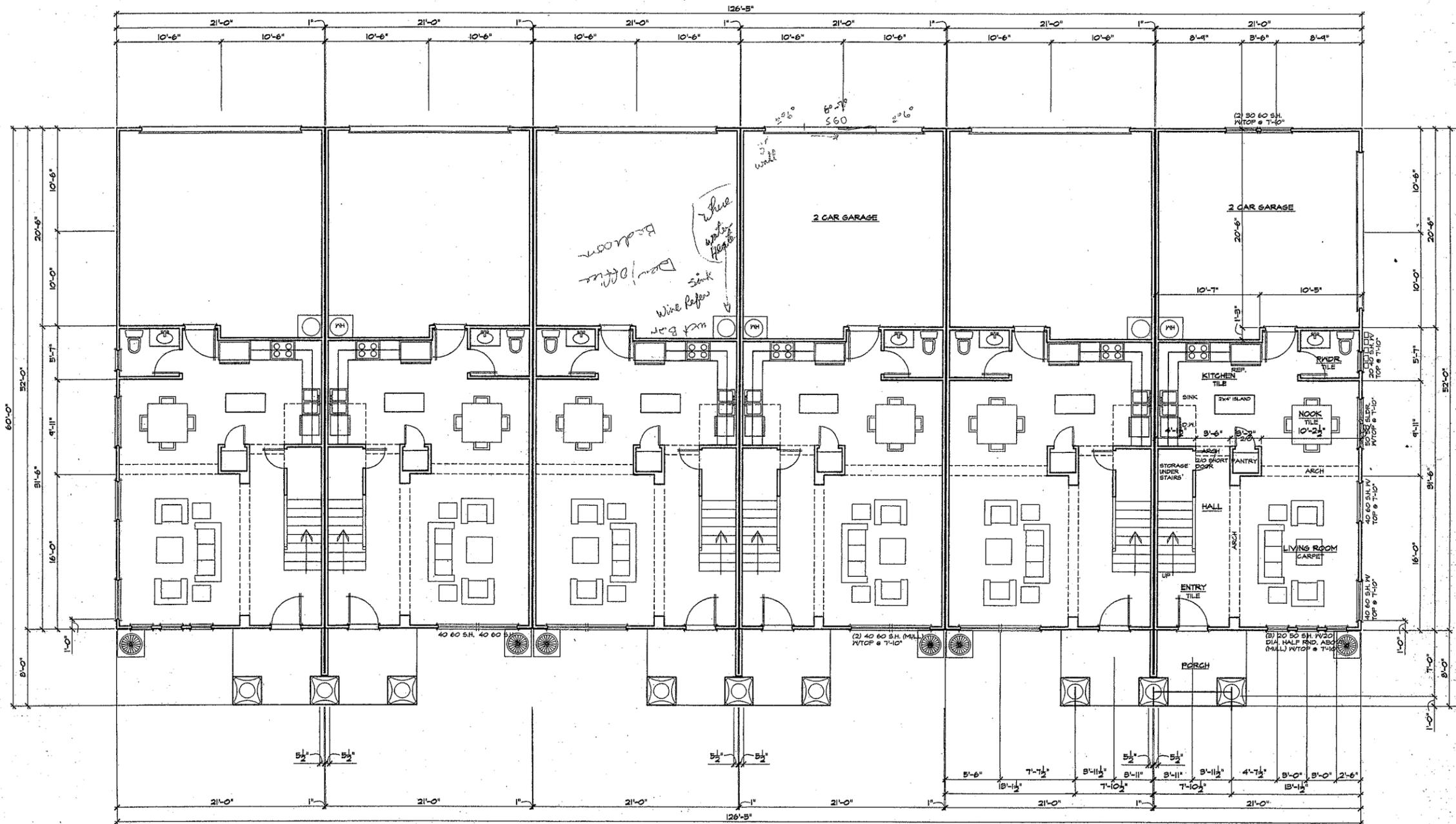
Notes

2110 Tienda Drive
(APN: 027-410-19)
Lodi, CA 95242



PRELIMINARY PLANT LEGEND

Symbol	Size	Botanical Name	Common Name	Quantity
	15 Gal.	Lagerstroemia indica	Crape Myrtle	
	15 Gal.	Pistacia chinensis	Chinese Pistache	
	15 Gal.	Pyrus c. 'Krauter Vesuvius'	Flowering Pear	
	15 Gal.	Ulmus parvifolia	Chinese Evergreen Elm	
SHRUBS				
	5 Gal.	Abelia grandiflora	Glossy Abelia	
	5 Gal.	Berberis t. 'Atropurpurea'	Red-Leafed Japanese Barberry	
	5 Gal.	Buxus japonicus	Japanese Boxwood	
	5 Gal.	Coleonema p. 'Sunset Gold'	Gold Breath of Heaven	
	5 Gal.	Euonymus j. 'Aureo-Marginata'	Gold Leaved Euonymus	
	5 Gal.	Lavandula dentata	French lavender	
	5 Gal.	Loropetalum 'Razalberry'	Chinese Fringe Flower	
	5 Gal.	Phormium t. 'Bronze Baby'	New Zealand Flax	
	5 Gal.	Fittosporum t. 'Wheeler's Dwarf'	Dwarf Tobira	
	5 Gal.	Rhaphiolepis t. 'Springtime'	India Hawthorn	
GROUNDCOVERS AND ACCENTS				
	1 Gal.	Dietes vegeta	Fortnight Lily	
	1 Gal.	Ficus pumila	Creeping Fig	
	1 Gal.	Gazania hybrid (clumping)	Clumping Gazania	
	1 Gal.	Hemerocallis 'Stella de Oro'	Day Lily	
	1 Gal.	Myoporum parvifolium	Myoporum	
	1 Gal.	Solanum jasminoides	Potato Vins	
	1 Gal.	Trachelospermum jasminoides	Star Jasmine	



FIRST FLOOR PLAN
SCALE: 1/4" = 1'-0"

UNIT "C"
644 SQ. FT.

Giannoni Dev.

Year	Population	% Actual Growth	102% Pop. projection	Persons/ Household	Total units per year	Single Fam. @ 65%	Med density @ 10%	High Density @ 25%
** Sep-89	50,991		1,020	2.795	397	258	40	99
Sep-90	52,011	2.00%	1,040	2.806	404	263	40	101
Sep-91	53,051	2.00%	1,061	2.817	403	262	40	101
Jan-92	53,186	0.25%	1,064	2.828	399	259	40	100
Jan-93	53,701	0.97%	1,074	2.839	401	261	40	100
Jan-94	53,903	0.38%	1,078	2.850	402	261	40	101
Jan-95	54,694	1.47%	1,094	2.861	406	264	41	102
Jan-96	54,473	-0.40%	1,089	2.872	409	266	41	102
Jan-97	54,812	0.62%	1,096	2.883	412	268	41	103
Jan-98	55,681	1.59%	1,114	2.894	415	270	42	104
Jan-99	56,926	2.24%	1,139	2.905	423	275	42	106
Jan-00	57,935	1.77%	1,159	2.916	428	278	43	107
Jan-01	58,600	1.15%	1,172	2.927	432	281	43	108
Jan-02	59,431	1.42%	1,189	2.938	433	282	43	108
Jan-03	60,521	1.83%	1,210	2.949	437	284	44	109
Jan-04	60,769	0.41%	1,215	2.959	440	286	44	110
Jan-05	62,467	2.79%	1,249	2.970	448	291	45	112
Jan-06	62,817	0.56%	1,256	2.789	419	272	42	105
Jan-07	62,820	0.00%	1,256	2.790	450	293	45	113
Jan-08	63,362	0.86%	1,267	2.792	454	295	45	113
Jan-09	63,313	-0.08%	1,266	2.745	461	300	46	115
Jan-10	63,549	0.37%	1,271	2.762	460	299	46	115
Jan-11	62,344	-1.90%	1,247	2.791	447	290	45	112
Jan-12	62,825	0.77%	1,257	2.812	447	290	45	112
TOTALS TO 2012:					10,228	6,648	1,023	2,557

CITY COUNCIL AWARDED BUILDING PERMIT ALLOCATIONS 1989-PRESENT

TOTAL RESIDENTIAL UNITS (1989 TO 2012) = 10,228

SINGLE FAMILY (10,228*65%)=6,648

Available Single Family Residences for allocation				6,648.20																				ALLOCATION	
PROJECT	1989	1990	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	TOTALS
ALMOND WOOD ESTATES	0	0	0	0	0	0	0	0	0	0	0	0	74	0	0	0	0	0	0	0	0	0	0	0	74
ALMOND NORTH	0	0	0	0	0	0	0	0	0	0	0	0	0	0	34	0	0	0	0	0	0	0	0	0	34
BANG'S RANCH	34	35	35	0	0	19	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	123
BECKMAN PROPERTY	0	0	0	0	0	0	0	0	0	0	0	46	0	0	0	0	0	0	0	0	0	0	0	0	46
BRIDGETOWN	0	0	0	0	0	0	53	51	36	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	140
CENTURY MEADOWS 1	16	16	16	0	0	0	52	55	45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	200
CENTURY MEADOWS 2	25	26	25	0	29	0	0	0	60	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	165
CENTURY MEADOWS 3	24	24	25	0	29	0	51	50	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	203
CENTURY MEADOWS 4	29	29	29	33	0	0	0	0	0	0	17	0	0	0	0	0	0	0	0	0	0	0	0	0	137
COLVIN RANCH	20	20	20	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	60
FUGAZI BROTHERS	0	0	0	0	5	0	0	0	0	-5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
JOHNSON RANCH 2	43	43	43	44	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	173
KENNETH TATE	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	0	0	0	0	6
KIRST PROPERTY	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	0	0	0	0	0	6
LEGACY ESTATES 2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	141	0	0	0	0	0	0	0	0	0	141
LODI ESTATES	6	7	6	46	0	35	0	0	0	-100	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
LODI WEST	26	27	27	80	55	69	0	0	53	41	0	0	0	0	0	0	0	0	0	0	0	0	0	0	378
LUCKEY/LACKYARD	0	0	0	0	0	0	0	0	0	0	0	0	77	0	0	0	0	0	0	0	0	0	0	0	77
MILLSBRIDGE 2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	28	0	0	0	0	0	0	0	0	0	28
PARISIS PROPERTY	0	0	0	0	0	0	39	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	39
PERLEGOS PROPERTY	0	0	0	0	0	0	0	0	0	0	0	57	0	0	0	0	0	0	0	0	0	0	0	0	57
PROF. CONSTRUCTORS INC.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	0	0	0	0	7
RICHARDS RANCH ***	0	0	0	0	34	0	0	15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	49
RIVERPOINTE	0	0	0	0	0	44	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	44
SASAKI PROPERTY	0	0	0	0	0	0	0	0	0	60	0	0	0	0	0	0	0	0	0	0	0	0	0	0	60
SUNWEST XIV	0	0	0	0	0	0	0	31	36	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	67
THAYER PROPERTY	0	0	0	0	0	0	34	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	34
TSUTAOKA PROPERTY	0	0	0	0	0	0	0	63	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	63
TOWNE RANCH	35	36	36	56	52	151	37	0	6	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	415
VINTAGE OAKS	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	15	0	0	0	0	0	0	0	0	15
VINTNER'S SQUARE	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	52	0	0	0	0	0	0	0	0	52
	258	263	262	259	204	318	266	265	236	2	17	103	151	0	209	80	0	0	0	0	0	0	0	0	2,893

EXPIRED

MEDIUM DENSITY

MEDIUM DENSITY (10,228*10%) = 1,023

Available Medium Density Residences for allocation																	ALLOCATION									
PROJECT	1989	1990	1991	1992	1,022.80	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	TOTALS
BANG'S RANCH **	18	18	0	0	0	-36	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
BRIDGEHAVEN	22	22	6	0	0	0	0	0	0	0	-50	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
CLUFF, LLC	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	0	0	0	0	0	0	0	0	11
LALAZAR ESTATES	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	0	0	0	0	0	9
LODI ESTATES **	0	0	22	0	0	-22	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
LODI WEST *	0	0	0	0	57	0	0	0	0	0	-57	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
MILLS AVENUE TOWN HOMES	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	19	0	0	0	0	0	0	0	0	31
NEUSCHAFFER PROPERTY ***	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	80	0	0	0	0	0	0	0	0	0	80
SASAKI PROPERTY	0	0	0	0	0	0	0	0	0	100	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	103
SUNWEST GARDEN	0	0	0	0	0	0	0	0	0	18	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18
WINCHESTER WOODS	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	0	0	0	0	8
WINE & ROSES HOMES	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	31	0	0	0	0	0	0	0	0	0	31
WOODHAVEN PARK	0	0	0	0	75	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	75
MILLER RANCH DEV.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	65	0	0	0	0	0	0	0	65
Kathy Haring (Muir Wood)	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	0	7
Taj Khan	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19	0	0	0	0	0	19
	40	40	28	0	132	-58	0	0	0	118	-104	0	0	0	0	132	38	65	0	26	0	0	0	0	0	438

* In '93 the Planning Commission awarded 40, 1994 medium density allocations to the Lodi West project.
 ** The Bangs Ranch and Lodi Estates projects each were awarded single family allocations in place of their medium density allocations.
 *** The Neuschaffer Property project was awarded 154 allocations in 2003 but only used 80 leaving a balance of 74.
 **** The Miller Ranch Development project was awarded 65 allocations in 2005 (45 from the 2005 schedule and 20 from unused allocations from previous years)

HIGH DENSITY

MEDIUM DENSITY (10,259*25%) = 2,557

Available High Density Residences for allocation																	ALLOCATION									
PROJECT	1989	1990	1991	1992	2557	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	TOTALS
BENNETT & COMPTON	99	45	0	0	-144	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	99	45	0	0	-144	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

* The Bennett and Compton project was awarded 75 medium density allocations under the project name of Woodhaven Park.

EXPIRED

RESOLUTION NO. P.C. 12-

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LODI
RECOMMENDING TO THE CITY COUNCIL THE APPROVAL OF THE REQUEST OF MR.
JOHN GIANNONI FOR 12 MEDIUM DENSITY RESIDENTIAL GROWTH MANAGEMENT
UNITS AT 2110 TIENDA DRIVE.**

WHEREAS, the Planning Commission of the City of Lodi has heretofore held a duly noticed public hearing, as required by law, on the requested Growth Management Development Plan as required by Lodi Municipal Code Chapter 15.34; and

WHEREAS, on May 31, 2012, the City of Lodi received an application from Mr. John Giannoni for Growth Management Allocations to permit and construct 12 dwelling units at 2110 Tienda Drive; and

WHEREAS, the project proponent is Mr. John Giannoni 2111 West Kettleman Lane, Suite D, Lodi, CA 95242; and

WHEREAS, property owners of record are John and Kerry Giannoni, 2960 Applewood Drive, Lodi, CA, 95242; and

WHEREAS, the project site is at 2110 Tienda Drive, Lodi, CA 95242 (APN: 027-410-19); and

WHEREAS, the City General Plan 2010 designates the project site as Mixed Use Corridor; and

WHEREAS, the City's Municipal Code classifies the project as R-C-P, Residential, Commercial and Professional Zoning District; and

WHEREAS, the General Plan land use designation of Mixed Use Corridor and R-C-P Zoning District permit residential development density range of 7.1-20 dwelling units per acre; and

WHEREAS, the request is for approval of 12 Medium Density Residential Growth Management Allocations for a 12-unit, proposed as a two-phased project; and

WHEREAS, as required by the Lodi Municipal Code Section 17.81.030 (A), future developments and construction plans shall be reviewed and approved by the Planning Commission and, if necessary, by Site Plan and Architectural Review Committee prior to the issuance of a building permit; and

WHEREAS, all legal prerequisites to the approval of this request have occurred; and

NOW BE IT FOUND, as follows, by the Planning Commission of the City of Lodi, based on the entirety of the record before it, which includes without limitation, the staff report, project file, written and oral testimony, makes the following findings:

1. The required public hearing by the Planning Commission was duly advertised and held in a manner prescribed by law.
2. The project is found to be categorically exempt according to the standard exemption of CEQA Section 15332, Class 32. – In-Fill Development Projects. The project is consistent with the general plan and zoning, is less than 5-acres in size, is within the City and surrounded by development, there is no habitat value, approval of the project will not result in significant effects relating to traffic, noise, air quality, or water quality, and the project will be adequately served by all required utilities and public services. The project is exempt from further review under CEQA. No significant impacts are anticipated and no mitigation measures have been required.
3. The proposed design and improvement of the site will be designed to be consistent with all applicable standards adopted by the City in that the project, as conditioned, shall

conform to the standards and improvements mandated by the City of Lodi Public Works Department Standards and Specifications, and Zoning Ordinance.

4. The standard size, shape and topography of the site are physically suitable for a medium density residential development in that the site is generally flat and has no unusual or extraordinary topographic features.
5. The proposed density of 20 dwelling units per acre is consistent with the General Plan Land Use and Growth Management Elements that limits the density of the project site to a maximum of 20 dwelling units per acre.
6. The proposal is compatible with surrounding developments, zoning and land uses.
7. The site is suitable for the type of development proposed by the project in that the site can be served by all public utilities and creates design solutions for storm water, traffic and air quality issues.
8. The design of the proposed project and type of improvements are not likely to cause serious public health problems in that all public improvements will be built per City standards and all private improvements will be built per the California Building Code.
9. The design of the proposed project and type of improvements are not likely to cause serious public health problems in that all public improvements will be built per City standards and all private improvements will be built per the California Building Code.
10. The project allows for the orderly development of Lodi in that the Land Use and Growth Management Element calls for the development of the site at a density of 7.1 to 20.0 dwelling units per acre and the allocation of units proposed sets a density of 8.2 dwelling units per acre.
11. The Development Plan complies with the requirements of Section 15.34.070 of the Growth Management Plan for Residential Development Ordinance.
12. No new impacts were identified in the public testimonies that were not addressed as normal conditions of project approval.

NOW, THEREFORE, BE IT DETERMINED AND RESOLVED, by the Planning Commission of the City of Lodi hereby recommends that the City Council award Mr. John Giannoni 12 medium density growth management allocation units, subject to the following development conditions and standards:

1. The property owner and/or developer and/or successors in interest and management shall, at their sole expense, defend, indemnify and hold harmless the City of Lodi, its agents, officers, directors and employees, from and against all claims, actions, damages, losses, or expenses of every type and description, including but not limited to payment of attorneys' fees and costs, by reason of, or arising out of, this development approval. The obligation to defend, indemnify and hold harmless shall include, but is not limited to, any action to arbitrate, attack, review, set aside, void or annul this development approval on any grounds whatsoever. The City of Lodi shall promptly notify the developer of any such claim, action, or proceeding and shall cooperate fully in the defense.
2. This recommendation for approval by the Planning Commission shall not constitute a authorization to begin any construction. The proposed residential development plan shall be submitted for Planning Commission/SPARC review and approval. The said plan shall comply with all applicable zoning and design standards adopted by the City prior to issuance of any construction permits. This will require the applicant to submit a detailed development plan that shows the exact dimensions and building details. The plan must

show that the proposed number of units can be built on the property and meet all City development requirements.

3. The property owner and/or developer and/or successors in interest and management shall submit for approval by the City all required condominium map and related documents to create legal parcels.
4. All applicable state statutes, and local ordinances, including all applicable Building and Fire Code requirements for hazardous materials shall apply to the project. In an event of a conflict, the strictest law or regulation shall apply.
5. Any fees due the City of Lodi for processing this project shall be paid to the City within thirty (30) calendar days of final action by the approval authority. Failure to pay such outstanding fees within the time specified shall invalidate any approval or conditional approval granted. No permits, site work, or other actions authorized by this action shall be processed by the City, nor permitted, authorized or commenced until all outstanding fees are paid to the City.
6. No variance from any City of Lodi adopted code, policy or specification is granted or implied by this approval.

Dated: August 8, 2012

I certify that Resolution No. 12- was passed and adopted by the Planning Commission of the City of Lodi at a regular meeting held on August 8, 2012 by the following vote:

AYES:	Commissioners:
NOES:	Commissioners:
ABSTAIN:	Commissioners:
ABSENT:	Commissioners:

ATTEST _____

Secretary, Planning Commission

Item 3b.

**CITY OF LODI
PLANNING COMMISSION
Staff Report**

MEETING DATE: August 8, 2012

APPLICATION NO: Use Permit: 12-U-06
SPARC: 12-SP-02

REQUEST: Request for Planning Commission approval of a Use Permit and a SPARC review to allow development of a gas station with 8-dispenser canopy, 3,078 square foot convenience store with sale of beer and wine (Type-20), and a drive through carwash facility on a .94-acre site located at 255 East Harney Lane. (Applicant: Peter Tobin, on behalf of Hardev Singh Gill; File Number: 12-U-06 and 12-SP-02).

LOCATION: 255 East Harney Lane
APN: 062-580-40
Lodi, CA 95240

APPLICANT: Pete Tobin
Barghausen Consulting Engineers, Inc.
1255 Treat Blvd Suite 329
Walnut Creek, CA 94597

PROPERTY OWNER: Nanak State, Inc.
4304 Payne Road
Pleasanton, CA 94588

RECOMMENDATION

Staff recommends the Planning Commission approve the Use Permit and SPARC applications to develop an automobile service station with 8-dispenser canopy, 3,078 square foot convenience store, and drive-through carwash facility at 255 East Harney Lane, subject to the conditions and based on the findings contained in the attached Planning Commission Resolution.

PROJECT AREA DESCRIPTION

General Plan Designation: Industrial
Zoning Designation: C-1, Neighborhood Commercial
Property Size: .94-ac (40,946 sq. ft.)

	ADJACENT ZONING AND LAND USE CHARACTERISTICS		
	GENERAL PLAN	ZONING CLASSIFICATION	EXISTING LAND USE
North	Low Density Residence	R-2, Single Family Res.	Single family residences
South	Low Density Residence and Commercial	Planned Development 39	Reynolds Ranch Development
East	Low Density Residence	R-2, Single Family Res.	Single family residences
West	Industrial	M-2, Heavy Industrial	Industrial use

SUMMARY

The applicant proposes to construct a gas station, convenience store with beer and wine available for off-site consumption, and carwash facility at a vacant lot located northwest 255 East Harney Lane (corner of Stockton Street and Harney Lane). The project site is currently designated Neighborhood

Commercial (C-1). The applicant proposes to operate the gas station and convenience store 24 hours each day and operate the car wash from 7:00 AM to 10:00 PM seven days each week. The Zoning Ordinance requires a Use Permit for sale of alcohol and SPARC application for development of a commercial property. The project site is accessed from a driveway on Stockton Street and a driveway on Harney Lane. Harney Lane is a heavily traveled transportation corridor. The project site is surrounded by residential uses to the north and east, by industrial uses to the west, and by a development commonly known as Reynolds Ranch, which will contain a mixture of commercial and residential developments.

BACKGROUND

The project site was annexed into the City in 1995 along with the property that is now a single family residential subdivision located north and east of the site. This project site was designated for commercial uses when the property was annexed to the City. Back in 2004, the property owners petitioned the Planning Commission for review and approval for construction of a gas station, a carwash facility and convenience store with off-sale beer and wine license. The Commission reviewed and approved the applicant's request.

The area involved is a mixture of recent single family residential on the east side of Stockton Street, industrial use on the west and recently annexed single family residences on the south. The subject property is located at the northeast corner of Stockton Street and Harney Lane. The site is level with existing 8-ft masonry walls located adjacent to single family residences to the north and east. Existing street improvements include curb and gutter with drive approaches. Access to the site is provided on Harney Lane and Stockton Street. The site is nearly one acre in size. It has been vacant since its annexation in 1995.

PROJECT DESCRIPTION

The applicant proposes to construct a four-island gas station, convenience store with beer and wine available for sale, and a drive-through carwash facility with associated outdoor vacuum equipment. The convenience store and carwash facility would involve approximately 4,265 square feet of building footprint. The site plan includes a drive-through car wash with circulation. Parking needs for the project are provided onsite. The applicant proposes to operate the gas station and convenience store 24 hours each day and operate the carwash facility from 7:00 AM to 10:00 PM seven days each week. The applicant anticipates employing approximately 6-8 persons total at this facility with up to two employees per shift.

The .94-acre property is zoned Neighborhood Commercial (C-1), and a mix of retail, office, fuel station, grocery stores and other similar uses are allowed by right. The C-1 Zoning District is typically located in a residential neighborhood and often abuts residential properties. More heavily intensive General Commercial uses (such as auto repair) are specifically excluded from the C-1 Zoning District in order to be compatible with adjacent residential uses. Gasoline service stations and general merchandise/groceries are listed as permitted uses for the site. However, an automated car wash facility is listed as a permitted use with a Use Permit review and approval by the Planning Commission. Automated carwash facilities typically have been incorporated into gasoline stations/convenience stores, and currently there are other similar types of automated self-service carwash facilities throughout the City.

There are two different approvals necessary for this project. The first is a Use Permit application for the sale of alcohol for off-site consumption and to allow a carwash facility at this location. The second part of the approval process is the development review (SPARC) application, which includes a review of the physical elements of the project for all of the proposed structures, including the site plan, architecture, landscape and hardscape elements. Generally, the City combines the processing of applications such as Use Permits and SPARC reviews as a means of streamlining reviews.

ANALYSIS

Use Permit Application:

The Lodi Municipal Code requires a Use Permit for the sale of alcohol. The provision covering alcohol sales is found in Section 17.72.040 of the Zoning Code and applies to a new business, to an expansion or relocation of an existing business, or to any modification of an existing license. The ordinance applies to off-sale as well as on-sale alcohol outlets. The applicant has submitted an application with the State for an Alcohol Beverage License (ABC) 21-Off-Sale General license. Type-21 ABC license authorizes the sale of beer, wine and distilled spirits for consumption off the premises where sold. Minors are allowed on the premises.

The project site belongs to Census Tract 44.02. Census Tract 44.02 covers the area south of Kettleman Lane, west of Central California Traction Company (C.C.T) Line, north of Harney Lane, and east of Union Pacific Rail Road Company (U.P.R.R). According to ABC, Census Tract 44.02 contains two (2) existing off-sale licenses with four (4) off-sale licenses allowed based on the ABC criteria. This site is rather unique in that it is within a Census Tract that has fewer establishments than the State guidelines. Moreover, there are no other sites within the general vicinity along Stockton Street. The other off-site ABC license along Harney Lane belongs to COSTCO, which is not within this Census Tract. As a standard precaution, staff has included standard conditions related to off-sale licenses.

Site Design and Circulation:

As proposed, the project involves construction of a four-island gas station, convenience store, outdoor vehicle vacuum area and a drive-through carwash facility. The convenience store is located on north end of the parcel and maintains a minimum of 32-ft setback from adjacent single family residences to the north, which are screened by an existing 8-ft tall masonry wall. A 10-ft wide carwash drive-through aisle is located in the 32-ft setback area. A minimum of 15-ft landscape buffer zone separates the drive-through isle from the residential properties to the north and east. The drive-through lane is accessed at the west end of the parcel, travels along the north side of the convenience store, leads to the carwash facility located on the east end of the parcel and exits into the parking area at the south end of the property. The drive-through circulation moves in a clockwise pattern and drive-up queue allows a maximum of eight vehicles. The carwash facility is placed on the east end of the parcel and maintains 10-ft setback from the residential properties to the east and approximately 100-ft from Harney Lane. The gas dispenser canopy is located south of the convenience store and west of the carwash facility. Vehicular vacuum area is provided on the west end to the parcel, adjacent to Stockton Street. The location of the outdoor vacuum area was selected in order to provide appropriate separation from the residences to the east and north and also not to conflict with on-site circulation around the pump island, especially for fueling trucks.

This layout helps to keep the line of cars that may be waiting at the car wash away from adjacent public right-of-ways, and provides for efficient circulation patterns on the site, with the car wash exit at a drive aisle adjacent to the dispenser canopy. Staff is comfortable that the amount of que is sufficient to not create a problem elsewhere on or off site. Parking is provided adjacent to the convenience store building, away from the carwash drive-isle. This configuration will allow for an efficient flow of traffic through the auto-oriented site, and for convenient parking.

Noise:

A Noise Study was prepared by Bollard Acoustical Consultants to evaluate the potential impacts from the proposed carwash facility and the outdoor vacuum. Since the proposed project site is adjacent to single family residences, the project must conform to the residential daytime standard of 60 dBA (average one-hour sound level) along the residential property boundaries. The commercial standard of 65 dBA would be required along the western and southern property boundaries. Noise generated from the carwash primarily would be from two sources: the wash/dry cycle/vacuum equipment (primarily the drying equipment) and from vehicles exiting the carwash facility. The study

concluded limiting hours of carwash operations to 7:00 a.m. to 10:00 p.m. and installation of doors on both end of the carwash facility would reduce noise levels in conformance with the City's General Plan. The vacuum equipment is placed along industrial uses on the west and has no impact on the residential uses.

At staff's request, Bollard Acoustical Consultants conducted an addendum noise study at an existing facility similar to the project site. Staff's concern was that the original noise study relied upon manufacturer's reference noise level data to arrive at its conclusion. Staff felt that a short-term and built-in environment would provide the most accurate noise level measurements to verify the findings found on the original report. Staff selected an existing carwash location based on its similarity to the project site (adjacent to residences, separated by masonry wall, existing outdoor vacuum operation and entry/exist doors installed and operating). As shown on the addendum study, the results of the original noise study were verified. The addendum study concluded that no mitigation measures beyond those incorporated in the original findings would be necessary. The carwash facility also would be in conformance with the commercial limit of 65 dBA at the northern and eastern property boundaries.

Since the facility is adjacent to residential uses, staff recommends the use of the carwash facility and associated outdoor vacuum equipments should only be in operation during the hours of 7:00 a.m. and 7:00 p.m. Further, the carwash facility is conditioned to incorporate the Ryko Thrustpro Dryer with Silencer system within the carwash tunnel and be set back a minimum of three feet from the exit on the southern side of the building. In addition, the carwash facility is required to install doors on both ends of the carwash facility. The use of the said equipment and installation of doors would reduce the average noise level of the combined carwash and use of vacuum equipment to within the City's Noise Ordinance Limits of 65 dBA for daytime hours (7:00 a.m. - 10:00 p.m.) at the property line for residences.

Parking:

Parking is provided adjacent to the convenience store building. This configuration places parking stalls closer to the building and does not interfere with onsite traffic circulation. Per Code, parking for the proposed convenience store use is required and provided at one space per every 250 square feet of gross floor area. Given its size, the project is required to provide a total of twelve parking stalls and the project provides fourteen onsite parking spaces. In addition, the project has been conditioned to provide bicycle rack enough to secure at least four bicycles simultaneously.

Landscape Plan

As shown on the preliminary landscape plan, a 15-ft and 10-ft landscape buffer areas are provided along the northern and eastern property lines, respectively. A combination of evergreen trees, shrubs and groundcover are proposed for these landscape areas. Additional landscape is also provided along Stockton Street and Harney Lane frontages. The singular purpose of the landscape along the residential boundaries is to serve as screening mechanism. Although the landscape plan meets the minimum requirements of the Municipal Code, staff recommends that the final landscape plan include evergreen trees, shrubs and/or vines in order to provide year-round foliage and screening. A recommended condition of approval requires the developer to submit a detailed landscape and irrigation plan for approval by the Community Development and Public Works Departments for review and approval. All landscape and irrigation improvements are to be designed and installed in compliance with the requirements of the Water Efficient Landscape Guidelines, Lodi Municipal Code, and all other applicable City standards.

Architecture

A key aspect of the design of the buildings on this site is to ensure that they would fit in with the existing residential neighborhoods nearby. The proposed convenience store, carwash facility and canopy structures are comprised mostly of stucco, and they use numerous stone accents at the

building entries and edges. The convenience store features slate roofing material where there is a visible shed roofline, at the entry of the convenience store and on the west elevation of the carwash. The remaining rooflines are designed with parapets, finished with a cornice cap. The dispenser canopy is comprised with a flat roofline, utilizing the same stucco, stone and cornice caps on the support columns. These three buildings are consistent with each other, using the same materials and mix of rooflines, and compatible colors. The overall design of the station embodies the hallmarks of the Arco and AM/PM architecture theme.

The project has been conditioned to install flash mounted metal landscape trellis on the south and west elevations of the convenience store and on the west elevation of the carwash facility, similar the type of landscape trellis found at the Vintner's Square (Lowe's Shopping Center). Building mounted landscape metal trellis serve twofold purposes: first, it discourages graffiti and other types of vandalism. Second, it prevents the operator from installing illegal signage. In the past, staff has found building mounted trellis to be extremely effective.

Trash Enclosure:

The project provides refuse enclosure on the west end of the property. A masonry wall and metal doors enclose refuse bins inside. It is large enough to provide trash bins for recyclable and non-recyclable materials. A concrete walkway will be installed in the parking area between the restaurant and the trash enclosure. The proposed enclosure meets the criteria of Section 17.58.130. Staff has placed a condition requiring the applicant to install creeping vine or similar landscape material around the refuse enclosures to discourage and prevent graffiti.

Signage:

Exterior building signage can have a significant visual impact on a project as it is viewed by the public, particularly in the case of a building fronting a major arterial street. The applicant has submitted proposed signage for the Arco project; however, signage is not part of this approval. Two conditions are included in this report. One condition that requires the applicant to obtain approved sign permits prior to the installation of any signs on the site. The second condition prohibits installations of signs facing to adjacent residences (north and west).

Conclusion

The applicant's requests are all related to establishing a service station with convenience store on the vacant parcel at Stockton Street and Harney Lane. The main concerns for the carwash facility relate to noise impacts to the residents, circulation and site appropriateness. The applicant has submitted a noise study and addendum noise study. The findings of the studies indicate the carwash facility, as proposed, could co-exist with the residential uses and comply with the City's Noise Ordinance. The carwash facility is proposed to operate 7:00 a.m. – 10:00 p.m. daily. Staff has placed a condition restricting carwash facility hours from 7:00 a.m. – 7:00 p.m. With respect to circulation, staff is comfortable that the amount of que is sufficient to not create a problem elsewhere on or off site.

The location and design of the proposed development is consistent with the goals and policies of the 2010 General Plan and the C-1 zoning district. The General Plan Land Use Commercial designation allows such commercial uses the proposed business. The C-1 zoning district allows fuel stations by right. Automated carwash facilities typically have been incorporated into gasoline stations/convenience stores, provided that appropriate measures are designed into the project to ensure compatibility with adjacent uses, and to mitigate potential noise, lighting, visual and traffic impacts. The proposed carwash facility would not create special problems in the area because the buildings incorporate design features meant to reduce noise levels to the point where city standards are met. Engineering Division indicated that traffic is not anticipated to increase, and all vehicular traffic generated by the project would be accommodated safely and without causing undue congestion upon the adjoining streets and intersections. An addition of a gas station in heavily

traveled corridor is appropriate use of the site. In addition, the project addresses one of the community's concerns related to employment opportunity. The project is expected to employ up to four people. As proposed, appropriate buffers and setbacks are to be provided between commercial and residential uses.

ENVIRONMENTAL ASSESSMENTS

The project was found to be Categorically Exempt according to the California Environmental Quality Act, Article 19 §15321, Class 21 (a) (2). The project is classified as an "Enforcement action by regulatory agencies" because it is the "adoption of an administrative decision or order enforcing or revoking the lease, permit, license, certificate, or entitlement for use or enforcing the general rule, standard, or objective." No significant environmental impacts are anticipated and no mitigation measures have been required.

PUBLIC HEARING NOTICE:

Legal Notice for the Use Permit was published on July 28, 2012. Forty-six (46) public hearing notices were sent to all property owners of record within a 300-foot radius of the subject property as required by California State Law §65091 (a) 3.

ALTERNATIVE PLANNING COMMISSION ACTIONS:

- Approve the request with attached or alternate conditions
- Deny the request
- Continue the request

Respectfully Submitted,

Concur,

Immanuel Bereket
Associate Planner

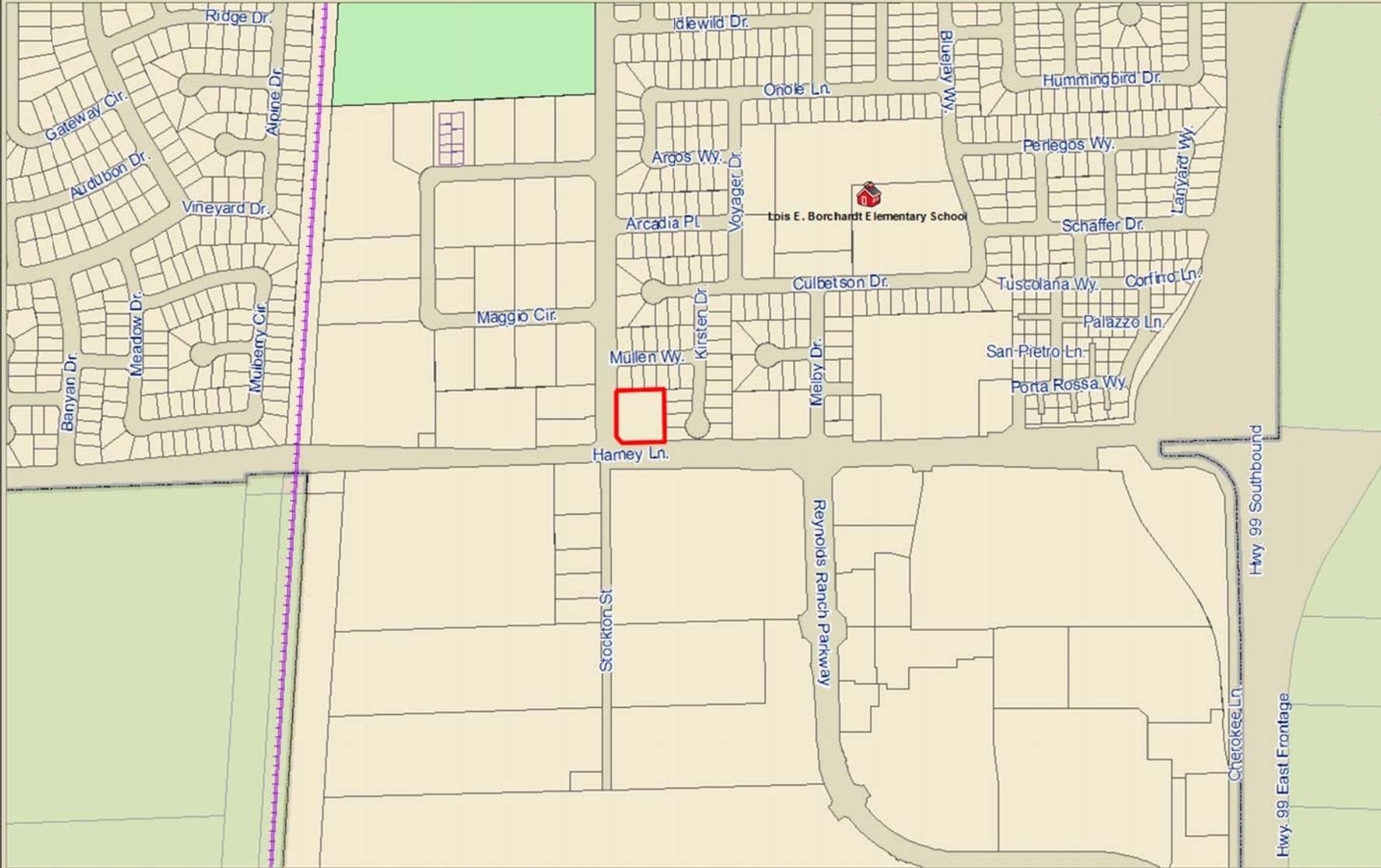
Konradt Bartlam
Community Development Director

ATTACHMENTS:

1. Vicinity Map
2. Aerial Photo
3. Acoustics Studies
4. Site Plan
5. Elevations
6. Draft Resolution



Vicinity Map



- Legend**
- City Limit
 - Park/Basin
 - County
 - Project Site

Map Scale
1:4,733



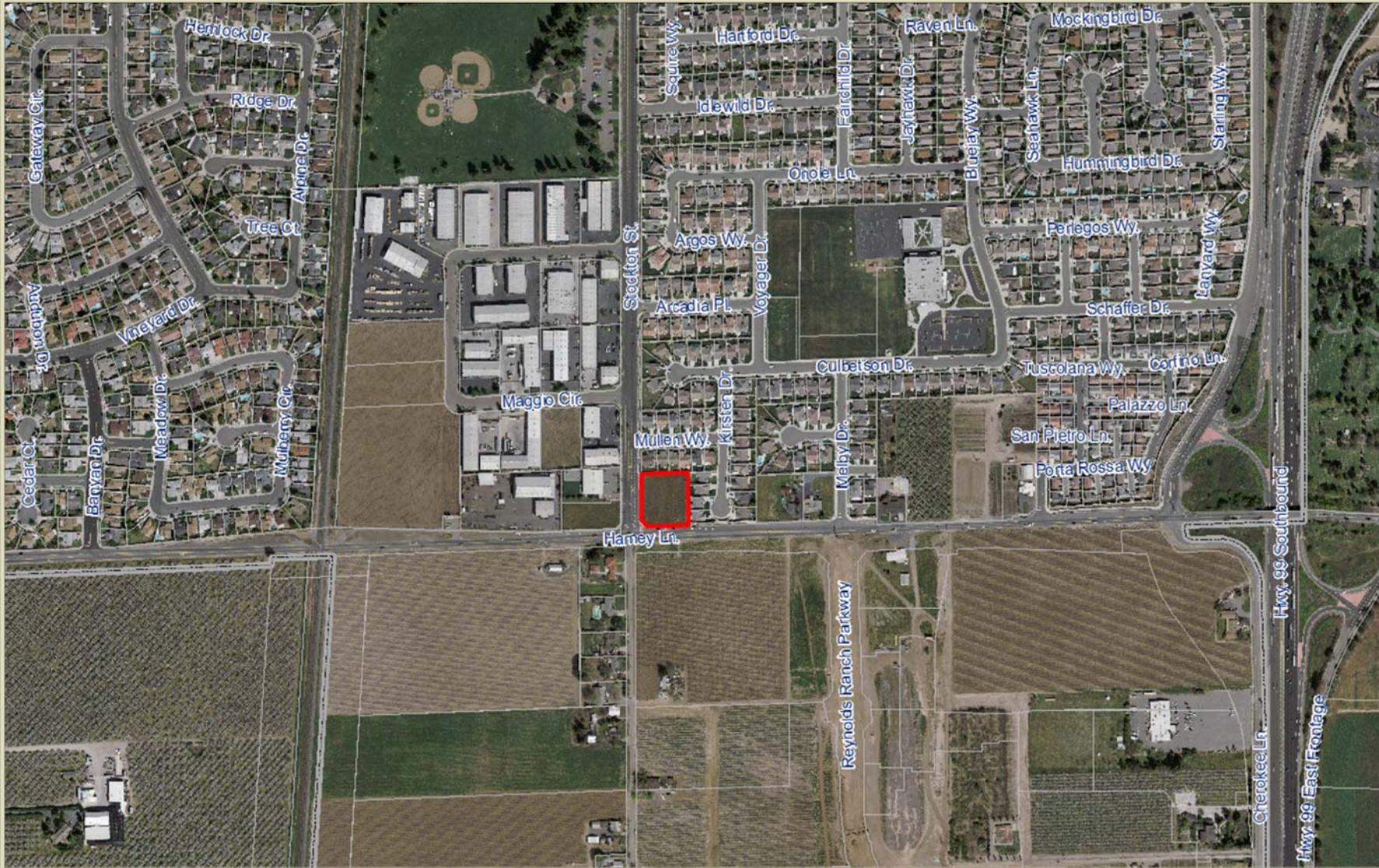
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© City of Lodi Geographic Information Systems

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.
THIS MAP IS NOT TO BE USED FOR NAVIGATION

Notes
AM PM Gas Station
255 East Harney Lane
Lodi, CA 95240



Aerial Map



Legend

- Project Site
- City Limit

Map Scale
1:4,733

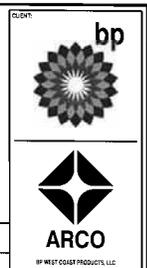
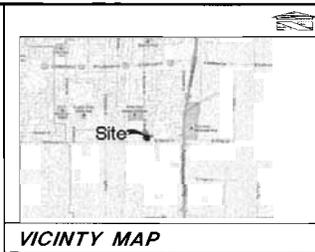
789 0 394 789Feet

NAD_1983_StatePlane_California_III_FIPS_0403_Feet
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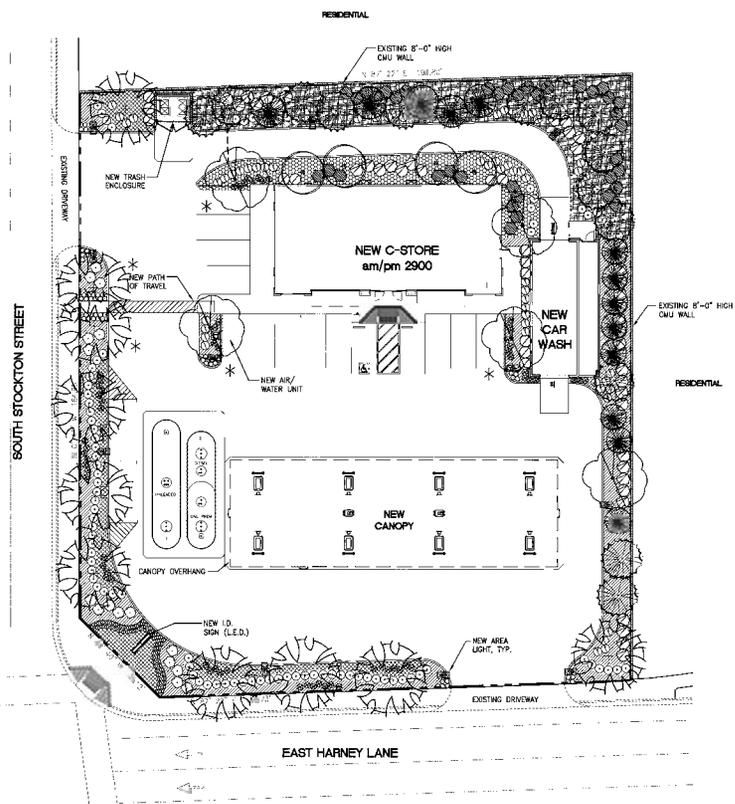
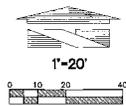
Notes
AM PM Gas Station
255 East Hamley Lane
Lodi, CA 93240

255 E. HARNEY LANE - LODI, CALIFORNIA PRELIMINARY LANDSCAPE PLAN



LANDSCAPE PLANT MATERIAL LEGEND

SYMBOL	BOTANICAL/COMMON NAMES	SIZE CONDITION	SPACING	QUANTITY	REMARKS
	TREES: ZELKOVIA SERRATA 'VILLAGE GREEN' / VILLAGE GREEN JAPANESE ZELKOVA	24" BOX	AS SHOWN	8	STAKE & GUY ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 6'
	LAGERSTROMIA 'NATCHEZ' / WHITE GRASS WORTLE	24" BOX	AS SHOWN	6	STAKE & GUY ONE GROWING SEASON; NURSERY GROWN, BRANCHED AT 6'
	LOQUIMBER STRYPTHELLA 'AURORA' / AMERICAN AURORA SWEETGUM	24" BOX	AS SHOWN	5	STAKE & GUY ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 6'
	PRUNUS MONTECOLA / WESTERN WHITE PINE	24" BOX	AS SHOWN	14	STAKE & GUY ONE GROWING SEASON; NURSERY GROWN, UN-CUT LEADER
	THUJA PLICATA 'GREEN GANT' / WESTERN RED CEDAR	24" BOX	AS SHOWN	13	STAKE & GUY ONE GROWING SEASON; NURSERY GROWN, UN-CUT LEADER
	DECORATIVE PARKING LOT TREE 1" TREE PER 4' SPALS 14' SPALS = 3.5 TREES REQUIRED 5.0 TREES PROVIDED				
	SHRUBS: CEANOTHUS GRISEUS 'HORIZ 'YANKEE POINT' / YANKEE POINT CEANOTHUS	1 GALLON	4" O.C.	16	FULL & BUSHY, MATCH HEIGHTS
	CEANOTHUS 'JULIA PHILIPS' / JULIA PHILIPS CEANOTHUS	5 GALLON	4" O.C.	27	FULL & BUSHY, MATCH HEIGHTS
	ESCALONIA 'APPLE BLOSSOM' / APPLE BLOSSOM ESCALONIA	5 GALLON	4" O.C.	41	FULL & BUSHY, MATCH HEIGHTS
	HEMEROCALLIS HYBRIDUS 'RUBY STELLA' / RED STELLA DAILEY	1 GALLON	16" O.C.	62	RED FLOWER
	HEMEROCALLIS HYBRIDUS 'STELLA DE OR' / STELLA DE OR DAILEY	1 GALLON	16" O.C.	63	YELLOW FLOWER
	MYRTICA CALIFORNICA / GORDON WORTLE	5 GALLON	4" O.C.	11	FULL & BUSHY, MATCH HEIGHTS
	NAUFRAGA DOMESTICA 'MOON BAY' / MOON BAY HEAVENLY BAMBOO	5 GALLON	2" O.C.	64	3 CANES MINIMUM
	NERANI BLENDER 'LITTLE RED' / LITTLE RED BLENDER	5 GALLON	3" O.C.	89	FULL & BUSHY, MATCH HEIGHTS
	PODOCARPUS MACROPHYLLUS 'MADON' / YEW PINE	5 GALLON	4" O.C.	12	FULL & BUSHY, MATCH HEIGHTS
	RHAPHIOLEPIS INDICA 'ENCHANTRESS' / ENCHANTRESS INDIA HAWTHORN	5 GALLON	3" O.C.	29	FULL & BUSHY, MATCH HEIGHTS
	GROUNDCOVERS: ARCTOSTAPHYLOS UVA-URSI / KIWANINGKA	FLAT	12" O.C.	AS REQ'D	HOLD 12" FROM BORDERS, SHRUBS AND TREES
	BACCHARIS PILULARIS / 'DWARF' COYOTE BRUSH	1 GALLON	24" O.C.	AS REQ'D	HOLD 12" FROM BORDERS, SHRUBS AND TREES
	LANTANA MONTEVIDEOSIS 'GOLD RUSH' / GOLD RUSH LANTANA	1 GALLON	24" O.C.	AS REQ'D	HOLD 18" FROM BORDERS, SHRUBS AND TREES
	MYRSINUM FARVEOLUM 'PROSTATUM' / NO COMMON NAME	FLAT	12" O.C.	AS REQ'D	HOLD 12" FROM BORDERS, SHRUBS AND TREES



18215 72ND AVENUE SOUTH
KENT, WA 98032
(206)291-8222
(206)291-8782 FAX

CIVIL ENGINEERING, LAND PLANNING,
SURVEYING, ENVIRONMENTAL SERVICES

NO.	DATE	REVISION DESCRIPTION
1	06/28/12	PER CITY COMMENTS
2		
3		
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9		
10		



DEVELOPMENT INFORMATION
2900 MDC
2900 AM/PM C-STORE
CAR WASH
8 MPD CANOPY

SITE ADDRESS:
255 E. HARNEY LANE @
STOCKTON STREET
LODI, CALIFORNIA

NTI
DESIGNER: MDC
CHECKED BY: MDC
DRAWN BY: MDC
PROJECT NO: 15441

PRELIMINARY
LANDSCAPE PLAN

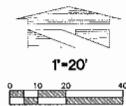
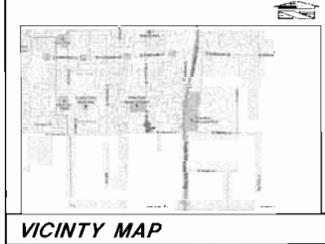
SHEET NO:

L-1

Preliminary Not For Construction

File Path: \\s000na15441\gms\m\255e - p1-1-2.dwg Date/Time: 5/25/2012 1:01 PM Scale: 1" = 1'-0" SHEET: L-1

255 E. HARNEY LANE - LODI, CALIFORNIA PRELIMINARY LANDSCAPE SITE SHADING PLAN



SITE SHADING

SITE SQUARE FOOTAGES:
 SITE: 41,225 S.F.
 STORE: 3,078 S.F.
 CAR WASH: 1,187 S.F.
 CANOPY: 4,395 S.F.

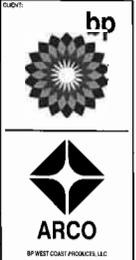
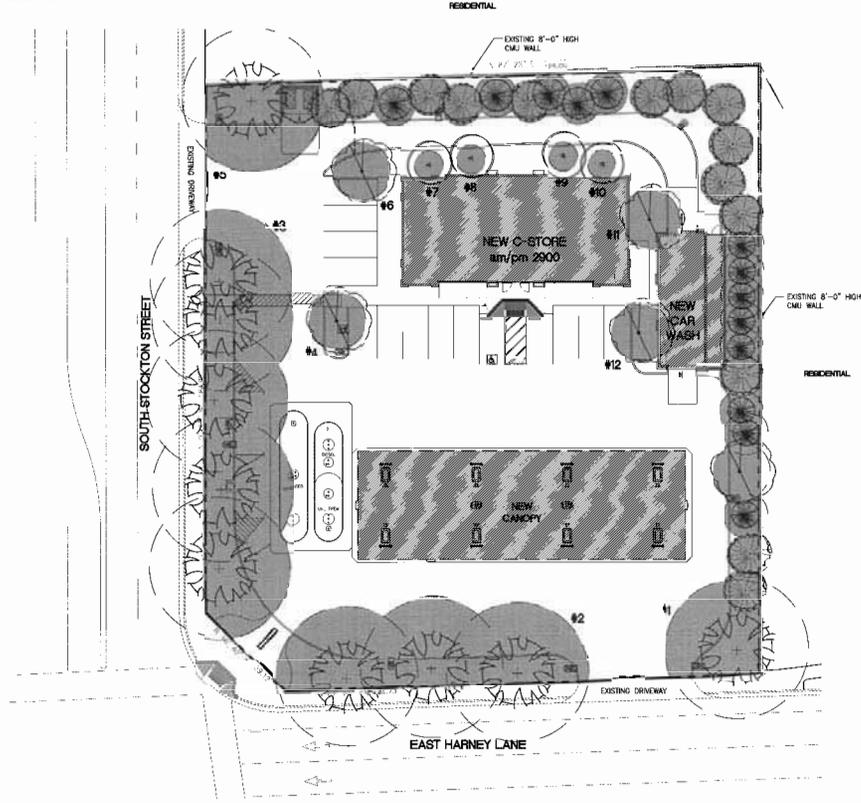
SITE SHADING:
 AREA REQUIRING SHADING: 32,565 S.F.
 40% REQUIRED = 13,026 S.F.
 44% PROVIDED = 14,360 S.F.

SHADE SQUARE FOOTAGES BY AREAS:

#1	3,404 S.F.
#2	2,810 S.F.
#3	4,114 S.F.
#4	314 S.F.
#5	2,442 S.F.
#6	314 S.F.
#7	78 S.F.
#8	78 S.F.
#9	78 S.F.
#10	78 S.F.
#11	277 S.F.
#12	223 S.F.
TOTAL	14,360 S.F.

DENOTES BUILDING AREA
 SEE ABOVE FOR SQUARE FOOTAGES

DENOTES TREE SHADE AREA
 SEE ABOVE FOR SQUARE FOOTAGES



18215 72ND AVENUE SOUTH
 KENT, WA 98032
 (425)651-0222
 (425)651-8782 FAX

CIVIL ENGINEERING, LAND PLANNING,
 SURVEYING, ENVIRONMENTAL SERVICES

NO.	DATE	REVISION DESCRIPTION
1	04/25/12	FOR CITY COMMENTS
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20		



EQUIPMENT REVISIONS:
 2900 MDC
 2900 AM/PM C-STORE
 CAR WASH
 8 MPD CANOPY

CITY ADDRESS:
 255 E. HARNEY LANE @
 STOCKTON STREET
 LODI, CALIFORNIA

NTI

DRAWN BY: MKL
 CHECKED BY: ANS
 DESIGNED BY: ANS
 APPROVED BY: ANS
 PROJECT NO.: 15441

DRAWING TITLE:
SITE SHADING PLAN

SHEET NO.:

Preliminary Not For Construction

L-2

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255 E. HARNEY LANE - LODI, CALIFORNIA PRELIMINARY IRRIGATION PLAN

LANDSCAPE IRRIGATION LEGEND

SYMBOL	DESCRIPTION	MIN. PSI	GPM	ENTER SPACING
☐	HUNTER R2WS-18" ROOF ZONE WATERING SYSTEM RANBIRD LANDSCAPE DRILLING COMPONENTS: WITH RANBIRD EASY FIT COMPRESSION FITTINGS AND ADAPTERS 10'-09" R-100	20	0.50	
○	FW225-W/F PR. PRESS. REGULATOR, APM4-075/100 FILTER FM225-HF (2-20 GPM), FM225-HF (10-32 GPM), APM4-075 (1-11 GPM), APM4-100 (7-20 GPM) RANBIRD AS - 1/2" ANTI-VACUUM RELIEF VALVE KIT, IN VALVE BOX RANBIRD 125-050 W/BEND GALVANIZED TE-DOWN STAKE, AT 10' ON CENTER	15	0.82	
●	RANBIRD 302-100-PRF, 3-15 GPM, 20"-120PSI, WITH PRESSURE-REGULATING, RSP FILTER, OUTLET PRESSURE 20PSI, IN VALVE BOX			
○	RANBIRD 100 PSIB ELECTRONIC REMOTE CONTROL VALVE, IN VALVE BOX, ONE VALVE PER BOX			
⊗	RANBIRD ESP-120UM, 12-STATION CONTROLLER WITH ETM-12M ET MANAGER CARTRIDGE WITH ETM-RD TIPPING RAIN GAUGE WEATHER STATION LWS, WITH PROVIDER PER MANUFACTURER'S INSTRUCTION AND WRITTEN SPECIFICATIONS. CONTRACTOR TO FULLY INSTANT CHECK ON THE OPERATIONS OF THE ET MANAGER AND WEATHER STATION HOOD-UP AND STATE CODES. COORDINATE LOCATION AND ELECTRICAL CONNECTION WITH OWNER, GENERAL, AND LANDSCAPE CONTRACTORS			
⊕	FERRIS 882 1 1/4" REDUCE PRESSURE ASSEMBLY WITH BALL VALVE AND UNIONS (STATE APPROVED); TEST AND CERTIFICATION BY LICENSED BACKFLOW TESTER, WITH ENCLOSURE SEE DETAIL SHEET 1-1			
○	RANBIRD 1" QUICK COUPLING VALVE WITH (2) 4KK KEYS AND ROSE SWIVELS, IN VALVE BOX			
○	ELASTIC BALL VALVE, MATCH PIPE SIZE, IN VALVE BOX			
○	LATERAL - CLASS 200 PVC (12" COVER), 1" SIZE MINIMUM			
○	MANHOLE - CLASS 200 PVC (12" COVER), SIZE PER PLAN, 3/4" SIZE MINIMUM			
○	SLUICE - CLASS 200 PVC, 24" MINIMUM COVER AT VELOCITY CROSSINGS, TWICE THE SIZE OF INSERT PIPE AND/OR WIRES, 4" SIZE MINIMUM, COORDINATE WITH GENERAL AND PAVING CONTRACTORS			

P.O.C.

TYPICAL VALVE CALL-OUT
VALVE IDENTIFICATION NUMBER
VALVE GPM
VALVE SIZE
IRRIGATION SHOWN DIAGMATICALLY FOR PLAN CLARITY. COMMON TRENCH AND PLAGE EQUIPMENT IN LANDSCAPE; MANIFOLD GROUPED VALVES IN ADJACENT STRIPS
COORDINATE CONTROLLER AND RAIN SENSOR LOCATIONS AND HOOD-LIPS WITH OWNER AND GENERAL CONTRACTOR PRIOR TO THE START OF ANY WORK.
COORDINATE ROCK WALKING BUBBLER HEADS WITH THE LOCATIONS, GENERAL, AND LANDSCAPE CONTRACTORS.

WATER EFFICIENT LANDSCAPE IRRIGATION NOTES AND CODE REQUIREMENTS

ELEMENTS OF THE LANDSCAPE DOCUMENTATION PACKAGE:

- PROJECT INFORMATION:
 - DATE: APRIL 17, 2012
 - PROJECT APPLICANT: PETER TOBIN, BARGHAUSEN CONSULTING ENGINEERS, INC., 18215 72ND AVENUE SOUTH, KENT, WA 98032
 - PROJECT ADDRESS: 255 E. HARNEY LANE @ STOCKTON, ASSESSOR'S PARCEL NO. 082-580-40, CITY OF LODI, CALIFORNIA
 - TOTAL LANDSCAPE AREA: 10,086 SQUARE FEET
 - PROJECT TYPE: SERVICE STATION/CONVENIENCE STORE/CAR WASH
 - WATER SUPPLY TYPE: POTABLE
 - CHECKLIST OF ALL DOCUMENTS IN LANDSCAPE DOCUMENTATION PACKAGE:
 - PROJECT APPLICANT: PETER TOBIN, BARGHAUSEN CONSULTING ENGINEERS, INC., 18215 72ND AVENUE SOUTH, KENT, WA 98032
- PROPERTY OWNER: NANA STATE INCORPORATION, 4302 PRINE ROAD, PLACENTIA, CA 95858
- APPLICANT SIGNATURE: DATE: _____
- AGREE TO COMPLY WITH THE REQUIREMENTS OF THE WATER EFFICIENT LANDSCAPE ORDNANCE AND SUBMIT A COMPLETE LANDSCAPE DOCUMENTATION PACKAGE.

2. WATER EFFICIENT LANDSCAPE WORKSHEET (APPENDIX B) SECTION A:

HYDROZONE	PLANT WATER USE	IRRIGATION METHOD	AREA (SQ. FT.)	% OF LANDSCAPE AREA
1	L	D	1,458	14
2	L	D	1,586	15
3	L	M	1,571	15
4	L	D	1,822	18
5	L	D	1,735	17
6	L	D	1,741	17
7	L	D	24	0.2
8	B	D	28	0.3
9	M	D	40	0.4
10	L	B	60	0.6

- WATER BUDGET CALCULATIONS (SECTION B1)
 - MAXIMUM APPLIED WATER ALLOWANCE (MAWA)
 - MAWA = (Eto) (0.82) (0.7) (LAI) + (0.3 x SLA)
 - Eto = MAXIMUM APPLIED WATER ALLOWANCE (GALLONS PER YEAR)
 - Eto = REFERENCE EVAPOTRANSPIRATION (INCHES PER YEAR)
 - DIES = COMMISSION FACTOR (PERCENTAGE PER SQUARE FOOT)
 - 0.7 = ET ADJUSTMENT FACTOR (ETAF)
 - LA = LANDSCAPE AREA INCLUDING SLA (SQUARE FEET)
 - SLA = ADDITIONAL WATER ALLOWANCE FOR SLA
 - MAWA = SPECIAL LANDSCAPE AREA (SQUARE FEET)
 - MAWA = (561 INCHES) (0.82) (0.7) (10,086 SQUARE FEET) + (0.3 x 0)
 - MAWA = 245,587.27 GALLONS PER YEAR = 245,587.27 / 3.785 = 64,918.75 GPM-FEET PER YEAR

- ESTIMATED TOTAL WATER USE (ETWU) (SECTION B2)
 - ETWU = (Eto) (0.82) (0.7) (LAI) + (0.3 x SLA)
 - ETWU = ESTIMATED TOTAL WATER USE FOR YEAR (GALLONS)
 - Eto = REFERENCE EVAPOTRANSPIRATION (INCHES)
 - LAI = PLANT FACTOR FROM WUCOLS (SEE SECTION 01)
 - HYDROZONE AREA (INCH, MEDIUM, AND LOW WATER USE AREAS)(SQUARE FEET)
 - SLA = SPECIAL LANDSCAPE AREA (SQUARE FEET)
 - DIES = COMMISSION FACTOR
 - E = IRRIGATION EFFICIENCY (MINIMUM 0.71)

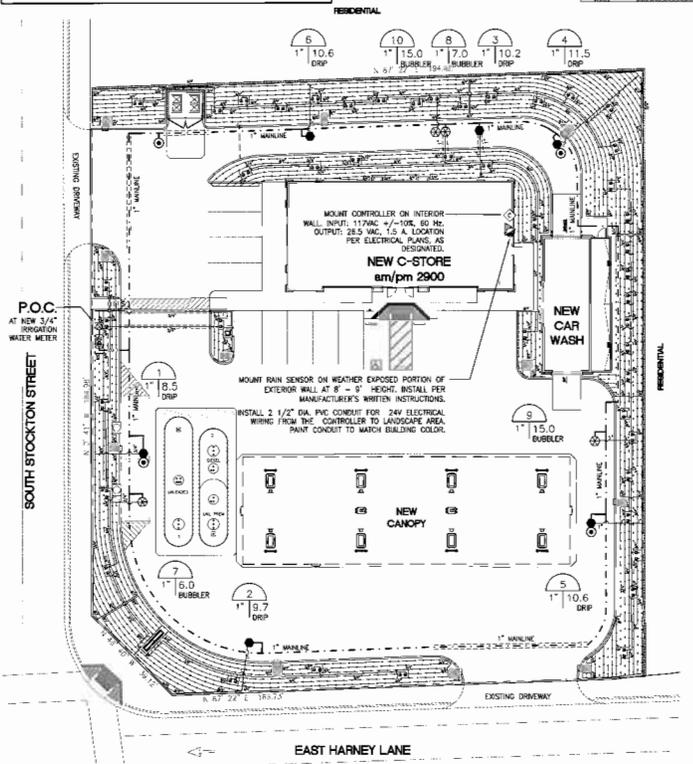
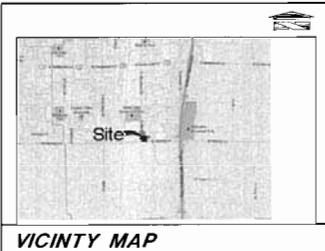
HYDROZONE INFORMATION TABLE

HYDROZONE	PLANT WATER USE TYPE	PLANT WATER FACTOR	HYDROZONE AREA (HA) (SQ. FT.)	PI x HA AREA
1	LOW	-3	1,458	437.90
2	LOW	-3	1,586	457.80
3	LOW/MED.	-4	1,571	628.40
4	LOW	-4	1,822	546.60
5	MEDIUM	-4	1,735	694.00
6	LOW	-3	1,741	522.30
7	LOW	-3	24	7.20
8	LOW	-3	28	8.40
9	MEDIUM	-4	40	24.0
10	LOW	-3	60	18.0
			SUM	3,262.40

ETWU FROM FLOW METER:
 FLOW = (06.1) (0.82) (3,262.40 / 0.71 x SLA)
 = 184,719.89
 = 184,719.89 / 748 = 246.94 GPM-FEET PER YEAR

- SOIL MANAGEMENT PLAN (PART 6)
 - SOIL MANAGEMENT PLAN (PART 6): CONTRACTOR TO OBTAIN THEREFORE, THE SOIL ANALYSIS REPORT WILL BE SUBMITTED TO THE LOCAL AGENCY AS PART OF THE CERTIFICATE OF COMPLETION BY THE INSTALLING LANDSCAPE CONTRACTOR. SOIL MANAGEMENT REPORT TO BE IN CONFORMANCE WITH THE CALIFORNIA CODE OF REGULATIONS TITLE 23, WATERS, DIVISION 2, DEPARTMENT OF WATER RESOURCES CHAPTER 2.7, MODEL EFFICIENT LANDSCAPE ORDNANCE, 482.5. BARGHAUSEN CONSULTING ENGINEERS, OWNER, AND GENERAL CONTRACTOR ARE TO BE COPIED ON SOIL ANALYSIS AND SOIL MANAGEMENT REPORT.
 - LANDSCAPE DESIGN PLAN ENCLOSED.
 - GROWING DESIGN PLAN ENCLOSED.
 - CERTIFICATE OF COMPLETION (APPENDIX C) (PART 2):
 - CERTIFICATE OF COMPLETION TO INCLUDE THE FOLLOWING SIX (6) ELEMENTS: PROJECT INFORMATION SHEET THE CONTAINS DATE, PROJECT NAME, PROJECT APPLICANT NAME/PHONE/MAILING ADDRESS, CERTIFICATION BY EITHER THE SIGNER OF THE LANDSCAPE DESIGN PLAN, THE SIGNER OF THE IRRIGATION DESIGN PLAN OR THE LICENSED LANDSCAPE CONTRACTOR THAT THE LANDSCAPE PROJECT HAS BEEN INSTALLED PER THE APPROVED LANDSCAPE DOCUMENTATION PACKAGE, WHERE THERE HAVE BEEN SIGNIFICANT CHANGES MADE IN THE FIELD DURING CONSTRUCTION, THESE "AS-BUILT" OR RECORD DRAWINGS SHALL BE INCLUDED WITH THE CERTIFICATE. A COPY OF THE IRRIGATION SCHEDULING PARAMETERS USED TO SET THE CONTROLLER, LANDSCAPE AND IRRIGATION MAINTENANCE SCHEDULE, IRRIGATION AUDIT REPORT, SOIL ANALYSIS REPORT, THE PROJECT APPLICANT TO SUBMIT THE BOUND CERTIFICATE OF COMPLETION TO THE LOCAL AGENCY FOR REVIEW. ENSURE THAT COPIES OF THE APPROVED CERTIFICATE OF COMPLIANCE ARE SUBMITTED TO THE LOCAL WATER PURVEYOR, PROPERTY OWNER, GENERAL CONTRACTOR, AND BARGHAUSEN CONSULTING ENGINEERS. CERTIFICATE OF COMPLETION TO BE IN CONFORMANCE WITH THE CALIFORNIA CODE OF REGULATIONS TITLE 23, WATERS, DIVISION 2, DEPARTMENT OF WATER RESOURCES CHAPTER 2.7, MODEL EFFICIENT LANDSCAPE ORDNANCE, 482.5.
 - IRRIGATION SCHEDULE (PART 3) (SEE SHEET 1.2.2 FOR IRRIGATION SCHEDULE): FOR THE EFFICIENT USE OF WATER, ALL IRRIGATION SCHEDULES TO BE DEVELOPED, MANAGED, AND ENFORCED TO UTILIZE THE MINIMUM AMOUNT OF WATER REQUIRED TO MAINTAIN PLANT HEALTH. IRRIGATION SCHEDULES TO MEET THE FOLLOWING CRITERIA: IRRIGATION SCHEDULING SHALL BE BASED ON WEATHER DATA, FLOW RATE, AND CURRENT REFERENCE EVAPOTRANSPIRATION, SO THAT APPLIED WATER MEETS THE ESTIMATED TOTAL WATER USE. TOTAL ANNUAL WATER TO BE LESS THAN OR EQUAL TO MAXIMUM APPLIED WATER ALLOWANCE (MAWA). ACTUAL IRRIGATION SCHEDULES TO BE REGULATED BY AUTOMATIC IRRIGATION CONTROLLER USING CURRENT REFERENCE EVAPOTRANSPIRATION DATA (E.g., CWS) OR SOIL MOISTURE SENSOR DATA. PARAMETERS USED TO SET THE AUTOMATIC CONTROLLER TO BE DEVELOPED AND SUBMITTED FOR EACH OF THE FOLLOWING: THE PLANT ESTABLISHMENT PERIOD AND THE ESTABLISHED IRRIGATION SCHEDULE TO CORRELATE FOR EACH STATION ALL THE FOLLOWING THAT APPLY: IRRIGATION INTERVAL (DAYS BETWEEN 1

- IRRIGATION: IRRIGATION RUN TIMES (HOURS OR MINUTES PER IRRIGATION EVENT TO AVOID RUNOFF); NUMBER OF CYCLE STARTS REQUIRED FOR EACH IRRIGATION EVENT TO AVOID RUNOFF; AMOUNT OF APPLIED WATER SCHEDULE TO BE APPLIED ON A WEEKLY BASIS; APPLICATION RATE; ROOT DEPTH SETTING; PLANT TYPE SETTING; SOIL TYPE, SLOPE FACTOR SETTING; SCHEDULING SETTING; AND IRRIGATION SCHEDULING OR EFFICIENCY SETTING. IRRIGATION SCHEDULING TO BE IN CONFORMANCE WITH THE CALIFORNIA CODE OF REGULATIONS TITLE 23, WATERS, DIVISION 2, DEPARTMENT OF WATER RESOURCES CHAPTER 2.7, MODEL EFFICIENT LANDSCAPE ORDNANCE, 482.5. BARGHAUSEN CONSULTING ENGINEERS, OWNER, AND GENERAL CONTRACTOR ARE TO BE COPIED ON THE LANDSCAPE AND IRRIGATION MANAGEMENT SCHEDULE.
- LANDSCAPE AND IRRIGATION MAINTENANCE SCHEDULE (PART 4): LANDSCAPE TO BE MAINTAINED TO ENSURE WATER USE EFFICIENCY; A REGULAR MAINTENANCE SCHEDULE TO BE SUBMITTED WITH THE CERTIFICATE OF COMPLETION BY THE INSTALLING LANDSCAPE CONTRACTOR. A REGULAR MAINTENANCE SCHEDULE TO INCLUDE, BUT NOT BE LIMITED TO, ROUTINE INSPECTION, ADJUSTMENT AND REPAIR OF THE IRRIGATION SYSTEM AND ITS COMPONENTS; REFRESHING MULCH; FERTILIZING; PRUNING; WEEDING IN ALL LANDSCAPE AREAS; AND REPAIRING AND CONSTRUCTION TO EMISSION SERVICES. OPERATION OF THE IRRIGATION SYSTEM OUTSIDE THE NORMAL WATERING WINDOW IS ALLOWED FOR AUDITING AND SYSTEM MAINTENANCE. REPAIR OF ALL IRRIGATION EQUIPMENT TO BE DONE WITH THE ORIGINALLY INSTALLED COMPONENTS OR THEIR EQUIVALENTS. A PROJECT APPLICANT IS ENCOURAGED TO IMPLEMENT SUSTAINABLE OR ENVIRONMENTALLY-FRIENDLY PRACTICES FOR OVERALL LANDSCAPE MAINTENANCE. LANDSCAPE AND IRRIGATION MAINTENANCE SCHEDULE TO BE IN CONFORMANCE WITH THE CALIFORNIA CODE OF REGULATIONS TITLE 23, WATERS, DIVISION 2, DEPARTMENT OF WATER RESOURCES CHAPTER 2.7, MODEL EFFICIENT LANDSCAPE ORDNANCE, 482.1. BARGHAUSEN CONSULTING ENGINEERS, OWNER, AND GENERAL CONTRACTOR ARE TO BE COPIED ON THE LANDSCAPE AND IRRIGATION MANAGEMENT SCHEDULE.
- IRRIGATION AUDIT, IRRIGATION SURVEY, AND IRRIGATION WATER USE ANALYSIS (PART 5): ALL LANDSCAPE IRRIGATION AUDITS TO BE CONDUCTED BY A CERTIFIED LANDSCAPE IRRIGATION AUDITOR. THE PROJECT APPLICANT TO SUBMIT AN IRRIGATION AUDIT REPORT WITH THE CERTIFICATE OF COMPLETION TO THE LOCAL AGENCY THAT MAY INCLUDE, BUT IS NOT LIMITED TO, INSPECTION, SYSTEM TUNE-UP, SYSTEM TEST WITH DISTRIBUTION UNIFORMITY, REPORTED OVERLAY OR RUN OFF THAT CAUSES OVERLAP FLOW, AND PREPARATION OF A IRRIGATION SCHEDULE, THE LOCAL AGENCY TO REASSESS PROGRAMS THAT MAY INCLUDE, BUT NOT BE LIMITED TO, IRRIGATION WATER USE ANALYSIS, IRRIGATION AUDITS AND IRRIGATION SURVEYS FOR COMPLIANCE WITH THE MAXIMUM APPLIED WATER ALLOWANCE. IRRIGATION SURVEY, IRRIGATION SURVEY, AND IRRIGATION WATER USE ANALYSIS TO BE IN CONFORMANCE WITH THE CALIFORNIA CODE OF REGULATIONS TITLE 23, WATERS, DIVISION 2, DEPARTMENT OF WATER RESOURCES CHAPTER 2.7, MODEL EFFICIENT LANDSCAPE ORDNANCE, 482.12. BARGHAUSEN CONSULTING ENGINEERS, OWNER, AND GENERAL CONTRACTOR ARE TO BE COPIED ON THE IRRIGATION AUDIT, IRRIGATION SURVEY, AND IRRIGATION WATER USE ANALYSIS.
- LOCAL WATER PURVEYOR: CITY OF LODI, PUBLIC WORKS DEPARTMENT (ENGINEERING), 221 WEST PINE STREET, LODI, CA 95240, 209-333-6708
- LANDSCAPE AND IRRIGATION DESIGNS ARE IN COMPLIANCE WITH THE STATE OF CALIFORNIA CODE OF REGULATIONS TITLE 23, WATERS, DIVISION 2, DEPARTMENT OF WATER RESOURCES CHAPTER 2.7, MODEL EFFICIENT LANDSCAPE ORDNANCE DATED SEPTEMBER 10, 2009 AND A GUIDE TO ESTIMATING ORIGINAL WATER ALLOWANCE (MAWA) PLANNING IN CALIFORNIA / THE LANDSCAPE COEFFICIENT METHOD AND WUCOLS IS DATED AUGUST 2000.



Preliminary Not For Construction

18215 72ND AVENUE SOUTH
KENT, WA 98032
(425)281-8222
(425)281-8765 FAX

CIVIL ENGINEERING, LAND PLANNING,
SURVEYING, ENVIRONMENTAL SERVICES

NO. DATE REVISION DESCRIPTION
 1 12/21/12
 2 01/24/13
 3 02/20/13
 4 03/20/13
 5 04/15/13
 6 05/15/13
 7 06/15/13
 8 07/15/13
 9 08/15/13
 10 09/15/13
 11 10/15/13
 12 11/15/13

2800 MFC
2900 AM/PM C-STORE
CAR WASH
8 MPD CANOPY

255 E. HARNEY LANE @
STOCKTON STREET
LODI, CALIFORNIA

NTI

DESIGNED BY: MFC
CHECKED BY: MFC
DRAWN BY: MFC
SCALE: AS SHOWN
PROJECT NO.: MDC-15441

PRELIMINARY IRRIGATION PLAN

L-3

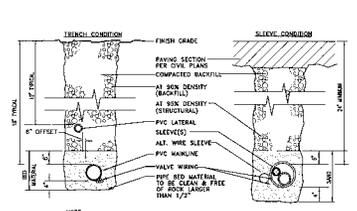
255 E. HARNEY LANE - LODI, CALIFORNIA

PRELIMINARY IRRIGATION NOTES, DETAILS AND SCHEDULING

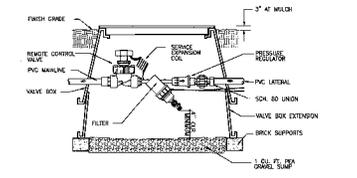
LANDSCAPE IRRIGATION NOTES

- GENERAL CONTRACTOR AND LANDSCAPE CONTRACTOR TO COORDINATE:
 - INSTALLATION OF 110V ELECTRICAL SERVICE FROM ELECTRICAL SOURCE TO AUTOMATIC CONTROLLER, INCLUDING WIRE RUN UP INTO MOUNTED CONTROLLER. IRRIGATION CONTRACTOR WILL MOUNT CONTROLLER PER DESIGN AND COORDINATE WITH GENERAL CONTRACTOR.
 - INSTALLATION OF IRRIGATION/SERVICE METER AND STUB TO IRRIGATION POINT OF CONNECTION, PER UTILITY PLANS.
 - VERIFICATION OF STATIC WATER PRESSURE AT POINT-OF-CONNECTION (P.O.C.) CONTRACTOR SHALL NOTIFY OWNER AND BARGHAUSEN CONSULTING ENGINEERS, INC., OF ANY VARIATION IN STATIC PRESSURE OVER 5 PSI GREATER/LESS THAN DESIGN PRESSURE.
 - INSTALLATION OF SLEEVING.
- SYSTEM HAS BEEN DESIGNED FOR 15 GPM AT 60 PSI. STATIC PRESSURE BY WATER DEPARTMENT TEST/CALCULATION 60 PSI.
- ALL WORK PER LOCAL CODE. INSTALLATION PER MANUFACTURER'S WRITTEN SPECIFICATIONS.
- ALL VALVES TO BE PLACED IN "PASSION" GRADE LEVEL BOXES WITH BOLT-LOCK LIDS (OR APPROVED EQUIVALENT). SET BOXES 2 INCHES HIGHER THAN FINISH GRADE IN MULCH AREAS AND FLUSH WITH FINISH GRADE IN LAWN AREAS. JUMBO BOX FOR CHECK VALVE, 10" ROUND BOX FOR GATE/QUICK COUPLER/WIRE SLEEVES, AND 12" STANDARD FOR CONTROL VALVES. PROVIDE BOX EXTENSIONS AS REQUIRED.
- MAINLINE PIPE TO BE BURIED 18 INCHES AND LATERALS 12 INCHES BELOW FINISH GRADE. NO ROCK OR DEBRIS TO BE BACKFILLED OVER PIPE.
- BUBBLER, TUBING, AND LINE POSITIONING IS DIAGNOSTIC ON PLAN. ADJUST IN FIELD AS NECESSARY FOR ADEQUATE COVERAGE. VALVES TO BE POSITIONED ADJACENT TO PAVEMENT/CURBS, IN SHRUB BEDS WHERE POSSIBLE.
- PROVIDE OWNER WITH TWO (2) SETS "AS-BUILT" DRAWINGS AND (3) SETS OF OPERATOR'S MANUALS UPON COMPLETION. INSTRUCT OWNER AS TO PROPER OPERATION AND MAINTENANCE OF THE IRRIGATION SYSTEM.
- FAMILIARIZE OWNERS FACILITY OPERATOR WITH IRRIGATION SYSTEM FUNCTION, CONTROLLER PROGRAMMING, SYSTEM OPERATION AND MAINTENANCE REQUIREMENTS.

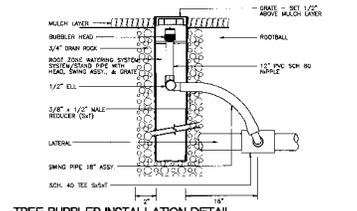
- ALL CONTROL WIRE SPICES TO BE MADE AT VALVE BOXES WITH WATER TIGHT ELECTRICAL SPICES, 3M SCOTTS LOCK SEAL, TACK 3076-75, OR EQUIVALENT.
- EACH VALVE BOX TO CONTAIN A MINIMUM OF TWO (2) SPARE ORANGE CONTROL WIRES FOR EACH WIRE. ROUTE SPARE WIRES FROM THE CONTROLLER TO THE LAST VALVE OF EACH MAINLINE BRANCH. COMMON WIRE TO BE WHITE. SINGLE STRAND WIRE TO BE A MINIMUM OF 14 GAUGE.
- ALL ELECTRICAL EQUIPMENT TO BE U.L. TESTED AND APPROVED, AND BEAR THE U.L. LABEL.
- CROSS CONNECTION PROTECTION INSPECTION REQUIRED. THE BACKFLOW DEVICE TO BE TESTED UPON THE ORIGINAL INSTALLATION. THE TESTING TO BE PERFORMED BY A PERSON HOLDING A CURRENT CERTIFICATE AS A BACKFLOW TESTER. THE TEST REPORT TO BE SUBMITTED TO THE LOCAL WATER DISTRICT OR PURVEYOR, AND OWNERS WITH A COPY TO BARGHAUSEN CONSULTING ENGINEERS, INC. CONTRACTOR TO INCLUDE TESTING IN THE SCOPE OF WORK. OWNER IS RESPONSIBLE FOR ANNUAL INSPECTIONS AFTER THE INITIAL INSPECTION.
- IRRIGATION SCHEDULING: THE IRRIGATION CONTROLLER CONTAINS A WATER BUDGET FEATURE. PERIODIC (WEEKLY) ADJUSTMENT OF THE WATER SCHEDULE IS INTENDED TO BE MADE VIA BUDGET ADJUSTMENT. RE-ADJUST WATERING DAYS AT 100 PERCENT BUDGET WHEN ADJUSTMENT EXCEEDS 30%. SET CONTROLLER FOR HIGHEST ETO WATER SCHEDULE, BASED ON PUBLISHED LOCAL EVAPOTRANSPIRATION DATA. SYSTEM HAS BEEN DESIGNED FOR 80 TO 90 PERCENT DISTRIBUTION UNIFORMITY. LAWN ZONES SHOULD BE SCHEDULED FOR 100 PERCENT REPLACEMENT FACTOR ON A TYPICAL MINIMUM 3-DAY CYCLE. SHRUB ZONES SHOULD BE PROGRAMMED AT 40 TO 70 PERCENT OF THE MONTHLY LAWN WATER REQUIREMENT ON A ONCE PER WEEK CYCLE. ALL WATERING IN EXCESS OF THE LOCAL ETO (FIELD RECHARGE) TO BE COMPLETED DURING THE CONSTRUCTION PHASE WHILE THE CONTRACTOR IS ON THE JOB SITE. OVER WATERING OF LANDSCAPE DUE TO CONTRACTOR SCHEDULING TO BE GROUNDS FOR CONTRACTOR TO REPAIR ANY RESULTANT DAMAGES AT CONTRACTOR'S OWN EXPENSE.
- SUBSTITUTION OF IRRIGATION MATERIAL/EQUIPMENT TO BE MADE ONLY UPON WRITTEN APPROVAL OF OWNER'S REPRESENTATIVE.
- ALL ZONES TO PASS A MINIMUM DISTRIBUTION UNIFORMITY OF 80 PERCENT, AS TESTED THROUGH AN IRRIGATION ASSOCIATION CERTIFIED WATER AUDIT.
- SEE SHEET L2.1 FOR ADDITIONAL IRRIGATION NOTES AND CODE REQUIREMENTS.



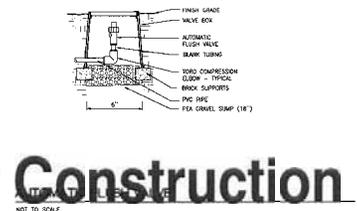
SLEEVE/TRENCHING DETAIL
NOT TO SCALE



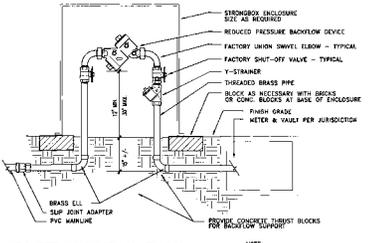
REMOTE CONTROL VALVE, PRESSURE REGULATOR AND FILTER (DRIP ZONE)
NOT TO SCALE



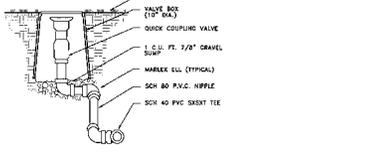
TREE BUBBLER INSTALLATION DETAIL
NOT TO SCALE



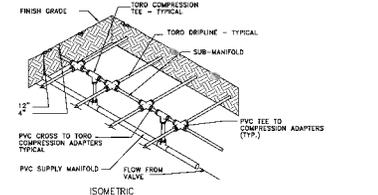
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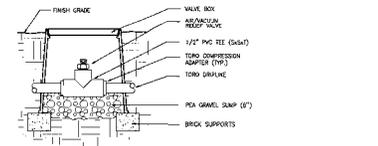
REDUCED PRESSURE ASSEMBLY ABOVE GRADE W/ STRAINER
NOT TO SCALE



QUICK COUPLING VALVE ASSEMBLY
NOT TO SCALE



CENTER-FEED SUPPLY SUB MANIFOLD (DRIP ZONE)
NOT TO SCALE



1/2\"/>

HYDR-ZONE	WINTER NOV.-MAR.	SPRING APR.-MAY	SUMMER JUN.-AUG.	FALL SEP.-OCT.	TOTAL GALLONS
(DRIP) 1	8.5/P 8/1 1/1N	8.5/P 15/L 3/N	8.5/P 13/L 3/N	8.5/P 15/L 1/N	658.55
(DRIP) 2	9.7/P 7/L 1/N	9.7/P 15/L 3/N	9.7/P 12/L 3/N	9.7/P 8/L 1/N	713.70
(DRIP) 3	10.2/P 9/L 1/N	10.2/P 9/L 2/N	10.2/P 11/L 4/N	10.2/P 12/L 2/N	942.60
(DRIP) 4	11.5/P 7/L 1/N	11.5/P 14/L 3/N	11.5/P 12/L 3/N	11.5/P 14/L 1/N	819.90
(DRIP) 5	10.6/P 10/L 1/N	10.6/P 10/L 2/N	10.6/P 13/L 4/N	10.6/P 10/L 2/N	1041.00
(BUBBLER) 6	10.6/P 7/L 1/N	10.6/P 15/L 3/N	10.6/P 12/L 3/N	10.6/P 12/L 1/N	783.45
(BUBBLER) 7	4.0/P 1/L 1/N	4.0/P 1/L 1/N	4.0/P 2/L 1/N	4.0/P 1/L 1/N	7.20
(BUBBLER) 8	12.0/P 1/L 1/N	12.0/P 1/L 1/N	12.0/P 2/L 1/N	12.0/P 1/L 1/N	21.60
(BUBBLER) 9	1.26/T 1.26/T	2.52/T 2.52/T	6.30/T 6.30/T	2.52/T 2.52/T	21.60
(BUBBLER) 10	3.60/T 6.0/P 1/L 1/N	7.20/T 6.0/P 1/L 1/N	18.0/T 6.0/P 1/L 1/N	7.2/T 6.0/P 1/L 1/N	28.80
SYSTEM TOTAL PER SEASON	504.36	1,008.72	2,521.8	1,008.72	5,043.60

HYDR-ZONE	WINTER NOV.-MAR.	SPRING APR.-MAY	SUMMER JUN.-AUG.	FALL SEP.-OCT.	TOTAL GALLONS
(DRIP) 1	8.5/P 5/L 1/N	8.5/P 10/L 1/N	8.5/P 10/L 2/N	8.5/P 10/L 1/N	437.70
(DRIP) 2	9.7/P 5/L 1/N	9.7/P 10/L 1/N	9.7/P 12/L 2/N	9.7/P 12/L 1/N	475.80
(DRIP) 3	10.2/P 6/L 1/N	10.2/P 11/L 1/N	10.2/P 11/L 2/N	10.2/P 12/L 1/N	628.40
(DRIP) 4	11.5/P 5/L 1/N	11.5/P 9/L 1/N	11.5/P 12/L 2/N	11.5/P 9/L 1/N	546.60
(DRIP) 5	10.6/P 6/L 1/N	10.6/P 13/L 1/N	10.6/P 11/L 3/N	10.6/P 10/L 2/N	694.00
(BUBBLER) 6	10.6/P 9/L 1/N	10.6/P 10/L 1/N	10.6/P 12/L 2/N	10.6/P 12/L 1/N	522.30
(BUBBLER) 7	4.0/P 1/L 1/N	4.0/P 1/L 1/N	4.0/P 2/L 1/N	4.0/P 1/L 1/N	4.80
(BUBBLER) 8	12.0/P 1/L 1/N	12.0/P 1/L 1/N	12.0/P 2/L 1/N	12.0/P 1/L 1/N	4.80
(BUBBLER) 9	1.44/T 1.44/T	2.88/T 2.88/T	7.20/T 7.20/T	2.88/T 2.88/T	14.40
(BUBBLER) 10	10.0/P 1/L 1/N	11.0/P 1/L 1/N	11.0/P 1/L 1/N	11.0/P 1/L 1/N	7.20
SYSTEM TOTAL PER SEASON	336.24	672.48	1,681.20	672.48	3,362.40

IRRIGATION SCHEDULE FOR ESTABLISHMENT PERIOD

IRRIGATION SCHEDULE FOR ESTABLISHED LANDSCAPE

Preliminary Not For Construction

CLIENT: **bp**

ARCO
BP WEST COAST PRODUCTS, LLC

BARGHAUSEN
CONSULTING ENGINEERS, INC.

18215 22ND AVENUE SOUTH
KENT, WA 98032
(425)251-8222
(425)251-8762 FAX

CAL. ENGINEERING, LAND PLANNING,
SURVEYING, ENVIRONMENTAL SERVICES

NO.	DATE	REVISION DESCRIPTION
1	01/23/12	REV 011 COMMENTS
2	01/23/12	REV 012 COMMENTS
3	01/23/12	REV 013 COMMENTS
4	01/23/12	REV 014 COMMENTS
5	01/23/12	REV 015 COMMENTS
6	01/23/12	REV 016 COMMENTS
7	01/23/12	REV 017 COMMENTS
8	01/23/12	REV 018 COMMENTS
9	01/23/12	REV 019 COMMENTS
10	01/23/12	REV 020 COMMENTS

SEAL: **REGISTERED LANDSCAPE ARCHITECT**
APRIL R. S. SUTHERLAND
12/21/12
566 S. 9TH ST.
SACRAMENTO, CA 95833
(916) 441-2777 (OFFICE)

DEVELOPMENT INFORMATION:
2900 MDC
2900 AM/PM C-STORE
CAR WASH
8 MPD CANOPY

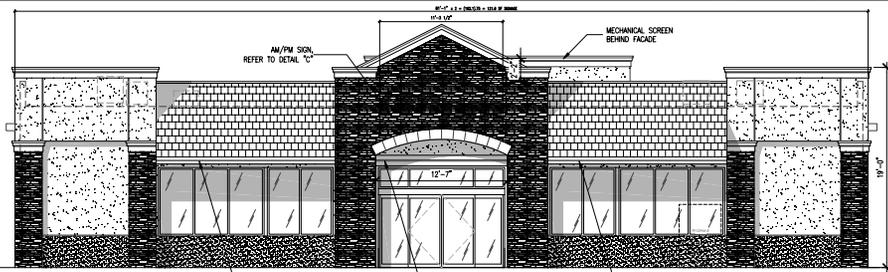
ONE ADDRESS:
255 E. HARNEY LANE @
STOCKTON STREET
LODI, CALIFORNIA

NTI
RECORD NO. 15441
DESIGN BY: MDC
DRAWN BY: MDC
CHECKED BY: MDC
VISION: MDC
PROJECT NO.: 15441

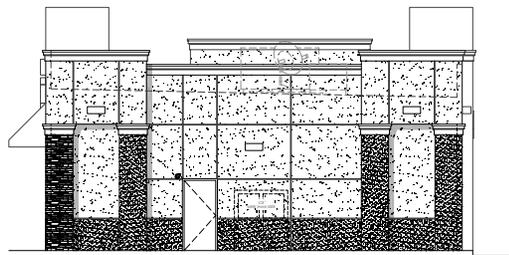
DRAWING TITLE:
PRELIMINARY IRRIGATION NOTES

SHEET NO.: **L-4**

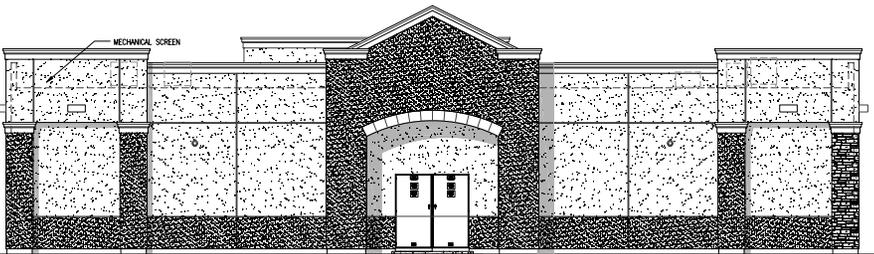
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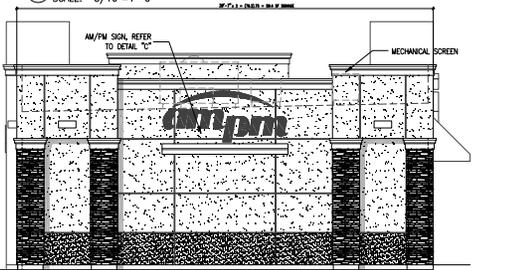
1 PROPOSED SOUTH ELEVATION
SCALE: 3/16"=1'-0"



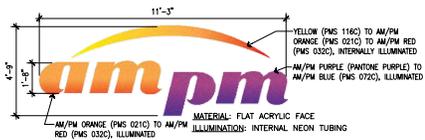
2 PROPOSED EAST ELEVATION
SCALE: 3/16"=1'-0"



3 PROPOSED NORTH ELEVATION
SCALE: 3/16"=1'-0"



4 PROPOSED WEST ELEVATION
SCALE: 3/16"=1'-0"



C 21" AM/PM CHANNEL LETTERS
SCALE: NTS

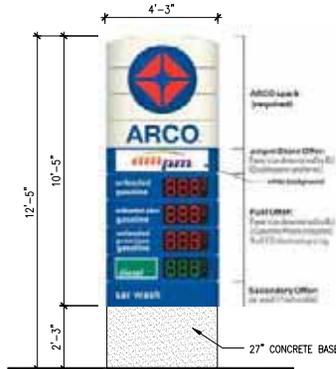


F CARWASH BUILDING SIGN
SCALE: NTS



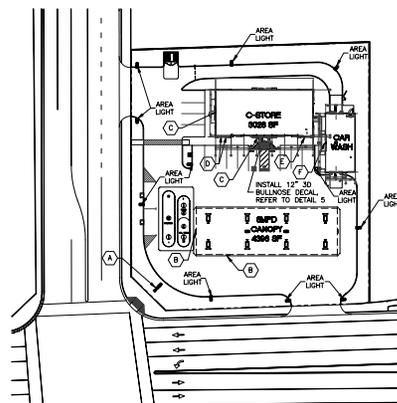
8 CANOPY DECAL ON ACM FASCIA W/LED
SCALE: NTS

7 AM/PM DECAL ON ACM BACKING
SCALE: NTS



A MONUMENT SIGN GRAPHICS
SCALE: NTS

SIGNAGE NOTES:
ZONING: C-1 NC DISTRICT:
SIGNAGE = 2 SQFT / FRONTAGE LFT x.75 WHEN LOCATED ON CORNER LOT.
ANY ONE SIGN SHALL NOT EXCEED 300 SQFT.
HEIGHT NOT GREATER THAN 35 FT
NOT CLOSER THAN 2 FT TO BACK OF CURB
DIRECTIONAL SIGNAGE EXEMPT



SIGN SITE PLAN
SCALE: NTS.

SIGNAGE TABULATION	ALLOWED	PROVIDED
C-STREET SOUTH ELEVATION	121.6 SQFT	38.0 SQFT
WEST ELEVATION	59.4 SQFT	38.0 SQFT
CANOPY SOUTH ELEVATION		16.8 SQFT
WEST ELEVATION		16.8 SQFT
CARWASH WEST ELEVATION	73.6 SQFT	12.6 SQFT
TOTAL TABULATED SIGNAGE:	254.6 SQFT	177.0 SQFT

*FREE-STANDING SIGN - TEXT GRAPHICS 2x 17.6 SQFT**
ALLOWABLE FREE-STANDING SIGN AREA: 300.0 SQFT 2x 51.1 SQFT**
TOTAL FREE-STANDING SIGN: 300 SQFT 137.4 SQFT
**ALLOWED BY ART.III - NEIGHBORHOOD COM. ZONE - SECT. 17.63.220
***EACH FACE (TWO FACES TOTAL)

SITE SIGNAGE TABLE			
KEY	SIGN	QTY.	AREA
A	MAIN I.D. SIGN/PRICE SIGN (71.7 sf ea face)	1	137.4 sf
B	CANOPY SIGN (SPARK + LETTERS; 16.8 SF)	2	33.6 sf
C	21" AM/PM CHANNEL LETTERS (38.0 sf ea.)	2	76.0 sf
D	(NOT USED)		
E	(NOT USED)		
F	CARWASH BUILDING IDENTITY SIGN	1	12.7 sf
TOTAL:			259.7 sf



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CIVIL ENGINEERING, LAND PLANNING,
SURVEYING, ENVIRONMENTAL SERVICES

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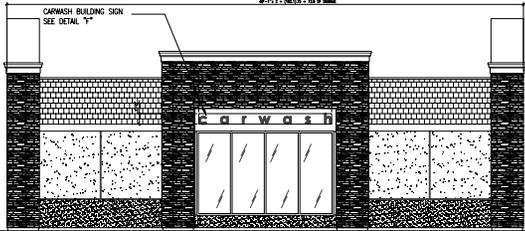
THIS SHEET IS FOR INFORMATION ONLY

DEVELOPMENT INFORMATION:
2900 MCD
2900 AM/PM C-STREET
CAR WASH
8 MPD CANOPY

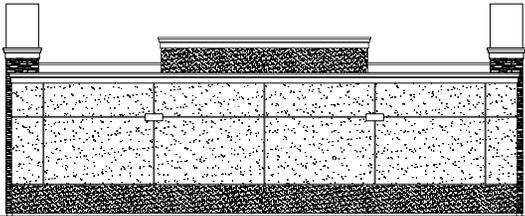
SITE ADDRESS:
285 E. HARNEY LANE @
STOCKTON STREET
LOS ANGELES, CALIFORNIA

NTI
DESIGNED BY: BBN ALLIANCE ZACH
CHECKED BY: BBN ALLIANCE ZACH
DRAWN BY: BBN ALLIANCE ZACH
VERSION: MDC PROJECT NO: 00-00-00
PROJECT NO: 15441
DRAWING TITLE: SIGN SITE PLAN AND ELEVATIONS
SHEET NO:

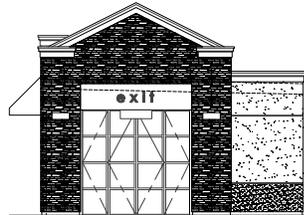
SNA.1



1 PROPOSED WEST ELEVATION
SCALE: 3/16"=1'-0"



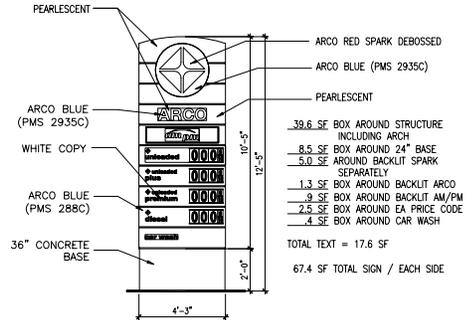
3 PROPOSED EAST ELEVATION
SCALE: 3/16"=1'-0"



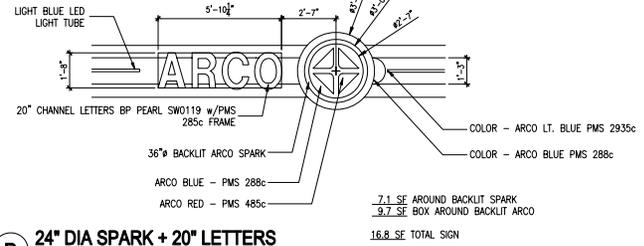
2 PROPOSED SOUTH ELEVATION
SCALE: 3/16"=1'-0"



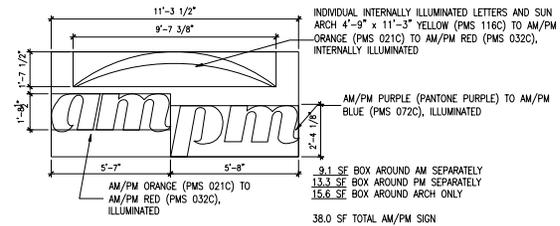
4 PROPOSED NORTH ELEVATION
SCALE: 3/16"=1'-0"



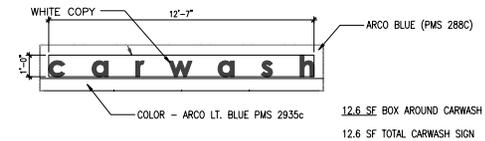
A MONUMENT SIGN
SCALE: 3/8"=1'-0"



B 24" DIA SPARK + 20" LETTERS
SCALE: 3/8"=1'-0"



C am/pm 21" CHANNEL LETTER SIGN
SCALE: 3/8"=1'-0"



F CARWASH BUILDING SIGN
SCALE: 3/8"=1'-0"

CLIENT

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CIVIL ENGINEERING, LAND PLANNING,
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THIS SHEET IS FOR INFORMATION ONLY

DEVELOPMENT INFORMATION:

2900 MCD

2900 AM/PM C-STORE

CAR WASH

8 MPD CANOPY

SITE ADDRESS:

285 E. HARNEY LANE @

STOCKTON STREET

LOS ANGELES, CALIFORNIA

NTI

DESIGNED BY: ALLIANCE ZADK

DRAWN BY: BP REP/NTI

VERSION: MDC PROJECT NO: 00-00-00

DRAWING TITLE: SIGNAGE DETAILS

SHEET NO:

SNA.2

DATE: 11/14/11
 DRAWN BY: NTI
 CHECKED BY: NTI
 PROJECT NO: 00-00-00
 DRAWING TITLE: SIGNAGE DETAILS
 SHEET NO: SNA.2



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CIVIL ENGINEERING, LAND PLANNING,
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NO.	DATE	REVISION DESCRIPTION
1	06/17/11	BUILDING PLAN REVISION COMPLETE

GENERAL NOTES:

- A. DIMENSION LINES ARE FROM FACE OF FRAMING OR FOUNDATION UNLESS NOTED OTHERWISE.
- B. FACE OF FRAMING ON EXTERIOR WALL IS SAME AS OUTSIDE FACE OF FOUNDATION WALL.
- C. METAL STUDS MAY BE USED IN PLACE OF WOOD STUDS. PLAN DIMENSIONS ARE BASED ON WOOD STUDS.
- D. MOISTURE RESISTANT GYPSUM TO BE USED IN COOLER ROOMS, BEHIND HOP LINE, AND ADJACENT TO COOLERS. FOR SPECIFIC SUBSTRATE REQUIREMENTS, REFER TO INTERIOR ELEVATIONS.
- E. PROVIDE BLOCKING AS REQ'D FOR ALL EQUIPMENT AND ACCESSORIES.
- F. PROVIDE CLEAR SILICONE SEALANT FOR ALL WALL TRANSITIONS.
- G. REFER TO DETAILS 3 & 4/A3.3 FOR ATTACHMENT OF ALL WALLS TO STRUCTURE ABOVE.
- H. FOR FLOORING LAYOUT REFER TO SHEET A1.2
- I. FOR CEILING LAYOUT REFER TO SHEET A1.3
- J. FOR FINISH MATERIAL REFER TO INTERIOR ELEVATIONS SHEET A2.3, A2.4, AND A2.5
- K. FOR INTERIOR GRAPHIC ELEVATIONS REFER TO SHEET A2.6
- L. FOR DOOR AND WINDOW DETAILS REFER TO SHEET A5.3
- M. FOR EQUIPMENT PLAN REFER TO SHEET Q1.1
- N. FOR EQUIPMENT SCHEDULE REFER TO SHEET Q2.1
- O. ALL MATERIALS AND APPLICATION TO MEET OR EXCEED LOCAL OR STATE REQUIREMENTS
- P. FOR DOOR SCHEDULE & ELEVATIONS REFER TO SHEET A5.3

KEYED NOTES:

- 05.050 PRE-FINISHED METAL LATTICE. REFER TO DETAIL 4/A5.1
- 06.130 INSTALL NEW LOW WALL PARTITION WITH SWINGING DOOR
- 06.170 CANOPY ABOVE
- 07.713 ROOF LADDER HATCH
- 07.716 ROOF LADDER
- 08.410 ALUMINUM ENTRANCE AND STOREFRONT SYSTEM. REFER TO A5.3 & SPEC.
- 08.415 AUTOMATIC BI-PARTING ENTRANCE DOORS
- 09.260 CLEAR PLASTIC CORNER GUARDS
- 11.005 RECESSED KNOX BOX. INSTALL PER MANUFACTURERS SPECIFICATIONS
- 11.400 FOR COOLER AND FREEZER INFORMATION AND DIMENSIONS, REFER TO SHEET Q1.2
- 11.401 MAINTAIN MINIMUM 1" AIR GAP BEHIND COOLERS AND FREEZERS ADJACENT TO WALLS. CLOSE SPACE AT ENDS WITH 2"x2" ALUMINUM ANGLE.
- 11.456 PROVIDE FILLER COUNTER AND MILLWORK FOR GAP WITH FINISHES TO MATCH

DEVELOPMENT INFORMATION
 2900 MCD
 2900 AM/PM C-STORE
 CAR WASH
 8 MPD CANOPY

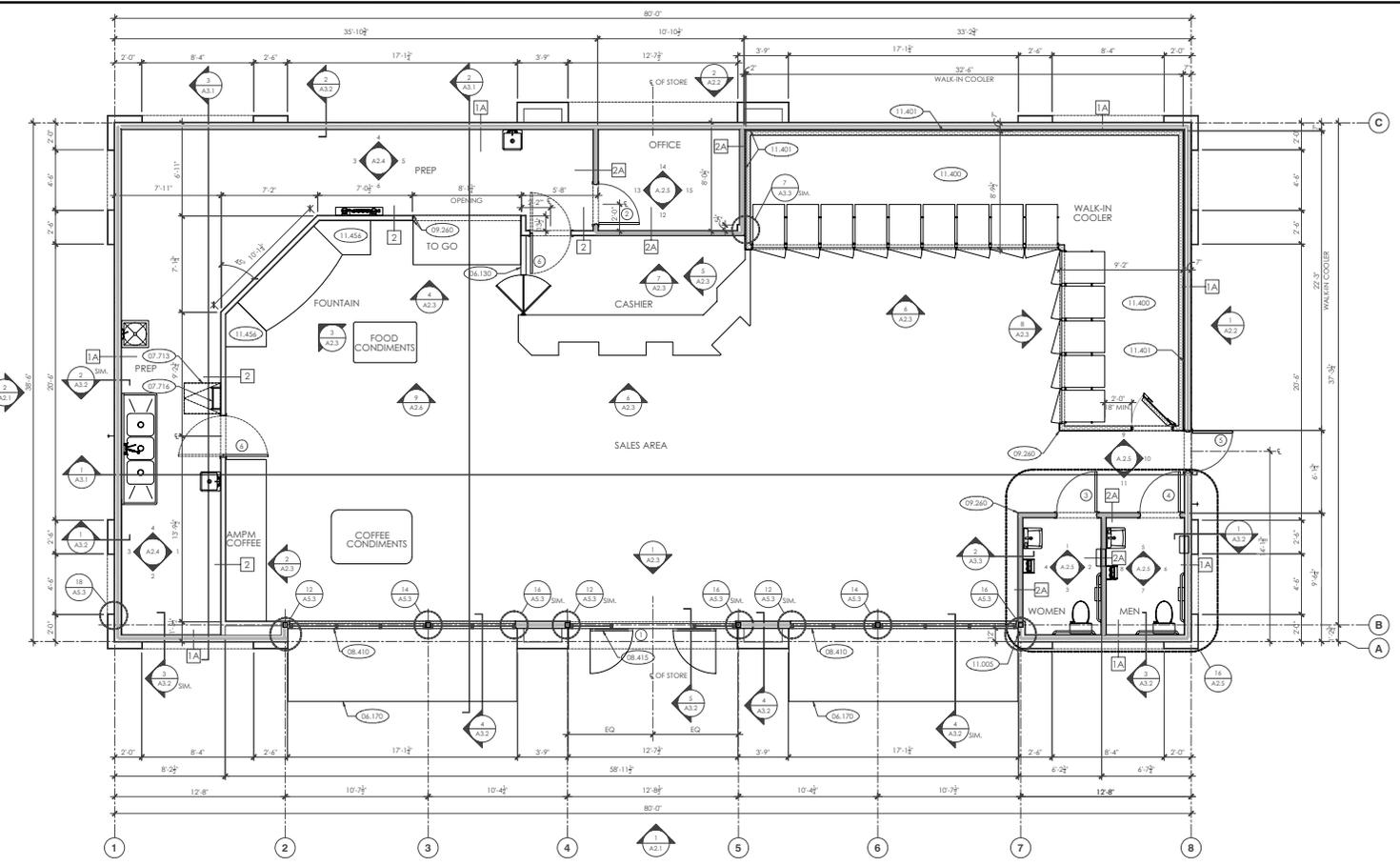
THE ADDRESS:
 288 E. HARNEY LANE @
 BROOKTON STREET
 LOS ANGELES, CALIFORNIA

NTI
 DESIGNED BY: JMW ALLIANCE TEAM
 CHECKED BY: DMD BY: JMW ALLIANCE TEAM
 DRAWN BY: JMW ALLIANCE TEAM
 VERSION: MDC PROJECT NO. 00-00-00 15441
 DRAWING TITLE:

CONSTRUCTION FLOOR PLAN

SHEET NO.

A1.1

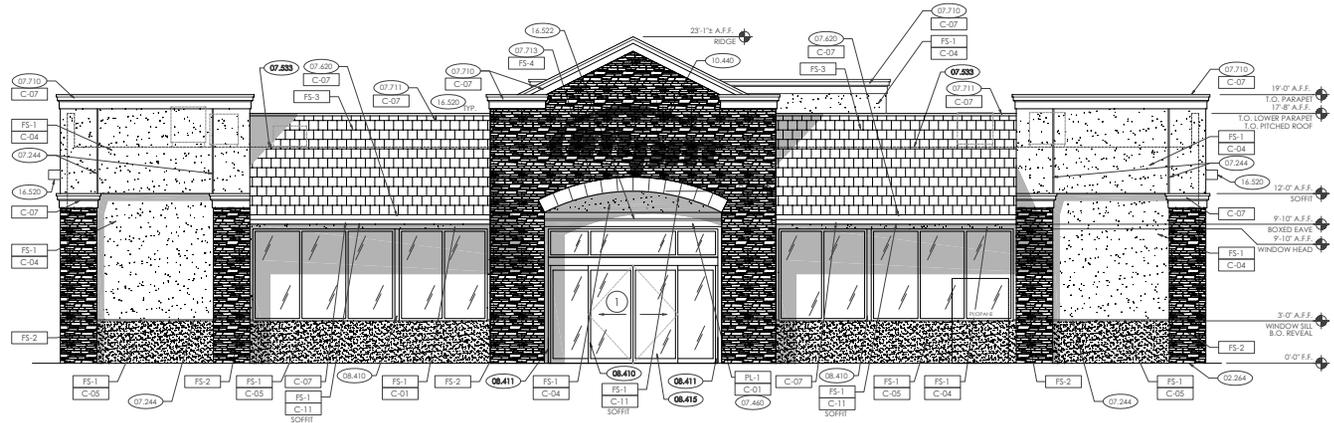


1 CONSTRUCTION FLOOR PLAN
 1/4"=1'-0"

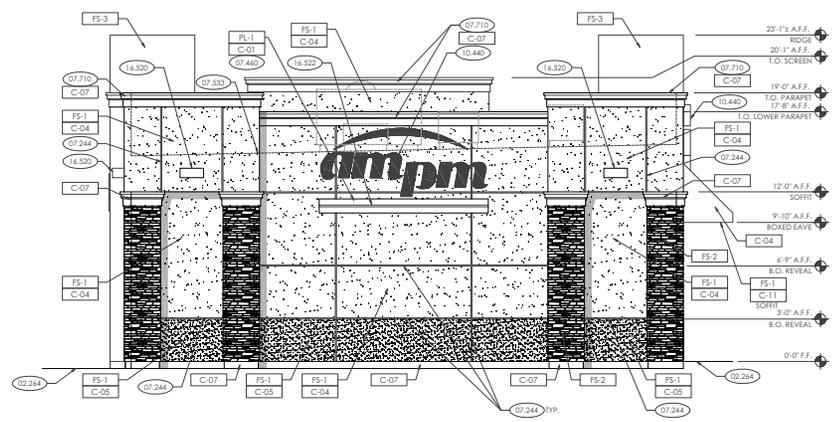
OCCUPANCY LOAD			
USE	SQ. FT.	FACTOR	OCC. LOAD
SALES AREA	1,651	.30	55.03
OFFICE/CASHIER	238	100	2.38
RESTROOMS *	112	-	-
COOLER	417	-	-
BACK ROOM	492	200	2.46
TOTAL: 2,910 SQ. FT.			59.87
OCCUPANCY LOAD = 60			
REQUIRED EXITS: 2			
PROVIDED EXITS: 2			
TOTAL = 2			
OCCUPANCY LOAD BY TABLE 1004.1.1 CBC			
* INDICATED ACCESSORY USE			

WALL LEGEND

[1A]	WOOD FRAMED WALL
[2]	2x4 STUDS @ 16" O.C. W/ THERMAL INSULATION
[2A]	2x4 STUDS @ 16" O.C. W/ SOUND BATT INSULATION
[Hatched]	INSULATED WALL
[Dashed]	PREFABRICATED COOLER UNIT



1 SOUTH ELEVATION
1/4"=1'-0"



2 WEST ELEVATION
1/4"=1'-0"

GENERAL NOTES:

A. REVEAL LOCATIONS IN FINISH SYSTEM SHOWN ARE TO ALIGN AS CLOSE AS POSSIBLE.
B. DIMENSIONS ARE TO THE CENTERLINE OF REVEALS

KEYED NOTES:

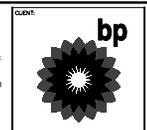
- 02.244 GRADE, REFER TO CIVIL PLAN
- 07.243 TERMINATION REFER TO DETAIL 9/A5.1
- 07.244 REVEAL IN FINISH REFER DETAIL 9/A5.1
- 07.245 CONTROL JOINT IN FINISH SYSTEM, MAXIMUM 20'-0" O.C.
- 07.460 ACM PANEL
- 07.533 APPROXIMATE ROOF LINE
- 07.620 6" PRE-FINISHED 22-GA. METAL GUTTER
- 07.621 3"x5" 22-GA. GALVANIZED PRE-FINISHED METAL DOWNSPOUT
- 07.710 SHOP FORMED POLYSTYRENE CORNICE
- 07.711 SHOP FORMED PRE-FINISHED METAL COPING CAP
- 07.712 PRE-FINISHED 22-GA. METAL FLASHING
- 07.713 STONE ARCHWAY
- 07.714 ELECTRICAL BOX STONE MFR: CULTURED STONE OR EQUAL MODEL: C3V-43924 COLOR: MOCHA
- 08.410 ALUMINUM ENTRANCE AND STOREFRONT SYSTEM, REFER TO A5.3 & SPEC.
- 08.411 ALUMINUM CLADDING (OMIT @ SIM)
- 08.415 AUTOMATIC BI-PARTING ENTRANCE DOORS, REFER TO DETAIL 17/A5.3
- 10.440 INTERNALLY ILLUMINATED SIGN BY SIGNAGE CONTRACTOR
- 10.442 64"Wx54"H POP POSTER, BY SIGNAGE CONTRACTOR
- 10.443 45"Wx52"H POP POSTER, BY SIGNAGE CONTRACTOR
- 10.446 VINYL DECAL APPLIED TO ACM PANEL
- 15.734 ROOF MECHANICAL UNITS
- 15.739 PRE-FINISHED HOT-DIP GALVANIZED STEEL TRELLIS, REFER TO DETAIL 4/A5.1
- 16.520 EXTERIOR LIGHT, REFER TO LIGHTING PLAN SHEET E1.3
- 16.521 GOOSENECK LIGHTING FIXTURES
- 16.522 LED LIGHT TUBE, SURFACE MOUNTED TO FACE OF ACM PANEL

EXTERIOR FINISHES:

- FL-1 ALUMINUM ACM PANEL
- FS-1 STUCCO
- FS-2 MANUFACTURED STONE VENER MFR: CULTURED STONE PRODUCT: PRO-FIT LEDGESTONE COLOR: 'AUTUMN'
- FS-3 SLATE ROOF MFR: MONIER LERLE PRODUCT: SAKONY 400 COLOR: HILLSIDE 2PAC3935
- FS-4 STONE LINTEL COLOR: NATURAL

EXTERIOR PAINTS:

- C-01 COLOR: BP PEARL - RAL 1013
- C-02 COLOR: BP DANK PEARL - RAL 9005 'INDIAN LEGEND'
- C-03 COLOR: BP DANK PEARL - RAL 9005 'DESERT VALLEY'
- C-04 COLOR: ampm TAN
- C-05 COLOR: ampm DARK TAN
- C-06 COLOR: ampm YELLOW
- C-07 COLOR: BP WARM GREY
- C-08 COLOR: ampm RED
- C-09 COLOR: ampm PURPLE
- C-10 COLOR: ampm BLUE
- C-11 COLOR: BP HIGH HINDING WHITE
- C-12 COLOR: ARCO BLUE
- C-13 COLOR: ARCO LIGHT BLUE
- C-14 COLOR: OMEGA YELLOW



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CIVIL ENGINEERING, LAND PLANNING,
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DEVELOPMENT INFORMATION:
2900 MCD
2900 AM/PM C-STORE
CAR WASH
8 MPD CANOPY

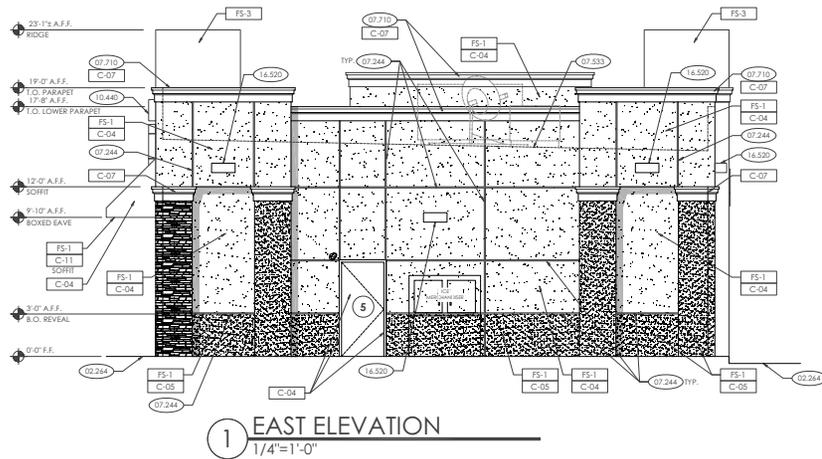
THE ADDRESS:
288 E. HARNEY LANE @
STOCKTON STREET
LOCAL, CALIFORNIA

NTI
DESIGNED BY: JMW ALLIANCE TEAM
CHECKED BY: JMW ALLIANCE TEAM
DRAWN BY: JMW ALLIANCE TEAM
VERSION: MDC 00-00-00
PROJECT NO: 15441

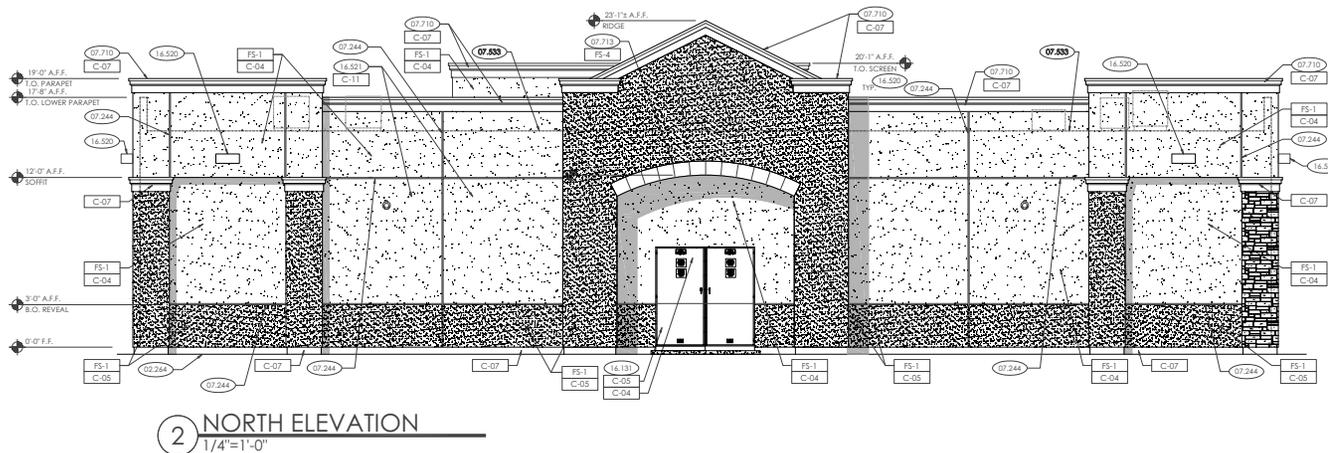
DRAWING TITLE:
SOUTH & WEST
EXTERIOR ELEVATIONS

SHEET NO:

A2.1



1 EAST ELEVATION
1/4"=1'-0"



2 NORTH ELEVATION
1/4"=1'-0"

GENERAL NOTES:

A. REVEAL LOCATIONS IN FINISH SYSTEM SHOWN ARE TO ALIGN AS CLOSELY AS POSSIBLE.

B. DIMENSIONS ARE TO THE CENTERLINE OF REVEALS

KEYED NOTES:

- 02.244 GRADE, REFER TO CIVIL PLAN
- 07.243 TERMINATION REFER TO DETAIL
- 9/A5.1
- 07.244 1" REVEAL IN FINISH REFER DETAIL
- 8/A5.1
- 07.246 CONTROL JOINT IN FINISH SYSTEM, MAXIMUM 20' O.C.
- 07.460 ACM PANEL
- 07.533 APPROXIMATE ROOF LINE
- 07.710 SHOP FORMED POLYSTYRENE CORNER
- 07.711 SHOP FORMED PREFINISHED METAL COPING CAP
- 07.712 PRE FINISHED 22 GA. METAL FLASHING
- 07.713 STONE ARCHWAY
- 08.415 AUTOMATIC BI-PARTING ENTRANCE DOORS, REFER TO DETAIL
- 17/A5.3
- 10.440 INTERNALLY ILLUMINATED SIGN BY SIGNAGE CONTRACTOR
- 10.443 NOT USED
- 10.446 VINYL DECAL APPLIED TO ACM PANEL
- 10.730 NOT USED
- 15.734 ROOF MECHANICAL LIMITS
- 15.739 PRE FINISHED HOT DIP GALVANIZED STEEL TRELLIS, REFER TO DETAIL 4/A5.1
- 16.131 ELECTRICAL CABINETS, REFER TO SHEET EI.2
- 16.520 EXTERIOR LIGHT, REFER TO LIGHTING PLAN SHEET EI.3

EXTERIOR FINISHES:

- PL-1 ALUMINUM ACM PANEL
- FS-1 STUCCO
- FS-2 MANUFACTURED STONE VENEER MFG: CULTURED STONE PRODUCT, PRO-FIT LEDGESTONE COLOR: "AUSUMBY"
- FS-3 SLATE ROOF MFG: MOHNER LITELLE PRODUCT: SARKONY 400 COLOR: HELLSIDE 2PAC13P35
- FS-4 STONE LINTEL COLOR: NATURAL

EXTERIOR PAINTS:

- C-01 COLOR: BP PEARL - RAL 1013 CO: #A0876 "INDIAN LEGEND"
- C-02 COLOR: BP DARK PEARL CO: #A0767 "DESERT VALLEY"
- C-04 COLOR: ampm TAN CO: #A0717 "INDIAN CORBY"
- C-05 COLOR: ampm DARK TAN CO: #A0763 "ONCROUSERS FAN"
- C-06 COLOR: ampm YELLOW PMS 114C
- C-07 COLOR: BP WARM GREY CO: #A0760 "GREY MOUNTAIN"
- C-08 PMS 033C
- C-09 COLOR: ampm PURPLE PANTONE PURPLE
- C-10 COLOR: ampm BLUE PMS 037C
- C-11 COLOR: BP HIGH HIDING WHITE CO: BP HIGH HIDING WHITE (CUSTOM)
- C-12 COLOR: ARCO BLUE PMS 288
- C-13 COLOR: ARCO LIGHT BLUE PMS 293S
- C-14 COLOR: OMEGA YELLOW CO: #A0775



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CIVIL ENGINEERING, LAND PLANNING,
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DEVELOPMENT INFORMATION:
2900 MCD
2900 AM/PM C-STORE
CAR WASH
8 MPD CANOPY

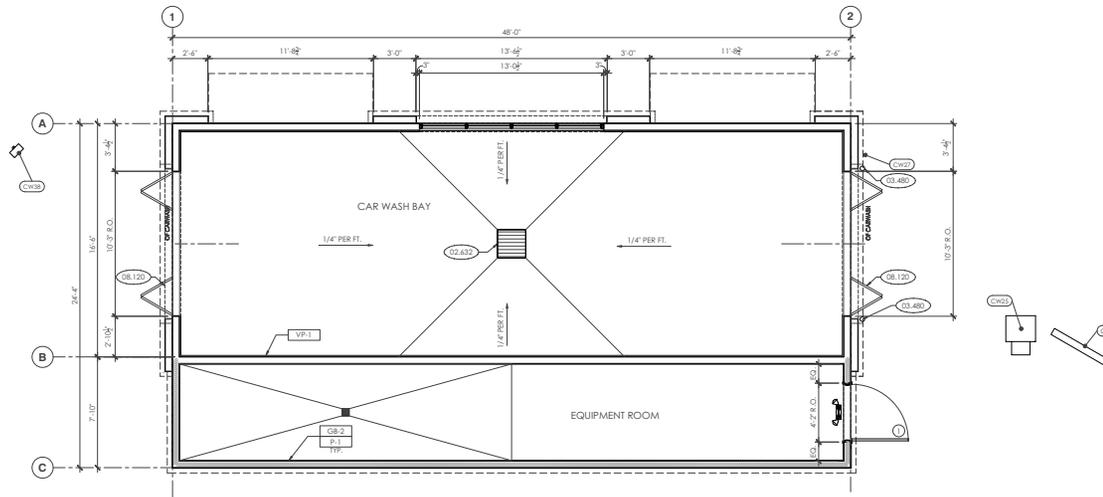
THE ADDRESS:
2805 E. HARNEY LANE @
BROOKTON STREET
LOS ANGELES, CALIFORNIA

NTI
DESIGNED BY: JMW ALLIANCE TEAM
DRAWN BY: JMW ALLIANCE TEAM
VERSION: MDC PROJECT NO: 00-00-00 15441

DRAWING TITLE:
EAST & NORTH
EXTERIOR ELEVATIONS

SHEET NO.

A2.2



1 CONSTRUCTION FLOOR PLAN
1/4"=1'-0"

GENERAL NOTES:

- A. DIMENSION LINES ARE FROM FACE OF FRAMING OR FOUNDATION UNLESS NOTED OTHERWISE.
- B. FACE OF FRAMING ON EXTERIOR WALL IS SAME AS OUTSIDE FACE OF FOUNDATION WALL.
- C. PROVIDE BLOCKING AS REQ'D FOR ALL EQUIPMENT AND ACCESSORIES.
- D. FOR CEILING LAYOUT REFER TO SHEET **CW1.2**.
- E. FOR DOOR AND WINDOW DETAILS REFER TO SHEET **CWAS.2**.
- F. ALL MATERIALS AND APPLICATION TO MEET OR EXCEED LOCAL OR STATE REQUIREMENTS.
- G. REFER TO CARWASH EQUIPMENT INSTALLATION MANUAL AND SUBSEQUENT SHOP DRAWINGS FOR FURTHER INFORMATION. IF THE INFORMATION PROVIDED IN THESE DOCUMENTS IS CONFLICTING WITH THE INSTALLATION MANUAL PROVIDED BY THE CARWASH INSTALLER, THE G.C. IS RESPONSIBLE TO COORDINATE THE DIFFERENCES PRIOR TO CONSTRUCTION AND AVOID CHANGE ORDERS.

KEYED NOTES:

- 02.632 PREFABRICATED CATCH BASIN, BURN #807
- 03.480 STEEL BOLLARD, REFER TO DETAIL **3/C6.2**
- 08.416 PRE-FINISHED ALUMINUM GLAZING SYSTEM
- 08.120 8"X10" ANODIZED ALUMINUM AND POLYCARBONATE DOOR
- CW25 AMT1 CS COIN BOX
- CW26 JUMBO LIGHTED SIGN
- CW27 LIGHTED 3-MESSAGE SIGN
- CW38 COUNT DOWN TIMER

WALL LEGEND

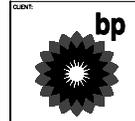
- WOOD FRAMED WALL
- 3 2x4 PRESSURE TREATED STUDS @ 16" O.C.
- 3A 2x4 PRESSURE TREATED STUDS @ 16" O.C. W/ THERMAL INSULATION
- INSULATED WALL

SUBSTRATE:

- CB-2 TYPE-F MOISTURE-RESISTANT GYPSUM BOARD
- VP-1 7" EXTRUTECH PANELS #P1300 WHITE W/ 6 MIL. PVC VAPOR BARRIER

INTERIOR PAINTS:

- P-1 CI 733 "CANDEL WAX" EGGSHELL FINISH



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14	-	-

DEVELOPMENT INFORMATION:
2900 MCD
2900 AM/PM C-STORE
CAR WASH
8 MPD CANOPY

THE ADDRESS:
288 E. HARNEY LANE @
STOCKTON STREET
LOS ANGELES, CALIFORNIA

NTI

DESIGNED BY: JMW ALLIANCE T&M

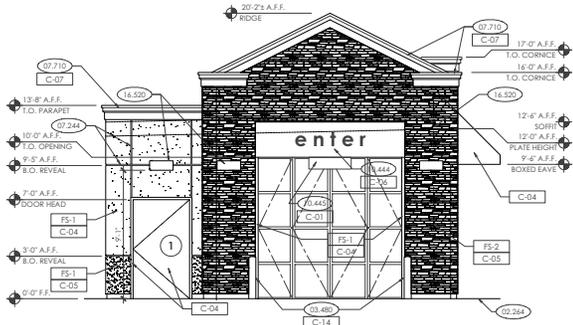
DRAWN BY: EMB (P. REPE)

VERSION: MDC PROJECT NO. 00-00-00 15441

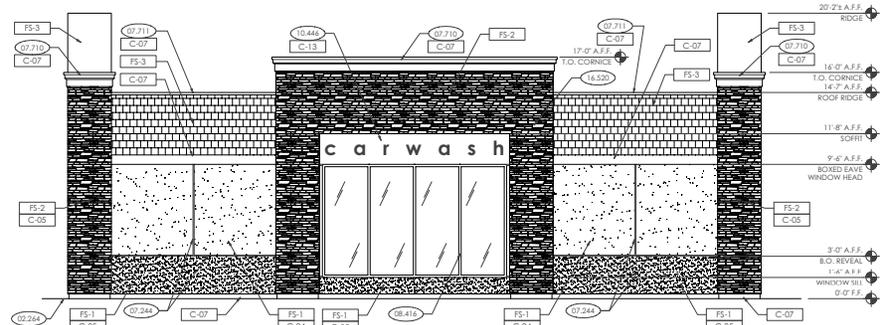
DRAWING TITLE:
CARWASH
CONSTRUCTION
FLOOR PLAN

SHEET NO.

CW1.1



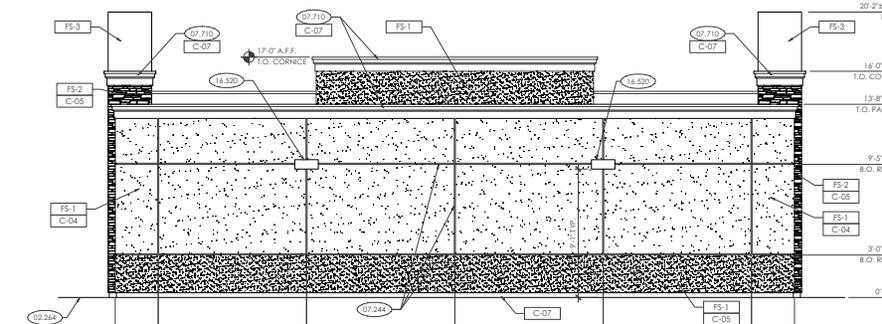
1 NORTH ELEVATION
1/4"=1'-0"



2 WEST ELEVATION
1/4"=1'-0"



3 SOUTH ELEVATION
1/4"=1'-0"



4 EAST ELEVATION
1/4"=1'-0"

GENERAL NOTES:
 A. REVEAL LOCATIONS IN FINISH SYSTEM SHOWN ARE TO ALIGN AS CLOSELY AS POSSIBLE.
 B. DIMENSIONS ARE TO THE CENTERLINE OF REVEALS.
KEYED NOTES:
 02.244 GRADE. REFER TO CIVIL PLAN 03.480 STEEL BOLLARD. REFER TO CIVIL PLAN 07.244 1" REVEAL IN FINISH REFER DETAIL 8/AS.1
 07.246 CONTROL JOINT IN FINISH SYSTEM. MAXIMUM 20'-0" O.C.
 07.710 SHOP FORMED POLYSTYRENE CORNICE. REFER TO DETAIL 1/AS.1
 07.711 SHOP FORMED PREFINISHED METAL CORNING CAP. REFER TO 2/CWAS.1
 07.714 ELECTRICAL BOX STONE MFR: CULTURED STONE OR EQUAL MODEL: CSV-439206 COLOR: MOCHA 08.416 PREFINISHED ALUMINUM GLAZING SYSTEM. REFER TO DETAIL 5/CWAS.2
 10.444 SIGN CENTERED ON OPENING 10.445 10"x6" "NO ENTRY" AT EXIT. REMOVABLE. CLEARANCE SIGN AND ENTRANCE. PROVIDED BY SIGNAGE CONTRACTOR
 14.520 EXTERIOR LIGHT. REFER TO LIGHTING PLAN

EXTERIOR FINISHES:
 FS-1 ALUMINUM ACM PANEL
 FS-1 3 COAT CEMENT PLASTER
 FS-2 STONE VENER
 MFR: NATURAL STONE RESOURCES PRODUCT: DESERT GOLD
 FS-3 SLATE ROOF
 MFR: MONIER LIFETILE PRODUCT: SAKOSKY 600 COLOR: HELLSIDE 2FAC33935
EXTERIOR PAINTS:
 C-01 COLOR: BP PEARL RAL 1013 (CI #A0283 "INDIAN LEGEND")
 C-02 COLOR: BP DARK PEARL (CI #A0767, "DESERT VALLEY")
 C-04 COLOR: ampm TAN (CI #A0717, "INDIAN CORN")
 C-05 COLOR: ampm DARK TAN (CI #A1761, "ONDSKIN TAN")
 C-06 COLOR: ampm YELLOW PMS 114C
 C-07 COLOR: BP WARM GREY (CI #A1540, "GREY MOUNTAIN")
 C-08 COLOR: ampm RED PMS 032C
 C-09 COLOR: ampm PURPLE PANTONE PURPLE
 C-10 COLOR: ampm BLUE PMS 072C
 C-11 COLOR: BP HIGH HIDING WHITE (CI BP HIGH HIDING WHITE (CUSTOM))
 C-12 COLOR: ARCO BLUE PMS 288
 C-13 COLOR: ARCO LIGHT BLUE PMS 293S
 C-14 COLOR: OMEGA YELLOW (CI #A0775)

CLIENT

bp FIRST COAST PROJECTS, LLC

18215 72ND AVENUE SOUTH
 KENT, WA 98032
 (425)251-6222
 (425)251-8752 FAX

CIVIL, ENGINEERING, LAND PLANNING,
 SURVEYING, ENVIRONMENTAL SERVICES

NO.	DATE	REVISION DESCRIPTION
1		
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DEVELOPMENT INFORMATION:
 2900 MCD
 2900 AM/PM C-STORE
 CAR WASH
 8 MPD CANOPY

THE ADDRESS:
 288 E. HARNEY LANE @
 STOCKTON STREET
 Lodi, CALIFORNIA

NTI

DESIGNED BY: JMW ALLIANCE TEAM
 CHECKED BY: EMB BY REVE
 DRAWN BY: JMW ALLIANCE NO
 VERSION: MDC PROJECT NO
 00-00-00 15441

DRAWING TITLE:
 CARWASH
 EXTERIOR
 ELEVATIONS

SHEET NO.

CW2.1



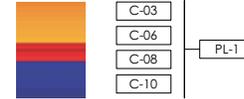
FS-3
COLOR: HILLSIDE



FS-2
COLOR: AUTUMN

EXTERIOR FINISHES:

- PL-1 ALUMINUM ACM PANEL
- FS-1 STUCCO
- FS-2 MANUFACTURED STONE VENEER
MFG: CULTURED STONE
PRODUCT: PRO-FIT LEDGESTONE
COLOR: "AUTUMN"
- FS-3 SLATE ROOF
MFG: MONIER LIFETILE
PRODUCT: SAXONY 600
COLOR: HILLSIDE 2FAC53935
- FS-4 STONE LINTEL
COLOR: NATURAL



- C-01 BP PEARL
ICI #A0083 "INDIAN LEGEND"
- C-02 BP DARK PEARL
ICI #A0767 "DESERT VALLEY"
- C-03 ampm ORANGE
PMS 021C
- C-04 ampm TAN
ICI #A0717 "INDIAN CORN"
- C-05 ampm DARK TAN
ICI #A1761 "ONIONSKIN TAN"
- C-06 ampm YELLOW
PMS 116C
- C-07 BP WARM GREY
ICI #A1860 "GREY MOUNTAIN"
- C-08 ampm RED
PMS 032C
- C-09 ampm PURPLE
PANTONE PURPLE
- C-10 ampm BLUE
PMS 072C
- C-11 BP HIGH HIDING WHITE
ICI BP HIGH HIDING WHITE
- C-12 ARCO BLUE
PMS 288
- C-13 ARCO LIGHT BLUE
PMS 2935
- C-14 OMEGA YELLOW
ICI #A0775



**255 E. HARNEY LANE @
STOCKTON STREET
LODI, CALIFORNIA**

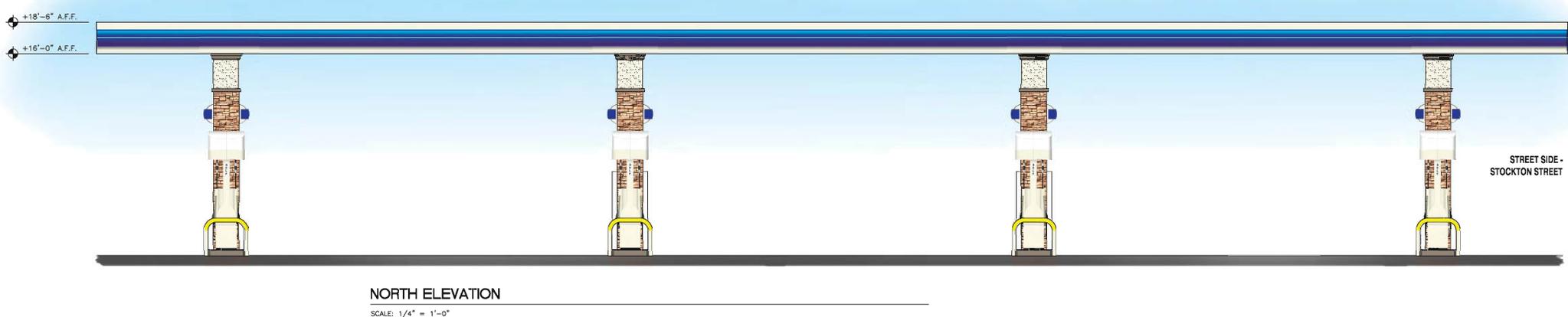
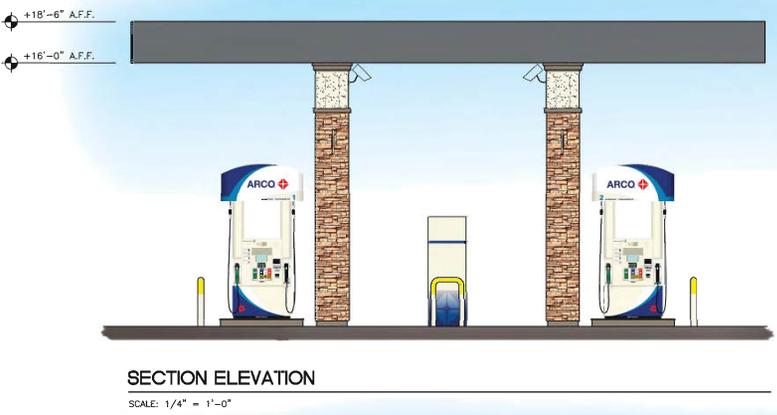
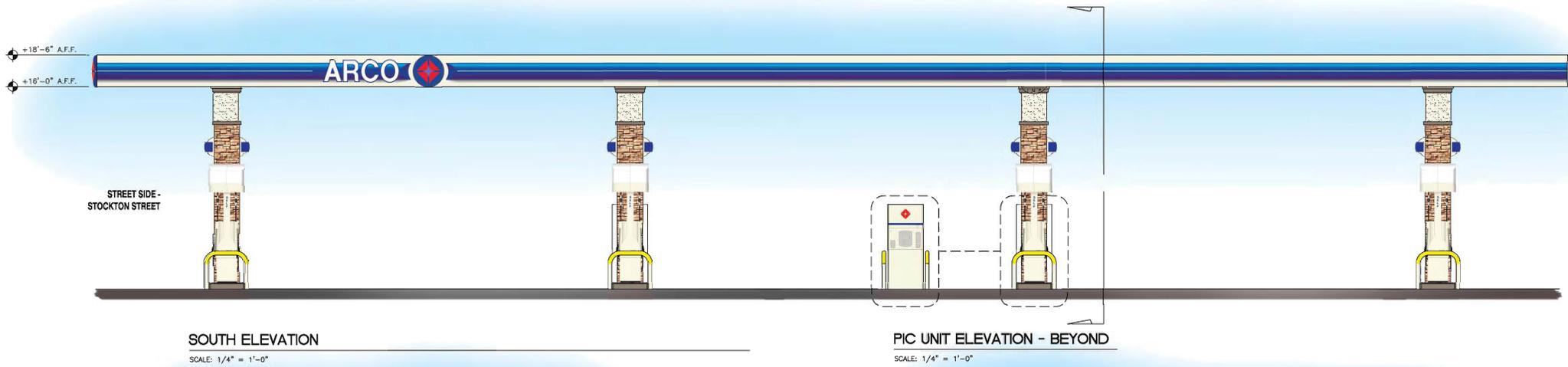
NTI

**2900 MDC
2900 AM/PM C-STORE
CAR WASH
8 MPD CANOPY**



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CIVIL ENGINEERING, LAND PLANNING,
SURVEYING, ENVIRONMENTAL SERVICES



Preliminary Not For Construction

Environmental Noise Assessment

Carwash at Stockton Street and Harney Lane

Lodi, California

BAC Job # 2012-013

Prepared For:

Mack Road AM/PM

Attn: Ken Dharni
6698 Mack Road
Sacramento, CA. 95821

Prepared By:

Bollard Acoustical Consultants, Inc.



Jonathan Lopez, Consultant

April 25, 2012



Introduction

The proposed project consists of the construction of a gas station, am/pm store, and carwash on the northeast corner of Stockton Street and Harney Lane in the City of Lodi, California. Existing land uses in the project vicinity include single-family residences to the immediate north and east, a preschool to the west, and agricultural uses to the south. The project site area is identified on Figure 1. Figure 2 shows the project site plan.

Due to the proximity of the proposed project to the existing residential uses to the immediate north and east, the project applicant has retained Bollard Acoustical Consultants, Inc. (BAC) to prepare an acoustical analysis for this project. The purposes of this analysis are to quantify noise levels associated with the proposed project, to assess the state of compliance of those noise levels with applicable noise standards, and if necessary, to recommend measures to reduce those noise levels to acceptable limits at the nearest noise sensitive uses.

Background on Noise and Acoustical Terminology

Noise is often described as unwanted sound. Sound is defined as any pressure variation in air that the human ear can detect. If the pressure variations occur frequently enough (at least 20 times per second), they can be heard and are called sound. The number of pressure variations per second is called the frequency of sound, and is expressed as cycles per second, called Hertz (Hz).

Measuring sound directly in terms of pressure would require a very large and awkward range of numbers. To avoid this, the decibel scale was devised. The decibel scale uses the hearing threshold (20 micropascals of pressure), as a point of reference, defined as 0 dB. Other sound pressures are then compared to the reference pressure, and the logarithm is taken to keep the numbers in a practical range. The decibel scale allows a million-fold increase in pressure to be expressed as 120 dB. Another useful aspect of the decibel scale is that changes in decibel levels correspond closely to human perception of relative loudness. Figure 3 illustrates common noise levels associated with various sources.

The perceived loudness of sound is dependent upon many factors, including sound pressure level and frequency content. However, within the usual range of environmental noise levels, perception of loudness is relatively predictable, and can be approximated by weighing the frequency response of a sound level meter by means of the standardized A-weighting network. There is a strong correlation between A-weighted sound levels (expressed as dBA) and community response to noise. For this reason, the A-weighted sound level has become the standard tool of environmental noise assessment. All noise levels reported in this section are in terms of A-weighted levels. Please see Appendix A for definitions of acoustical terminology used in this report.

Figure 1

Carwash at Stockton Street and Harney Lane - Lodi, California
Project Site Vicinity and Ambient Noise Measurement Location

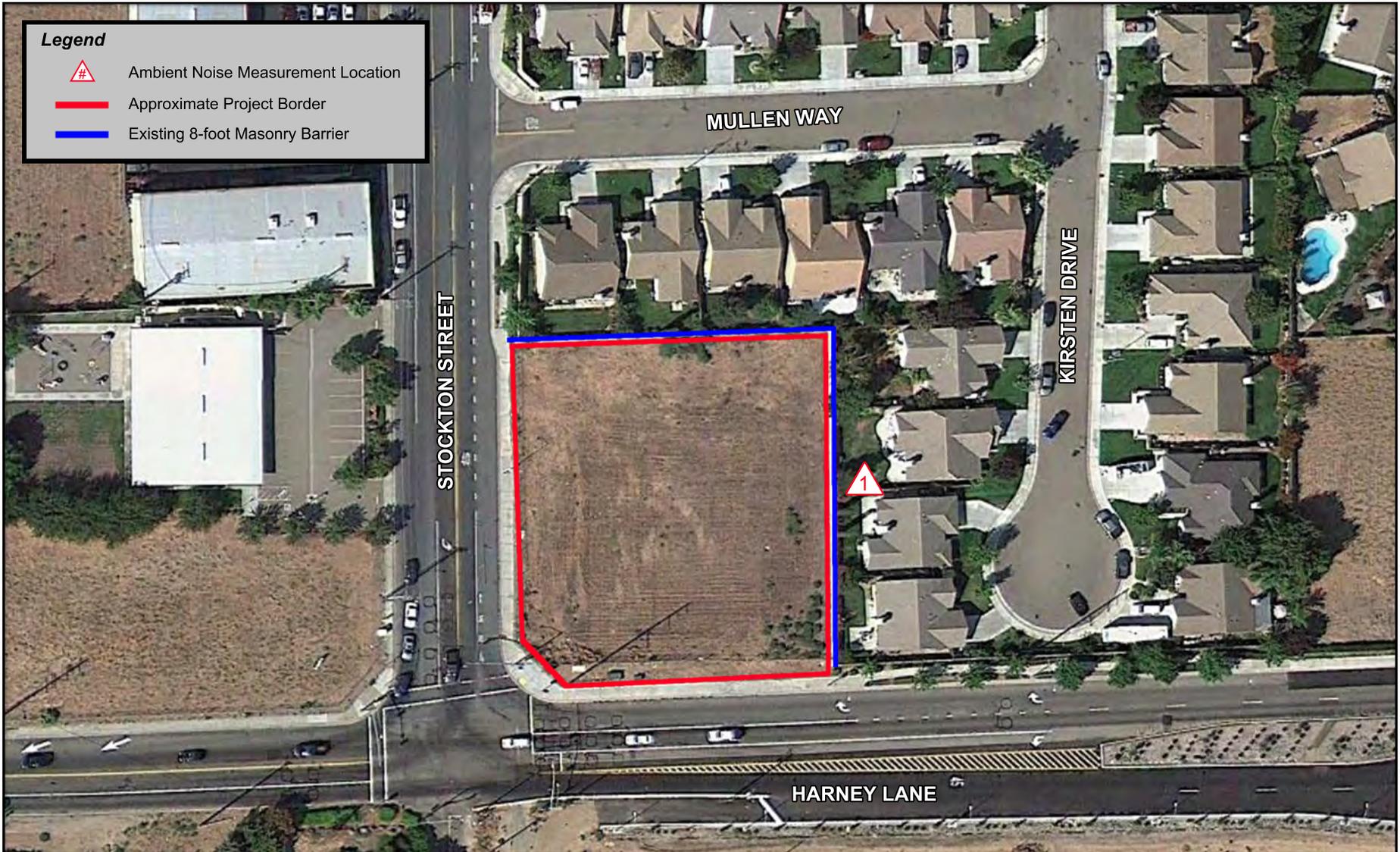
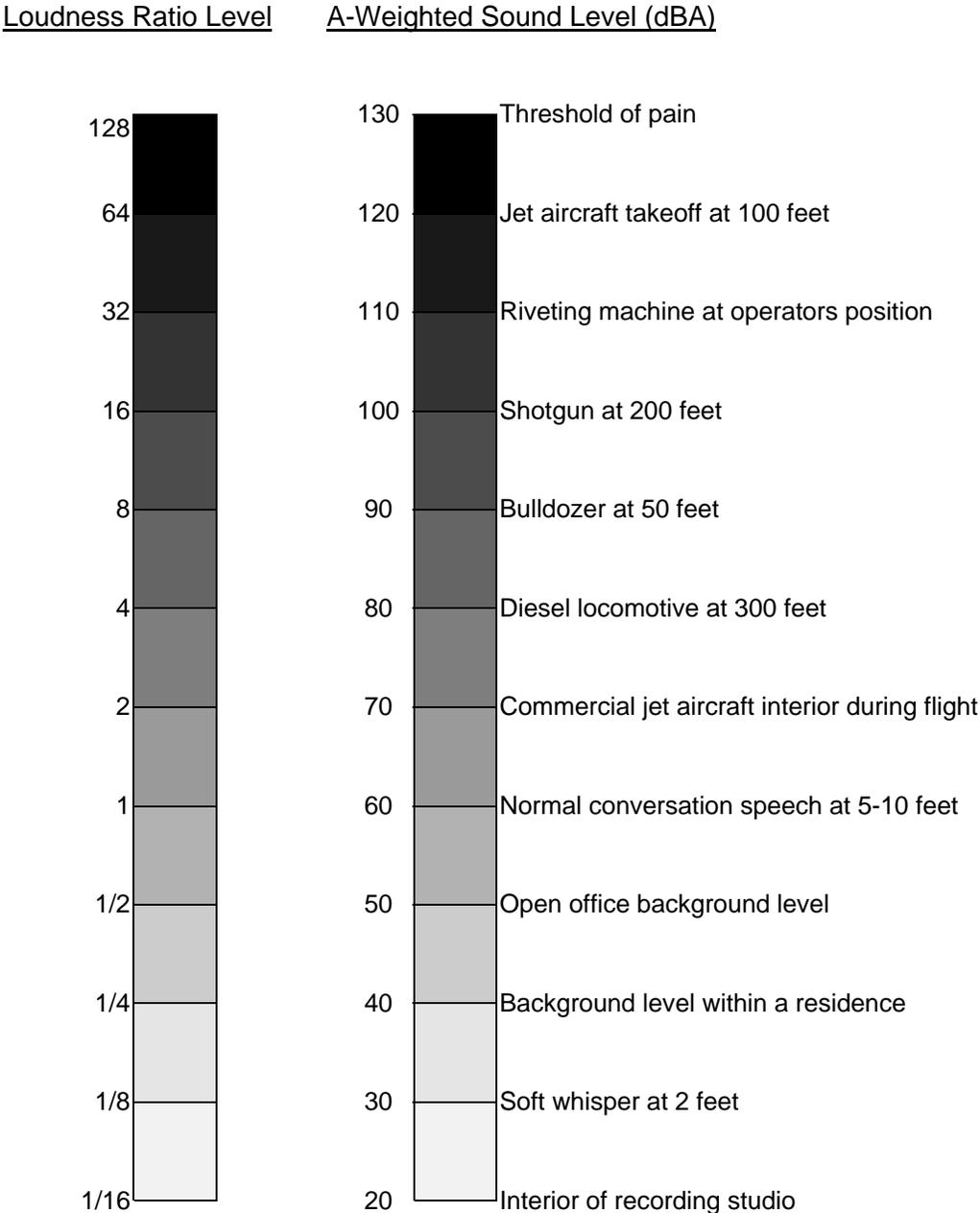


Figure 3
Typical A-Weighted Sound Levels of Common Noise Sources



Criteria for Acceptable Noise Exposure

City of Lodi General Plan Noise Element

The City of Lodi General Plan identifies noise sensitive land uses within Lodi as all single- and multi-family residential uses, schools, and long-term care medical facilities, such as hospitals and rest homes. The Noise Chapter identifies goals and policies to protect the residents of Lodi from the harmful effects of exposure to excessive noise and attempt to protect areas within the city where the present noise environment is considered acceptable. The land use compatibility guidelines contained within the General Plan identify acceptable noise levels for residential uses, provided below in Table 1. These levels are a guide to acceptable/unacceptable noise levels for the nearest residences that could be affected by the operation of the project.

Table 1
Exterior Noise Level Standards for Residential Uses
City of Lodi General Plan

Category	Noise Level
Normally Acceptable	Less than 60 dBA, L _{dn}
Conditionally Acceptable	60-70 dBA, L _{dn}
Normally Unacceptable	70-75 dBA, L _{dn}
Clearly Unacceptable	Above 75 dBA, L _{dn}

Source: City of Lodi General Plan

Existing Ambient Noise Environment

The noise environment in the vicinity of the nearest noise-sensitive receivers is defined primarily by traffic noise from the local roadways. To generally quantify background noise levels at the nearest residential locations, Bollard Acoustical Consultants, Inc. conducted a long-term (24-hour) ambient noise level measurement in the backyard of the residence located at 2553 Kirsten Court on March 9-11, 2012. The noise measurement location is depicted in Figure 1 and the measurement results are provided in Table 3. Detailed noise measurement results can be seen in Appendices B and C.

**Table 3
Summary of Ambient Noise Level Measurements
Carwash at Stockton & Harney – March 9-11, 2012**

Location	Date	Daytime				Nighttime		
		L _{dn}	L _{eq}	L ₅₀	L _{max}	L _{eq}	L ₅₀	L _{max}
Backyard of 2553 Kirsten Court	3/9/12 - Friday	59	55	49	82	52	48	78
	3/10/12 - Saturday	63	62	49	95	54	44	82
	3/11/12 - Sunday	56	52	48	78	49	40	79

Source: Bollard Acoustical Consultants, Inc.

The background noise level data provided in Table 3 indicate that noise levels measured at the nearest noise-sensitive receiver locations are generally less than the “normally acceptable” exterior noise level standards for residential uses shown in Table 2. As a result, compliance with the Table 2 noise standards will ensure that the project does not result in a significant noise level increase in the community.

Evaluation of Project-Related Noise Levels

Vacuum Noise

In order to assess the noise generation of the vacuum operations, Bollard Acoustical Consultants, Inc. utilized reference noise level data for a typical vacuum system. Specifically, reference noise level data was obtained from a vacuum unit manufactured by Vac Lover’s Inc. / Industrial Vacuum Systems. The quietest unit for which the manufacturer has quantified noise level data is the combination unit with Poly Domes and Amteck Acustek motors. The proposed vacuum system is expected to generate a noise level of approximately 68 dB at a distance of 20 feet. This analysis is based on the use of this vacuum type.

Based upon the manufacturer’s data, the proposed location of the vacuum units, the existing 8-foot property line masonry barrier (5 dB reduction) and the worst-case operating conditions (two vacuums operating continuously during daytime hours), vacuum noise levels are predicted to be approximately 50 dB L_{dn} at the nearest residential property line to the north. This noise exposure would be expected to comply with the City of Lodi’s 60 dB L_{dn} noise level criteria. As a result, no further noise mitigation measures would be warranted for this project.

Carwash Noise

Based on the experience of Bollard Acoustical Consultants, noise levels generated by carwashes are primarily due to the drying portion of the operation. As a means of determining the potential noise impacts associated with the proposed carwash, Bollard Acoustical

Consultants, Inc. utilized noise level data provided by Ryko Manufacturing Company for the Ryko Thrustpro Dryer model with the noise reduction package. The reference noise level at the entrance of the Thrustpro model is 65 dB L_{max} at 80 feet. The reference noise level at the exit of the Thrustpro model is 80 dB L_{max} at 20 feet.

Because the drying cycle represents a small portion of the overall wash, the dryers are anticipated to operate for no more than 15 minutes during any given hour. The calculated Hourly L_{eq} given 15 minute usage of the dryer cycle at the entrance would be 59 dB at a reference distance of 80 feet. The calculated Hourly L_{eq} given 15 minute usage of the dryer cycle at the exit would be 74 dB at a reference distance of 20 feet. The Table 4 data shows the predicted noise levels at the nearest residential property lines from the entrance and exit of the carwash.

**Table 4
Unmitigated Carwash Noise Levels
Carwash at Stockton & Harney – Lodi, California**

Receiver	Noise Source	Distance (feet)	Predicted Level, dB		
			L_{eq}	L_{max}	L_{dn}
Residences to the North	Dryers (Carwash Entrance)	75	60	66	53
Residences to the East	Dryers (Carwash Exit)	20	74	80	67

Notes:

- Based on manufacturers reference noise emission data for a typical carwash of this size.
- Distances are measured in feet from the property line of the nearest existing residences.
- L_{dn} calculated based upon no nighttime (10 pm to 7 am) carwash usage.
- Predicted L_{dn} assumes a 5 dB reduction in carwash noise levels due to property line 8-foot noise barrier.

The Table 4 data indicate that predicted project-related noise levels at the nearest residences to the north would be 53 dB L_{dn} and would be expected to comply with City of Lodi’s 60 dB L_{dn} noise level criteria. However, the Table 4 data indicate that the predicted project-related noise levels at the nearest residences to the east would be 67 dB L_{dn} and would be expected to exceed the City of Lodi’s 60 dB L_{dn} noise level criteria. As a result, consideration of additional noise mitigation measures would be warranted for this project.

Noise Mitigation Measures

Based on the data in Table 4, proposed carwash noise levels would exceed the City’s 60 dB L_{dn} noise level criterion at the nearest residences to the east. It is recommended, therefore, that further noise reduction be required of the carwash dryer. The dryer model mentioned previously (Ryko Thrustpro Dryer) provides optional entrance and exit doors to provide further acoustical attenuation. The reference noise level at the entrance of the Thrustpro model, with doors closed during the drying portion of the carwash, is 50 dB L_{max} at 80 feet. The reference noise level at the exit of the Thrustpro model, with doors closed during the drying portion of the carwash, is 67 dB L_{max} at 20 feet.

Because the drying cycle represents a small portion of the overall wash, the dryers are anticipated to operate for no more than 15 minutes during any given hour. The calculated Hourly L_{eq} given 15 minute usage of the dryer cycle at the entrance would be 44 dB at a reference distance of 80 feet. The calculated Hourly L_{eq} given 15 minute usage of the dryer cycle at the exit would be 61 dB at a reference distance of 20 feet. The Table 5 data shows the predicted noise levels at the nearest residential property lines from the entrance and exit of the carwash after inclusion of this mitigation measure.

Project representatives have indicated that they intend to install carwash entrance and exit doors other than the model referenced previously (Ryko Thrustpro Dryer with Noise Reduction Package). Specifically, the entrance and exit doors selected will be Full Vision Polycarbonate Garage Doors manufactured by American Garage Door Supply, Inc. Based on the manufacturer transmission loss data, the doors are expected to provide approximately 17-18 dB of noise level reduction. The entrance and exit doors referenced in Table 5 (Ryko Thrustpro Dryer with Noise Reduction Package) provide approximately 13-15 dB of noise level reduction. As a result, the carwash generated noise levels seen in Table 5 are considered to be a conservative assessment.

**Table 5
Mitigated Carwash Noise Levels
Carwash at Stockton & Harney – Lodi, California**

Receiver	Noise Source	Distance (feet)	Predicted Level, dB		
			L_{eq}	L_{max}	L_{dn}
Residences to the North	Dryers (Carwash Entrance)	75	45	51	38
Residences to the East	Dryers (Carwash Exit)	20	61	67	54

Notes:

- Based on manufacturers reference noise emission data for a typical carwash of this size.
- Distances are measured in feet from the property line of the nearest existing residences.
- L_{dn} calculated based upon no nighttime (10 pm to 7 am) carwash usage.
- Predicted L_{dn} assumes a 5 dB reduction in carwash noise levels due to property line 8-foot noise barrier.

The Table 5 data indicate that the predicted project-related noise levels of 38 and 54 dB L_{dn} would be expected to comply with City of Lodi's 60 dB L_{dn} noise level criteria. As a result, no further noise mitigation measures would be warranted for this project.

Conclusions and Recommendations

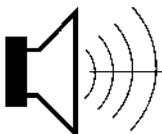
Noise levels generated by the proposed carwash are predicted to comply with the City of Lodi Noise Element standards given the following noise mitigation measures are incorporated in the project design:

1. Carwash usage should be limited to daytime hours: (7 am to 10 pm).
2. Ensure the inclusion of carwash entrance and exit doors that result in a reference noise level of 50 dB L_{\max} at 100 feet.

The above mentioned mitigation measures would result in compliance with the City of Lodi noise level criteria. These conclusions are based on the site plan shown in Figure 2 and on the manufacturers noise level data cited herein. Deviations from these plans or data could cause noise levels to differ from those predicted in this assessment.

Appendix A Acoustical Terminology

Acoustics	The science of sound.
Ambient Noise	The distinctive acoustical characteristics of a given space consisting of all noise sources audible at that location. In many cases, the term ambient is used to describe an existing or pre-project condition such as the setting in an environmental noise study.
Attenuation	The reduction of an acoustic signal.
A-Weighting	A frequency-response adjustment of a sound level meter that conditions the output signal to approximate human response.
Decibel or dB	Fundamental unit of sound, A Bell is defined as the logarithm of the ratio of the sound pressure squared over the reference pressure squared. A Decibel is one-tenth of a Bell.
CNEL	Community Noise Equivalent Level. Defined as the 24-hour average noise level with noise occurring during evening hours (7 - 10 p.m.) weighted by a factor of three and nighttime hours weighted by a factor of 10 prior to averaging.
Frequency	The measure of the rapidity of alterations of a periodic signal, expressed in cycles per second or hertz.
L_{dn}	Day/Night Average Sound Level. Similar to CNEL but with no evening weighting.
Leq	Equivalent or energy-averaged sound level.
L_{max}	The highest root-mean-square (RMS) sound level measured over a given period of time.
Loudness	A subjective term for the sensation of the magnitude of sound.
Masking	The amount (or the process) by which the threshold of audibility is for one sound is raised by the presence of another (masking) sound.
Noise	Unwanted sound.
Peak Noise	The level corresponding to the highest (not RMS) sound pressure measured over a given period of time. This term is often confused with the "Maximum" level, which is the highest RMS level.
RT₆₀	The time it takes reverberant sound to decay by 60 dB once the source has been removed.
Sabin	The unit of sound absorption. One square foot of material absorbing 100% of incident sound has an absorption of 1 sabin.
SEL	A rating, in decibels, of a discrete event, such as an aircraft flyover or train passby, that compresses the total sound energy of the event into a 1-s time period.
Threshold of Hearing	The lowest sound that can be perceived by the human auditory system, generally considered to be 0 dB for persons with perfect hearing.
Threshold of Pain	Approximately 120 dB above the threshold of hearing.



BOLLARD

Acoustical Consultants

Appendix B-1
Carwash at Stockton Street and Harney Lane
24hr Continuous Noise Monitoring at 2553 Kirsten Court (Backyard)
Friday, March 09, 2012

Hour	Leq	Lmax	L50	L90
0:00	50	64	48	44
1:00	52	71	47	43
2:00	51	71	47	44
3:00	51	72	48	44
4:00	46	61	45	42
5:00	51	61	50	46
6:00	56	71	54	52
7:00	54	66	53	50
8:00	52	68	50	46
9:00	50	63	47	43
10:00	52	69	50	44
11:00	50	66	48	43
12:00	51	65	49	45
13:00	50	65	47	44
14:00	52	70	49	45
15:00	51	71	48	45
16:00	53	73	49	46
17:00	54	77	50	46
18:00	50	65	49	47
19:00	58	82	50	47
20:00	59	80	50	47
21:00	62	81	50	47
22:00	54	76	48	46
23:00	51	78	46	43

	Statistical Summary					
	Daytime (7 a.m. - 10 p.m.)			Nighttime (10 p.m. - 7 a.m.)		
	High	Low	Average	High	Low	Average
Leq (Average)	61.8	49.6	55.0	55.6	45.9	52.1
Lmax (Maximum)	82.2	63.0	70.8	77.7	60.6	69.5
L50 (Median)	52.8	47.4	49.4	54.4	44.7	48.1
L90 (Background)	49.7	43.1	45.6	51.8	42.4	44.9

Computed Ldn, dB	59.0
% Daytime Energy	77%
% Nighttime Energy	23%

Appendix B-2
Carwash at Stockton Street and Harney Lane
24hr Continuous Noise Monitoring at 2553 Kirsten Court (Backyard)
Saturday, March 10, 2012

Hour	Leq	Lmax	L50	L90
0:00	57	79	45	41
1:00	56	82	45	41
2:00	44	59	42	39
3:00	54	81	43	40
4:00	48	72	41	37
5:00	56	79	46	41
6:00	56	79	50	45
7:00	54	76	49	45
8:00	49	64	48	44
9:00	52	77	47	43
10:00	73	95	51	46
11:00	61	85	50	46
12:00	54	76	50	46
13:00	55	76	50	47
14:00	52	72	50	46
15:00	56	78	51	47
16:00	55	76	51	48
17:00	58	82	52	48
18:00	55	77	50	46
19:00	55	80	49	45
20:00	54	80	46	43
21:00	53	73	46	42
22:00	47	71	44	41
23:00	51	76	43	37

Statistical Summary						
Daytime (7 a.m. - 10 p.m.)			Nighttime (10 p.m. - 7 a.m.)			
	High	Low	Average	High	Low	Average
Leq (Average)	73.1	49.2	62.3	57.3	43.6	53.9
Lmax (Maximum)	95.0	63.9	77.9	81.6	59.4	75.2
L50 (Median)	51.7	46.1	49.2	49.9	40.8	44.2
L90 (Background)	48.1	42.1	45.5	44.9	36.7	40.2

Computed Ldn, dB	63.0
% Daytime Energy	92%
% Nighttime Energy	8%

Appendix B-3
Carwash at Stockton Street and Harney Lane
24hr Continuous Noise Monitoring at 2553 Kirsten Court (Backyard)
Sunday, March 11, 2012

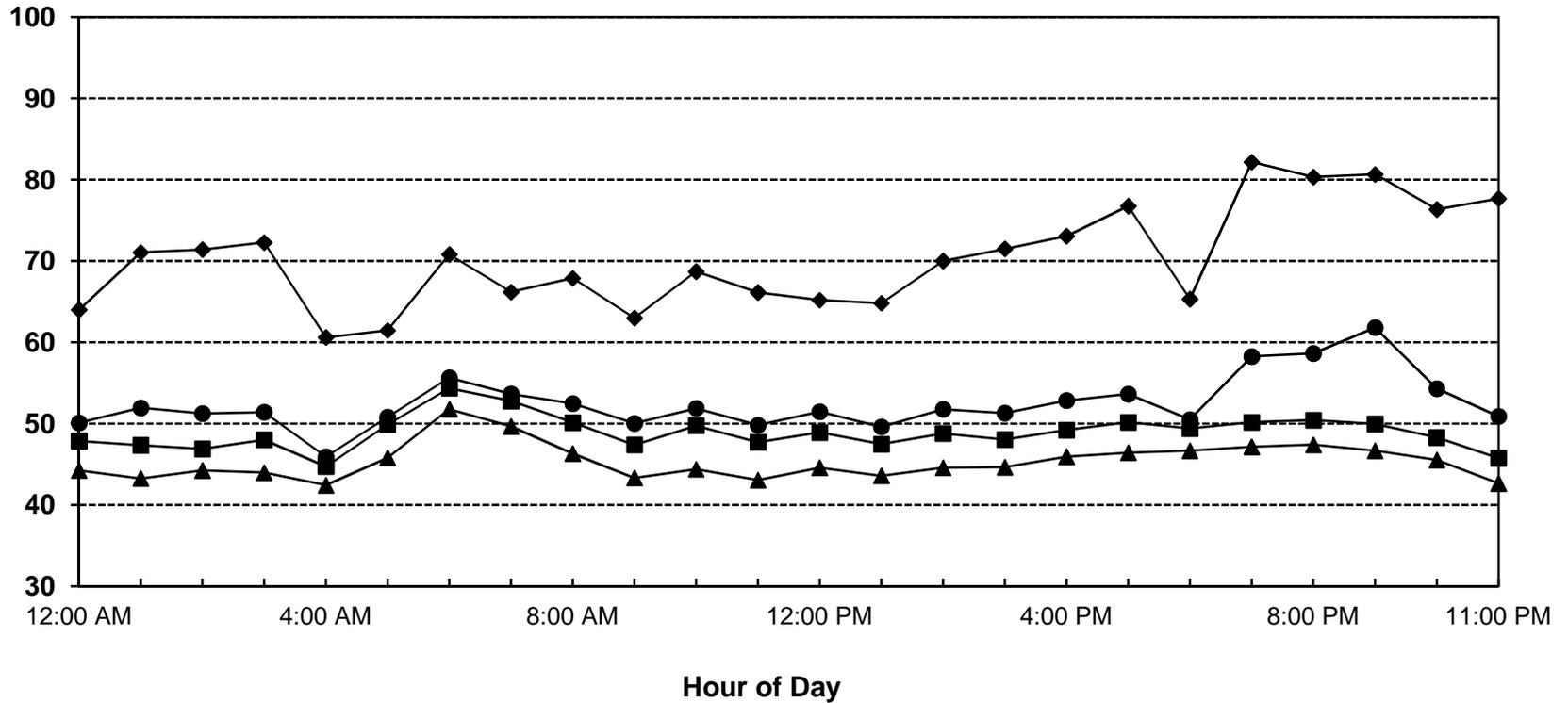
Hour	Leq	Lmax	L50	L90
0:00	54	79	40	35
1:00	44	67	38	34
2:00	42	58	41	36
3:00	44	63	37	33
4:00	39	56	36	32
5:00	50	76	41	36
6:00	50	63	47	41
7:00	51	67	49	44
8:00	49	69	46	41
9:00	52	66	50	44
10:00	52	72	49	43
11:00	50	69	47	43
12:00	50	64	48	44
13:00	53	71	49	45
14:00	53	73	49	45
15:00	49	62	48	44
16:00	53	72	49	44
17:00	54	76	50	45
18:00	53	76	47	43
19:00	50	71	45	42
20:00	52	78	44	40
21:00	49	74	42	38
22:00	52	75	40	36
23:00	51	72	39	34

Statistical Summary						
Daytime (7 a.m. - 10 p.m.)				Nighttime (10 p.m. - 7 a.m.)		
	High	Low	Average	High	Low	Average
Leq (Average)	54.1	49.3	51.8	53.6	38.8	49.3
Lmax (Maximum)	77.8	62.1	70.8	78.9	55.8	67.6
L50 (Median)	49.6	42.4	47.5	47.4	36.3	39.9
L90 (Background)	45.4	37.6	43.0	41.2	32.3	35.3

Computed Ldn, dB	56.1
% Daytime Energy	75%
% Nighttime Energy	25%

Appendix C-1
Carwash at Stockton Street and Harney Lane
24hr Continuous Noise Monitoring at 2553 Kirsten Court (Backyard)
Friday, March 09, 2012

Sound Level, dBA

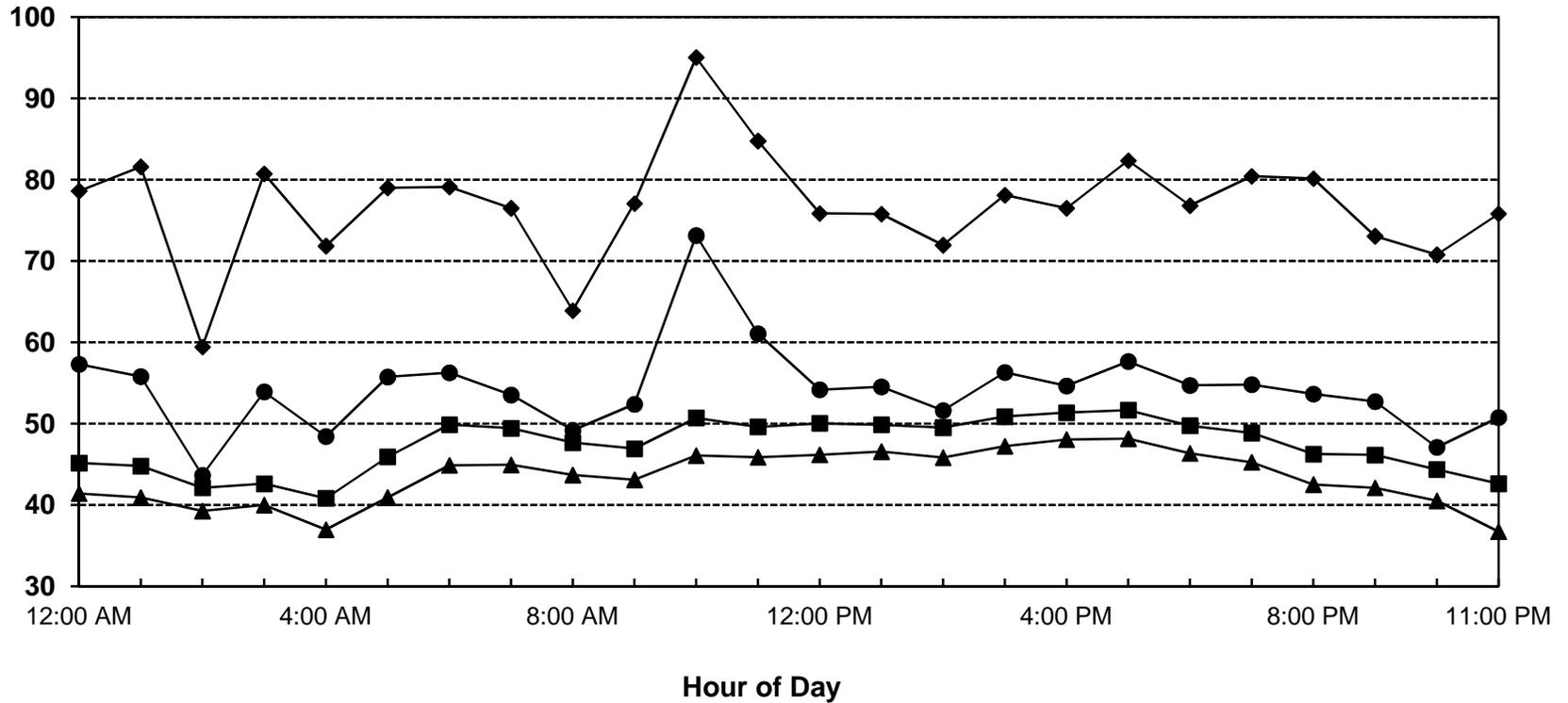


● Average (Leq)
 ◆ Maximum (Lmax)
 ■ L50
 ▲ L90

Ldn: 59 dB

Appendix C-2
Carwash at Stockton Street and Harney Lane
24hr Continuous Noise Monitoring at 2553 Kirsten Court (Backyard)
Saturday, March 10, 2012

Sound Level, dBA

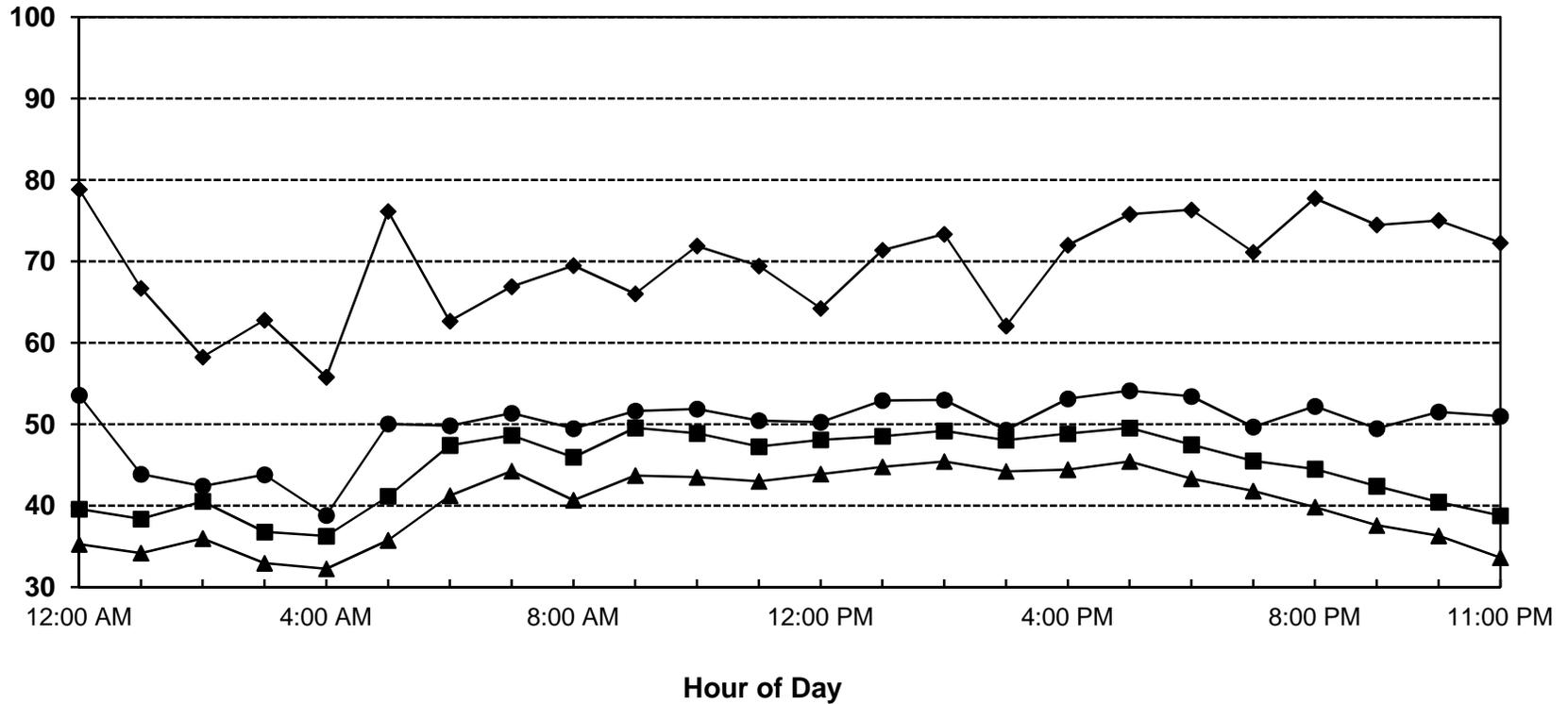


● Average (Leq)
 ◆ Maximum (Lmax)
 ■ L50
 ▲ L90

Ldn: 63 dB

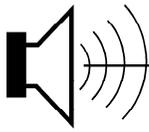
Appendix C-3
Carwash at Stockton Street and Harney Lane
24hr Continuous Noise Monitoring at 2553 Kirsten Court (Backyard)
Sunday, March 11, 2012

Sound Level, dBA



● Average (Leq)
 ◆ Maximum (Lmax)
 ■ L50
 ▲ L90

Ldn: 56 dB



May 23, 2012

Mr. Ken Dharni
Mack Road AM/PM
6698 Mack Road
Sacramento, CA 95821

Subject: Noise Study Report Addendum for the Carwash at Stockton Street and Harney Lane in Lodi, California

Dear Mr. Dharni,

Pursuant to the City of Lodi's request, Bollard Acoustical Consultants, Inc. (BAC) has completed an addendum to the original noise study report (April 25, 2012). Due to the proximity of the proposed carwash to the adjacent residential uses, the noise study report recommended the inclusion of entry and exit doors to reduce carwash noise levels sufficiently enough to satisfy the City of Lodi's noise level criteria. Since the report relied on manufacturer's reference noise level data to arrive at its conclusion, the City of Lodi requested that short-term noise level measurements be taken at a facility similar to the proposed project site to verify the reports' predicted carwash noise levels. Specifically, the selected existing carwash location was required to be located adjacent to residences (separated by masonry barrier) and have entry and exit doors installed and operating.

While multiple carwash sites were considered, the Chevron Gas Station Carwash located at 21501 Foothill Boulevard in Hayward, California, was ultimately determined to most closely match the environment of the proposed project in Lodi. As depicted in Attachment A, the selected carwash was located adjacent to residences and separated by a masonry barrier (6-feet high). Attachment B provides photographs of the entry and exit of the carwash. The carwash model at the selected representative site was the Ryko Softglass Maxx with Entry and Exit Automatic Polycarbonate Doors.

Larson Davis Laboratories (LDL) Model 820 precision integrating sound level meters were used to complete the noise level measurement survey. The meters were calibrated before use with an LDL Model CAL200 acoustical calibrator to ensure the accuracy of the measurements. The equipment used meets all pertinent specifications of the American National Standards Institute for Type 1 sound level meters (ANSI S1.4).

The short-term noise level measurements were conducted on the morning of May 18, 2012 and were taken at the three locations identified in Attachment A. Site 1 was located at the entrance of the carwash, Site 2 was located at the exit of the carwash, and Site 3 was located at the exit of the carwash on the opposite side of the masonry barrier within the residences' side yard. Measurements were unable to be conducted at the carwash entrance, on the opposite side of the barrier, due to the resident not permitting access. BAC staff observed 6 carwash cycles during the noise level measurement period. The summary of the noise level measurements can be seen in Table 1.

Table 1
Carwash Short-term Noise Measurement Results
Carwash at Stockton Street and Harney Lane – Lodi, California

Location ¹	Description	Hayward Site ²	Lodi Report ³	Difference (dB)
		Measured Noise Level (dB) ⁴	Referenced Noise Level (dB) ⁴	
1	Entrance	66	64	+2
2	Exit	67	67	0
3	Behind Barrier – Exit	59	n/a	n/a

Notes:

- ¹ See Attachment A for noise level measurement locations.
² Noise level measurements taken at the carwash located at 21501 Foothill Boulevard in Hayward, California.
³ Reference noise levels provided in original noise study report (April 25, 2012).
⁴ Noise levels normalized to a reference distance of 20 feet in order to compare measured versus referenced values.

Table 1 data indicate that the existing 6-foot high masonry barrier is providing a noise level reduction of approximately 8 dB. However, it is important to note that the carwash pad elevation was approximately 2-3 feet higher than the adjacent residence pad elevation. Therefore, the barriers' effective height at the test location was 8-9 feet high, similar to the existing barrier at the proposed project site in Lodi.

As seen in Table 1, the measured noise levels at the selected representative carwash in Hayward are within 0-2 dB agreement with the referenced noise levels contained within the original report. As a result, the predicted carwash noise levels at the proposed carwash site are expected to satisfy the City of Lodi's noise level criteria. No additional noise mitigation measures would be required for this project.

Please contact me at (916) 663-0500 or jonl@bacnoise.com if you have any comments or questions regarding this addendum.

Sincerely,

Bollard Acoustical Consultants, Inc.



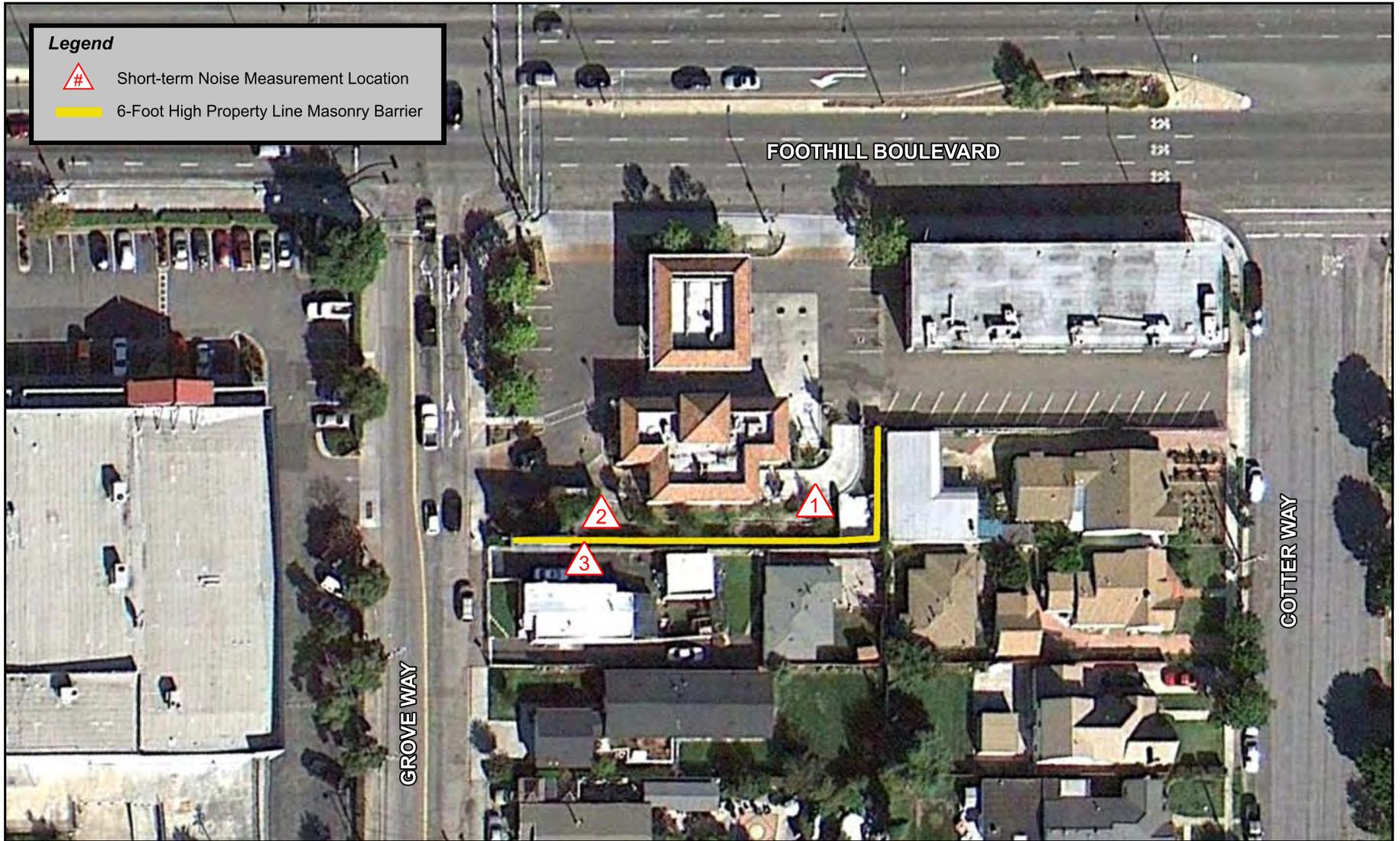
Jonathan Lopez
Consultant

Attachments

Attachment A

Lodi Carwash Noise Study Report Addendum

Representative Carwash Site Aerial & Noise Measurement Locations



- Notes:
1. Carwash seen above located at 21501 Foothill Boulevard, Hayward, California.
 2. Carwash Model: Ryko Softglass Maxx with Entry and Exit Automatic Polycarbonate Doors.



Attachment B

Lodi Carwash Noise Study Report Addendum Photos of Representative Carwash Entry/Exit Doors



Carwash Entrance



Carwash Exit

PLANNING COMMISSION RESOLUTION NO. 12-

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LODI APPROVING A USE PERMIT AND A SPARC REVIEW TO ALLOW DEVELOPMENT OF A GAS STATION WITH 8-DISPENSER CANOPY, 3,078 SQUARE FOOT CONVENIENCE STORE WITH SALE OF BEER AND WINE (TYPE-20), AND A DRIVE THROUGH CARWASH FACILITY ON A .94-ACRE SITE LOCATED AT 255 EAST HARNEY LANE

- WHEREAS**, the Planning Commission of the City of Lodi has heretofore held a duly noticed public hearing, as required by law, on the requested Use Permit, in accordance with the Lodi Municipal Code, Section 17.72.070; and
- WHEREAS**, the project site is located at 255 East Harney Lane, Lodi, CA 95240 (APN: 062-580-40); and
- WHEREAS**, the Planning Commission the City of Lodi has considered Use Permit 12-U-06 to establish a drive-through carwash facility and to allow sale of beer and wine for off site consumption license at 255 East Harney Lane; and
- WHEREAS**, the requested use permit to allow the selling of malt and wine based alcoholic beverages for off-site consumption is an enforcement action in accordance with the City of Lodi Zoning Ordinance; and
- WHEREAS**, the census tract in which the market is located does not have an undue concentration of licenses allowing off premise consumption of alcoholic beverages; and
- WHEREAS**, the Planning Commission the City of Lodi has considered SPARC application 12-SP-02 concerning development plans to construct a gas station, convenience store, and drive through carwash with associated outdoor vacuum area; and
- WHEREAS**, the project proponent is Mr. Pete Tobin, of Barghausen Consulting Engineers, Inc., on behalf of Hardev Singh Gill, 1255 Treat Blvd Suite 329, Walnut Creek, CA 94597; and
- WHEREAS**, the project property owner is Nanak State, Inc., 4304 Payne Road, Pleasanton, CA 94588; and
- WHEREAS**, the property has a General Plan designation of Commercial and is zoned C-1, Neighborhood Commercial; and
- WHEREAS**, all legal prerequisites to the adoption of this Resolution have occurred; and

Based upon the evidence within the staff report and project file the Planning Commission finds:

1. The project was found to be Categorical Exempt according to the California Environmental Quality Act, Article 19 §15321, Class 21 (a) (2). The project is classified as an "Enforcement action by regulatory agencies" because it is the "adoption of an administrative decision or order enforcing or revoking the lease, permit, license, certificate, or entitlement for use or enforcing the general rule, standard, or objective." No significant environmental impacts are anticipated and no mitigation measures have been required.
2. The location and design of the proposed development is consistent with the goals and policies of the Lodi General Plan relating to Commercial land use designations and C-1 Zoning District, which states appropriate buffers and setbacks shall be provided between commercial and residential uses. Automated carwash facilities typically have been incorporated into gasoline stations/convenience stores, provided that appropriate measures are designed into the project to ensure compatibility with adjacent uses, and to mitigate potential noise, lighting, visual and traffic impacts. The proposed carwash facility would not create special problems in the area because the building has been designed and located to reduce noise levels to the point where city standards are met. The Public Works Department indicated that traffic is not anticipated to significantly

increase, and all vehicular traffic generated by the project would be accommodated safely and without causing undue congestion upon the adjoining streets and intersections.

3. The proposed development will be well integrated with its surrounding since it is designed to complement the built-in environment. Appropriate setbacks and landscape buffers are provided from adjacent uses, especially residential uses to the north and east. The approximately .94-acre site is suitable for the proposed use since the site is relatively flat and would not require any extensive grading or result in any adverse impacts to surrounding uses or environmental resources. Appropriate on-site circulation would be provided, especially around the pump islands underground fuel tank access points. The project will not be visually obstructive or disharmonious with surrounding areas, or harm major views from adjacent properties, since the facility is relatively small in area and single-story in height.
4. The subject site will have adequate pedestrian and vehicular circulation and parking available.
5. The location and the design of the building and improvements proposed to accommodate the project are appropriate because the project, as conditioned, will be functional in providing adequate access and creating an attractive environment through approved building designs, landscaping plans and the installation of related improvements.
6. The harmony, bulk and scale of the proposed project is consistent with and compatible to the proposed land uses around the subject site.
7. The proposed location and design allows the project site to be adequately serviced by existing public facilities and utilities since sewer and water service currently is provided to the site and will not be affected by this project. An onsite water clarifier/recycling system would be incorporated into the carwash.

NOW, THEREFORE, BE IT DETERMINED AND RESOLVED by the Planning Commission of the City of Lodi that Use Permit Application No. 12-U-10 is hereby approved, subject to the following conditions:

1. The project proponent and/or the property owner and/or successors in interest and management shall, at their sole expense, defend, indemnify and hold harmless the City of Lodi, its agents, officers, directors and employees, from and against all claims, actions, damages, losses, or expenses of every type and description, including but not limited to payment of attorneys' fees and costs, by reason of, or arising out of, this Use Permit approval. The obligation to defend, indemnify and hold harmless shall include, but is not limited to, any action to arbitrate, attack, review, set aside, void or annul this Use Permit approval on any grounds whatsoever. The City of Lodi shall promptly notify the developer of any such claim, action, or proceeding and shall cooperate fully in the defense.
2. The applicant shall install metal landscape trellis on the western and southern elevations of the proposed convenience store building and on the western elevation of the carwash facility.
3. The project proponent and/or the property owner and/or successors in interest and management shall comply with all federal, State, and local laws. Material violations of any of those laws in connection with the use may be a cause for revocation of the permits granted herein.
4. The applicant shall operate and abide under all conditions of the State of California Department of Alcoholic Beverage Control license Type 20.
5. Prior to the issuance of a Type-20 license, the project proponent and/or the property owner and/or successors in interest and management shall of the convenience store shall complete Licensee Education on Alcohol and Drugs as provided by the State Department of Alcoholic Beverage Control.
6. The project proponent and/or the property owner and/or successors in interest and management shall insure that the serving of alcohol beverages does not cause any condition that will cause or

result in repeated activities that are harmful to the health, peace or safety of persons residing or working in the surrounding area. This includes, but is not limited to: disturbances of the peace, illegal drug activity, public drunkenness, drinking in public, harassment of passerby, assaults, batteries, acts of vandalism, loitering, excessive littering, illegal parking, excessive loud noises (especially in the late night or early morning hours), traffic violations or traffic safety based upon last drink statistics, curfew violations, lewd conduct, or police detention and arrests.

7. The project proponent and/or the property owner and/or successors in interest and management shall not allow the display or advertising of malt based beverages or wine to be visible from the public right-of-way.
8. This Use Permit shall be subject to periodic review for compliance with these conditions by the Planning Commission.
9. The automated carwash facility shall not operate between the hours of 7:00 p.m. and 7:00 a.m. daily. These hours may be further limited (as may be determined by the Planning Commission) based on any valid complaints due to early morning or early evening noise/nuisance issues.
10. The Ryko Thrustpro Dryer equipment with Silencer shall be incorporated into the carwash design. The dryer/blowers also shall be placed a minimum of three feet inside the building from the carwash exit. Should the Proto-Vest system not be available at the time of installation, a similar type system may be used, provided that it provides the same or better noise attenuation. This shall be documented by a revised noise analysis.
11. All construction and grading shall comply with all applicable requirements of the Lodi Municipal Code and requirements of the Planning Department, Engineering Department, Building Division, and Fire Department. Fire sprinklers may be required, as determined by the Fire Department.
12. All plan building permit submittals shall be based on the City of Lodi Building Regulations and currently adopted 2010 California Building code. Please review our policy handouts for specific submittal procedures.
13. The applicant shall obtain Operational Permits from the Lodi Fire Department, Fire Prevention Bureau. The Fire Department may be contacted at 25 East Pine Street, Lodi, CA 95240-2127. Phone Number (209) 333-6739.
14. Colors, materials and design of the project shall conform to the exhibits and references in the staff report and presented to the Planning Commission, to the satisfaction of the Community Development Director.
15. In the event that the proposed gas station use ceases operation for more than 6 months, the underground fuel tanks, fuel pumps and fuel pump canopy shall be removed from the project site to assist with marketing the site for alternate use(s).
16. A separate sign permit and building permit shall be required prior to the installation of any signs. All proposed signage shall be consistent with City of Lodi Sign Ordinance, exhibits in the staff report, and recommendations of the Planning Commission.
17. All new utilities shall be underground.
18. Any rooftop equipment must be fully screened from all public view utilizing materials and colors which match the building. This shall be clearly indicated on the building plans.

19. All project generated noise shall comply with the City's Noise Ordinance to the satisfaction of The Community Development Director. Any noise complaints regarding the operation of the facility shall be promptly addressed by the applicant/operator.
20. The applicant shall be responsible for all dust control during any construction and shall follow the construction methods established by the San Joaquin Valley Air Pollution Control District.
21. Prior to the issuance of building permits, the developer shall submit to the Community Development Department for review and approval a revised landscape plans showing the inclusion of evergreen trees, shrubs and/or vines near the carwash and trash enclosure. The said landscape plan shall conform to Water Efficient Landscape Ordinance and must include Statement of Compliance and water usage calculations.
22. All trees planted along the northern and eastern boundary lines, adjacent to residential properties, shall be exclusively of 24" box and shall be evergreen type of trees in order to provide adequate screening. Any gaps in the northern and eastern boundaries that allow views into the carwash or the convenience store area from the residences to the north and east shall be landscaped with specimen-sized trees (min. 24" box) and appropriate fast growing/tall shrubs. This shall be noted on the final landscape plans.
23. All vegetation shall be maintained in a flourishing manner, and kept free of all foreign matter, weeds and plant materials not approved as part of the landscape plan. All irrigation shall be maintained in fully operational condition.
24. Trash enclosures shall be designed to accommodate separate facilities for trash and recyclable materials. Trash enclosures having connections to the wastewater system shall install a sand/grease trap conforming to Standard Plan 205. Trash enclosure shall be covered or have covers on the separate facilities.
25. Any discharge into the City wastewater system from the carwash or canopy must be treated by a sand/oil separator or another approved clarifying device.
26. Existing drive ways will need to be upgraded to meet ADA standards and Special Commercial Driveway Standard Plan 111.
27. Provide proper signage specifying right turn only at the Harney Lane entrance/exit. One way only signage will be required in the Harney Lane median.
28. The monument sign located on the corner of Stockton Street and Harney Lane is shown within a 10-foot public utility easement (PUE). The sign must be either relocated outside of the PUE or a structural encroachment permit must be obtained prior to the issuance of the building permit.
29. The site does not specify the location of the bioswale. The bioswale cannot be located with in any public utility easements.
30. The site must conform to the Stormwater Development Standards Plan (DSP) requirements. The DSP Worksheet must be provided before the issuance of the Building Permit.
31. All project design and construction shall be in compliance with the Americans with Disabilities Act (ADA). Project compliance with ADA standards is the developer's responsibility.
32. Payment of the following prior to building permit issuance unless noted otherwise:
 - a. Filing and processing fees and charges for services performed by City forces per the Public Works Fee and Service Charge Schedule
 - b. Habitat Conservation Fee

- c. Stormwater Compliance Inspection Fee prior to building permit issuance or commencement of construction operations, whichever occurs first.
33. Payment of the following prior to temporary occupancy or occupancy of the building unless noted otherwise:
- a. Development Impact Mitigation Fees
 - b. Wastewater Capacity Impact Mitigation Fee
 - c. County Facilities Fees
 - d. Regional Transportation Impact Fee (RTIF)
 - e. Water Capacity Impact Mitigation Fee.
34. Dedication of public utility easements as required by the various utility companies and the City of Lodi.
35. Provision of all necessary Public Utility Easements, payment of Electric Utility Department charges, and installation of necessary equipment/infrastructure to provide electrical service to the properties in accordance with the Electric Department's rules and regulations.
36. Any fees due the City of Lodi for processing this Use Permit shall be paid to the City within thirty (30) calendar days of final action by the approval authority. Failure to pay such outstanding fees within the time specified shall invalidate any approval or conditional approval granted. No permits, site work, or other actions authorized by this action shall be processed by the City, nor permitted, authorized or commenced until all outstanding fees are paid to the City.
37. No variance from any City of Lodi adopted code, policy or specification is granted or implied by the approval of this resolution.

Dated: August 8, 2012

I certify that Resolution No. 12- was passed and adopted by the Planning Commission of the City of Lodi at a regular meeting held on August 8, 2012 by the following vote:

- AYES:** Commissioners:
- NOES:** Commissioners:
- ABSENT:** Commissioners:

ATTEST _____
Secretary, Planning Commission

Item 3c.

**CITY OF LODI
PLANNING COMMISSION
Staff Report**

MEETING DATE: August 8, 2012

APPLICATION NO: Use Permit: 12-U-11

REQUEST: Request for Planning Commission approval of a Use Permit to allow a Type 2 (Winery) Alcoholic Beverage Control license at 20 West Elm Street. (Applicant: Erin Taylor, on behalf of Riaza Wines, LLC; File Number: 12-U-11).

LOCATION: 20 West Elm Street
APN: 043-035-31
Lodi, CA 95240

APPLICANT: Erin Taylor, on behalf of Riaza Wines, LLC
230 Dunsmuir Drive
Lodi, CA 95240

PROPERTY OWNER: Kelly Eigenberger Etal
12 Evergreen Drive
Lodi, CA 95242

RECOMMENDATION

Staff recommends the Planning Commission approve Ms. Erin Taylor's request, on behalf of Riaza Wines, LLC., for a Use Permit to allow a Type 2 (Winery) Alcoholic Beverage Control license at 20 West Elm Street, subject to the conditions outlined in the attached resolution.

PROJECT/AREA DESCRIPTION

General Plan Designation: DMU, Downtown Mixed Use
Zoning Designation: C-2, General Commercial (Downtown Business District)
Property Size: Tenant space is approximately 1,893 sq. ft.

	ADJACENT ZONING DESIGNATIONS AND LAND USES		
	GENERAL PLAN	ZONING CLASSIFICATION	EXISTING LAND USE
North	Downtown Mixed use	C-2, General Commercial	Mixture of retail/commercial
South	Downtown Mixed use	C-2, General Commercial	Mixture of retail/commercial
East	Downtown Mixed use	C-2, General Commercial	Mixture of retail/commercial
West	Downtown Mixed use	C-2, General Commercial	Mixture of retail/commercial

SUMMARY

The applicant, Erin Taylor, on behalf of Riaza Wines, LLC, seeks a Use Permit approval to operate a wine retail and wine tasting room at 20 West Elm Street. The facility will be located within an existing commercial building in the Downtown District. No exterior changes to the building are proposed. However, the applicant proposes an outdoor seating area for wine tasting purposes, similar to the other wineries in the Downtown District, which features mixed use commercial and retail businesses. The proposed use is consistent with the City's vision of making Lodi a wine tasting tourist destination. The applicant is not requesting signage at this time. Any future signage will be reviewed at staff level to ensure conformance with the approved sign program for the Downtown Business District. No wine production is proposed.

BACKGROUND

Available City records indicate that the building has been used for various retail type uses since its construction. The subject tenant space measures approximately 1,100 sq. ft. in area. The most recent use was a second hand store. Currently, there are no outstanding City or Building Code violations. The project site is bordered by a variety of businesses on all sides. The project site is within the Downtown Parking District and, therefore, no onsite parking is required.

ANALYSIS

The applicant, Ms. Erin Taylor, on behalf of Riaza Wines, LLC, requests approval of a Use Permit to open a wine tasting room and retail sales in an existing building located at 20 West Elm Street. In addition, the applicant intends to use this location to sell wine related gift items such as decanters, stemware, cork screws and the like. Wearable logo items will also be made available for purchase. A Type 2 license is required by the State Department of Alcohol Beverage Control (ABC) for the operation of a wine retail and wine tasting. No food would be prepared at the project site. The proposed hours of operations are Monday-Thursday 11:00 a.m. – 9:00 p.m., Friday-Saturday 11:00 a.m.-10:00 p.m., and Sunday 11:00 a.m.-5:00 p.m. The proposed hours of operation are similar to other wine tasting rooms in Lodi.

The project is located in a C-2 (General Commercial) zoning district and the building offers space for the type of business the applicants propose. The proposed business will be located within the City's downtown and will support and compliment other downtown businesses. The C-2 zoning district is designated for a variety of general commercial uses, including wineries. The proposed winery is consistent with the types of uses one would expect to find in a Downtown Business District. In the future, the applicant would like to serve wine on the sidewalk, similar to the Cellar Door and Benson Ferry Wine Tasting rooms. No crushing or making activities will occur at this location.

Section 17.72.040 of the Lodi Municipal Code requires a Use Permit for new Off-Sale and On-Sale alcohol licenses as well as changes in license type. The City established the Use Permit requirement to gain local control over whether or not a license is appropriate for a particular location. The State Department of Alcoholic Beverage Control primarily controls issuance based on concentration of licenses within a particular Census Tract. Census Tract 42.04 covers the area south of Holly Drive, west of Sacramento Street, north of Lodi Avenue, and east of Ham Lane. Because this area is within the Downtown Business District, there is an existing over concentration of ABC licenses (29 on-sale and 4 off-sale) within this Census Tract. However, wineries, wine production facilities and associated wine tasting rooms and retail sales of wine are exempt from ABC regulations related to over-concentration and the need to obtain public necessity and convenience from the Planning Commission. The Commission should review the application to ensure that sale of alcohol will not adversely affect surrounding residents, businesses, and institutions and to ensure that any such use operates in a manner compatible with existing and future adjacent uses. In the past, the Planning Commission and the Planning staff have generally supported wineries and wine manufacturing operations that wish to acquire an ABC on-sale license because, typically, they do not create alcohol related problems.

The purpose of a Use Permit procedure is to adequately condition a project in a way to minimize potential negative impacts to the health, safety and general welfare of the community, residents, and surrounding properties. As part of the recommendation of approval, staff, working with the Police Department, has recommended operational conditions based upon the individual business plan to ensure the establishment will be compatible with the surroundings. The Police Department has reviewed and recommended operational conditions, which are incorporated into the draft resolution. The Public Works Department has reviewed the application and advised that the proposed outdoor area will require an encroachment permit. Their requirement has been incorporated into the attached draft resolution.

The Community Development Department is of the opinion that the proposed project is consistent with the surrounding land uses and zoning; and with the City's policy to encourage the wine

production and wine tasting industry. The intended nature of this operation does not promote excessive onsite consumption of alcohol and, therefore, staff does not expect any problems with the proposed establishment. Since no crushing is being proposed as part of this Use Permit, there will be no wine waste to be concerned about. The proposed use is consistent with the City's vision of making Lodi a wine tasting tourist destination. The wine tasting rooms are a large part of this vision for they not only compliment other downtown businesses such as restaurants and boutiques, but add to the character of the City by promoting local wineries and viticulture. Staff has contacted various City departments for review and approval. Their requirements have been incorporated into the attached draft resolution. We, therefore, are recommending that the Use Permit be approved, subject to the attached resolution.

ENVIRONMENTAL ASSESSMENTS

The project was found to be categorically exempt according to the California Environmental Quality Act, Article 19 15321 Class 21 (a) (2). The project is classified as an "Enforcement Action by Regulatory Agencies" because it is the "adoption of an administrative decision or order enforcing...the lease, permit, license, certificate, or entitlement for use or enforcing the general rule, standard, or objective." The project was also found to be categorically exempt according to the California Environmental Quality Act, Article 19 15332 Class 32 (a) (b) (c) (d) and (e). The project is classified as in-fill development meeting the conditions described therein. No significant impacts are anticipated and no mitigation measures have been required. No significant impacts are anticipated and no mitigation measures have been required.

PUBLIC HEARING NOTICE:

Legal Notice for the Use Permit was published on Saturday, July 28, 2012. Forty-six (46) public hearing notices were sent to all property owners of record within a 300-foot radius of the project site as required by California State Law §65091 (a) 3. Public notice also was mailed to interested parties who had expressed their interest of the project.

ALTERNATIVE PLANNING COMMISSION ACTIONS:

- Approve with additional/different conditions
- Deny the Use Permit and SPARC request
- Continue the request

Respectfully Submitted,

Concur,

Immanuel Bereket
Associate Planner

Konradt Bartlam
Community Development Director

ATTACHMENTS

- A. Vicinity Map
- B. Aerial Map
- B. Existing and Proposed Floor Plan
- C. Draft Resolution

**20 West Elm Street
Lodi, CA 95240
APN: 04303531**

**Property Owner:
Greg Soligan**

**Property Tenant:
Riaza Wines**

**Proposed Use:
Wine Tasting Room**

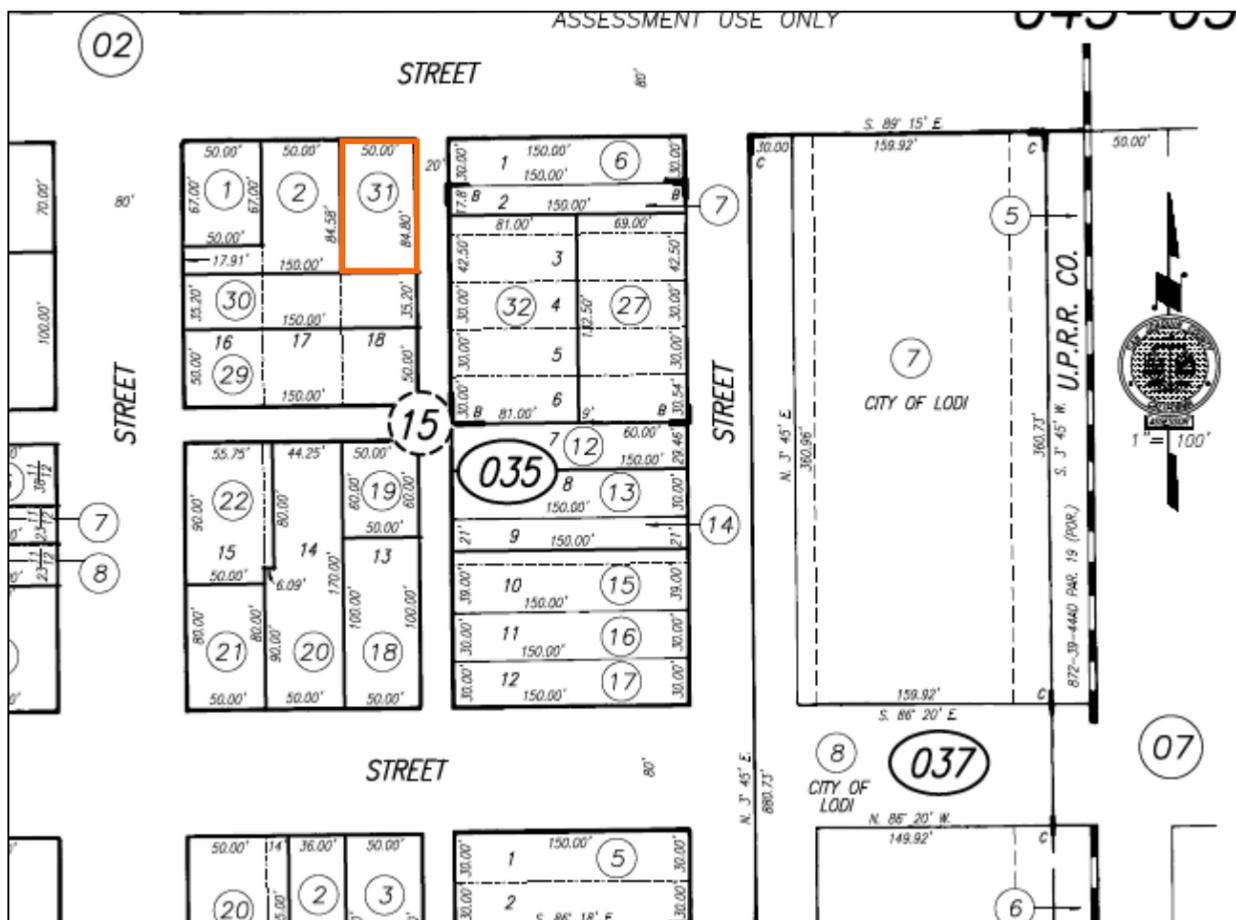
**Date:
July 16, 2012**

Site Plan

The property located at 20 West Elm Street, Lodi, CA 95240 is part of parcel number 04303531. The parcel is 50.00 feet in width by 84.80 feet in depth and is located adjacent to the alley that runs between Sacramento and School Streets. There is a two-story structure (approximately 25 feet in height) on the parcel, the first floor of which is commercial/retail. The proposed wine tasting room will occupy 20 West Elm (the space immediately to the west of an existing retail establishment located at 18 West Elm) - a space that is approximately 16.50 feet in width by 64.00 feet in depth or roughly 1,100 square feet in size. There is a property located immediately to the west of the proposed wine tasting room (24 West Elm) that is currently vacant. Since no construction is being proposed, neither the existing retail establishment at 18 West Elm or the vacant property at 24 West Elm will be impacted.



From San Joaquin County Community Development GIS

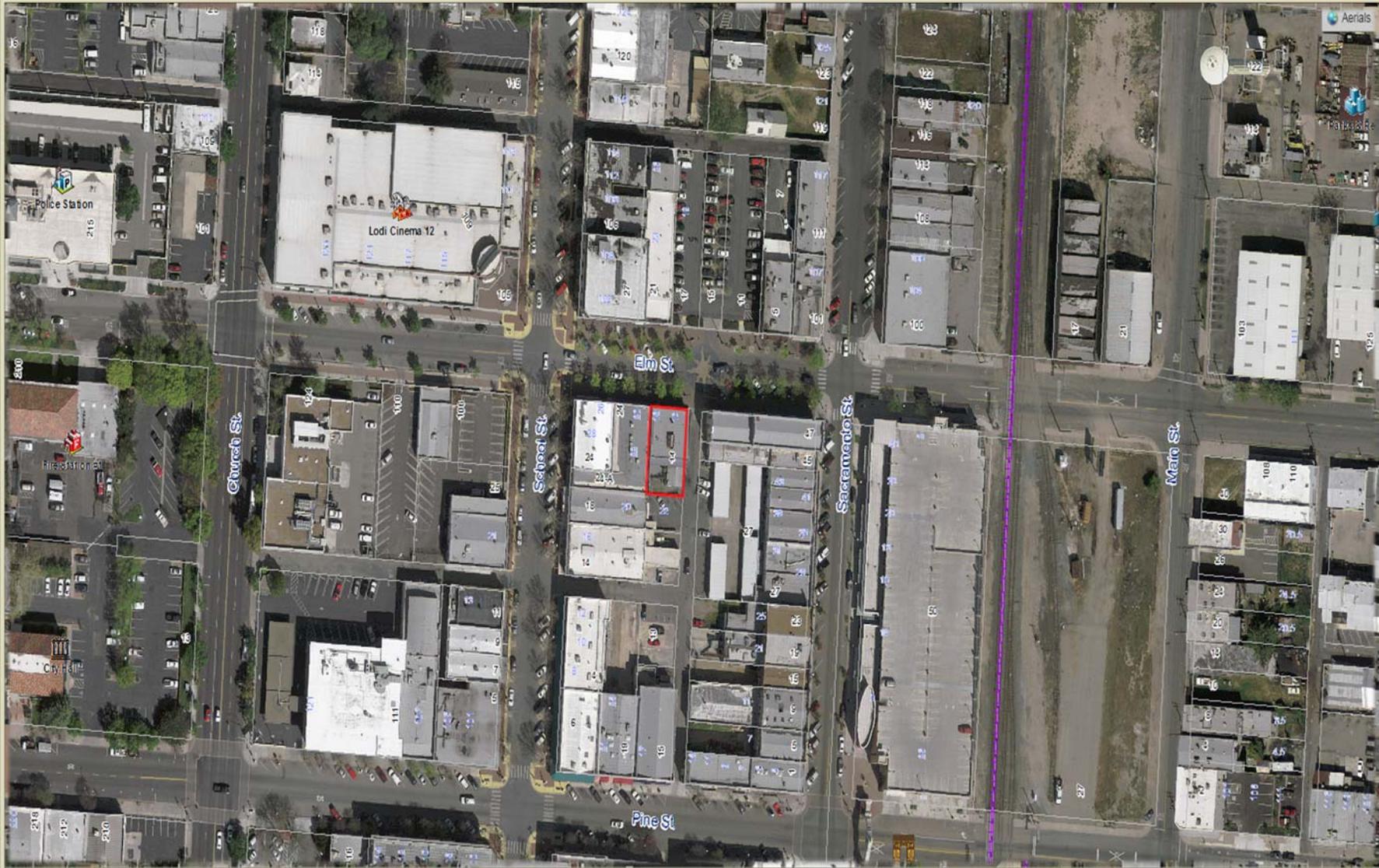


From San Joaquin County Assessor's Map Book 043 Page 03



Lodi

Aerial Map



Legend

Landmarks

- ARCH
- CITYBUILDINGS
- CITYHALL
- COURT
- DOGPAK
- FIRESTATIONS
- HIGH SCHOOLS
- HOSPITALS
- LIBRARY
- LND-MRKS
- PARKS
- POLICE
- POST OFFICE
- SCHOOLS
- SKATEPARK
- SOFTBALL
- STADIUM
- THEATRE
- TRAIN
- Project Site
- City Limit

Map Scale
1:1,656

276 0 138 276Feet

NAD_1983_StatePlane_California_III_FIPS_0403_Feet
© City of Lodi Geographic Information Systems

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.
THIS MAP IS NOT TO BE USED FOR NAVIGATION

Notes
20 West Elm Street
APN: 045-035-02
Lodi, CA 95240

**20 West Elm Street
Lodi, CA 95240
APN: 04303531**

Property Owner:
Greg Soligan

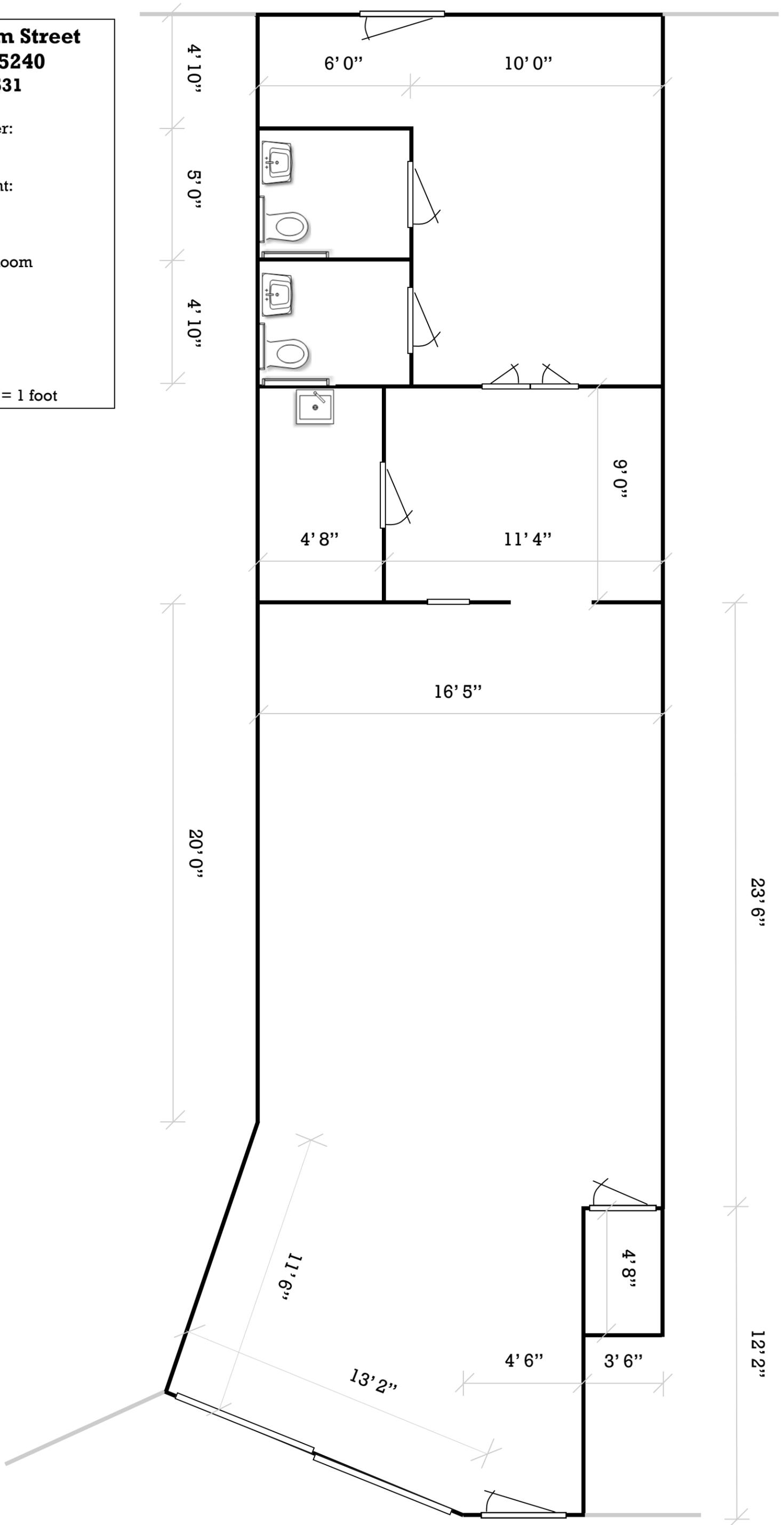
Property Tenant:
Riaza Wines

Proposed Use:
Wine Tasting Room

Date:
July 16, 2012

Existing Space

Scale: 1/4 inch = 1 foot



**20 West Elm Street
Lodi, CA 95240
APN: 04303531**

Property Owner:
Greg Soligan

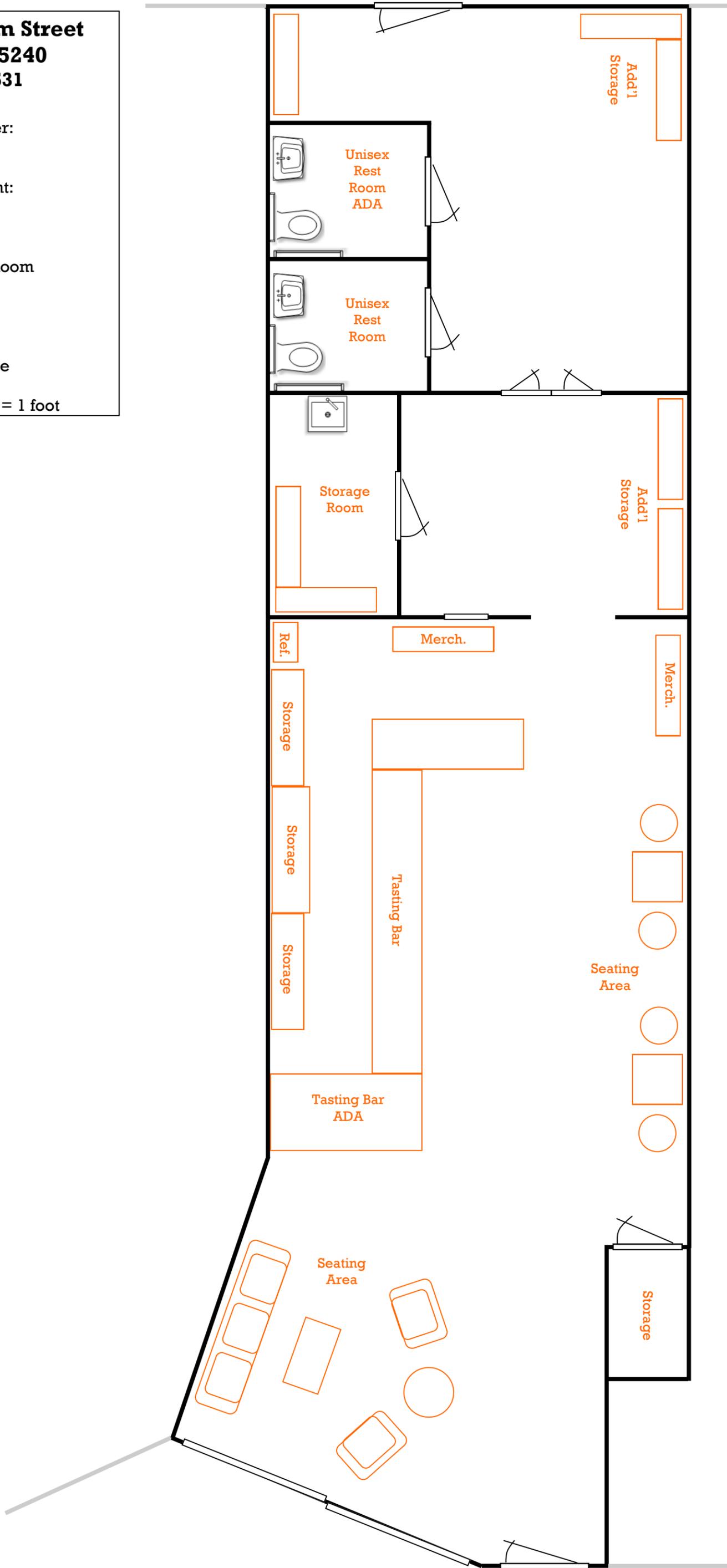
Property Tenant:
Riaza Wines

Proposed Use:
Wine Tasting Room

Date:
July 16, 2012

Proposed Space

Scale: 1/4 inch = 1 foot



RESOLUTION NO. P.C. 12-

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LODI APPROVING THE REQUEST OF ERIN TAYLOR, ON BEHALF OF RIAZA WINES, LLC FOR A USE PERMIT TO ALLOW A TYPE-2 WINEGROWER ALCOHOLIC BEVERAGE CONTROL LICENSE AT 20 WEST ELM STREET

WHEREAS, the Planning Commission of the City of Lodi has heretofore held a duly noticed public hearing, as required by law, on the requested Use Permit, in accordance with the Lodi Municipal Code, Section 17.72.070; and

WHEREAS, the project proponent is Erin Taylor, on behalf of Riaza Wines, LLC, 230 Dunsmuir Drive, Lodi, CA 95240; and

WHEREAS, the project is located at 20 West Elm Street, Lodi, CA 95240 (APN: 043-035-31); and

WHEREAS, the property has a General Plan designation of Downtown Mixed Use and is zoned C-2, General Commercial and is within the Downtown Business District as well as the Downtown Parking District; and

WHEREAS, the project parcel is owned by Kelly Eigenberger Et al, 12 Evergreen Drive, Lodi, CA 95242; and

WHEREAS, the requested Use Permit to allow on-site wine tasting and retail sales thereof is an enforcement action in accordance with the City of Lodi Zoning Ordinance; and

WHEREAS, the State Department of Alcoholic Beverage Control has training available that clearly communicates State law concerning the sale of alcoholic beverages; and

WHEREAS, pursuant to City of Lodi Zoning Ordinance § 17.72.110, this resolution becomes effective ten (10) business days from its adoption in the absence of the filing of an appeal; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred; and

Based upon the evidence within the staff report and project file the Planning Commission finds:

1. The project was found to be categorically exempt according to the California Environmental Quality Act, Article 19 15321 Class 21 (a) (2). The project is classified as an “Enforcement Action by Regulatory Agencies” because it is the “adoption of an administrative decision or order enforcing...the lease, permit, license, certificate, or entitlement for use or enforcing the general rule, standard, or objective.” The project was also found to be categorically exempt according to the California Environmental Quality Act, Article 19 15332 Class 32 (a) (b) (c) (d) and (e). The project is classified as in-fill development meeting the conditions described therein. No significant impacts are anticipated and no mitigation measures have been required. No significant impacts are anticipated and no mitigation measures have been required.
2. The sale of alcoholic beverages for on-premise consumption as part of a wine tasting room and wine production operations is a permitted use in the C-2 zoning District. The site is suitable and adequate for the proposed use because establishment of a winery on this site would not create negative impacts on businesses, residents and instructional uses in the vicinity, and the applicant proposes to perform a tenant improvement in order to meet building code requirements.
3. The on-site consumption of wine in conjunction with a wine tasting room operation, in accordance with a Type 2 Alcoholic Beverage Control License and with the conditions attached herein, would be consistent and in harmony with the Downtown Mixed Use General Plan Land Use Designation and C-2 zoning District.
4. The proposed use is consistent with the General Plan because commercial uses such as the one proposed are permitted in accordance with Land Use Policy subject to a discretionary review.
5. The proposed use would not have an adverse economic effect on nearby uses because operation of a wine production operation in accordance with applicable laws and under the conditions of this Use Permit is anticipated to be an economic benefit to the community.
6. The sale and consumption of alcohol can sometimes result in customer behavior problems that can require police intervention.

7. Steps can be taken by the Applicant/Operator to reduce the number of incidents resulting from the over-consumption of alcohol including the proper training and monitoring of employees serving alcohol; the careful screening of IDs of customers to avoid sales to under-aged individuals; limiting the number of drinks sold to individual customers to avoid over-consumption; providing properly trained staff to monitor customer behavior both in and outside of the establishment; and working with the Lodi Police Department to resolve any problems that may arise.
8. The proposed use can be compatible with the surrounding use and neighborhood if the business is conducted properly and if the Applicant/Operator works with neighboring businesses and residents to resolve any problems that may occur.
9. The proposed use would not be detrimental to the general welfare of persons residing and working in the immediate vicinity, the neighborhood or the community at large because the sale of alcohol with a wine tasting operation is not typically associated with detrimental impacts to the community.
10. The proposed use is consistent with the City's vision of making Lodi a wine tasting tourist destination point.
11. The wine tasting room will add to the character of the City by promoting local wineries and viticulture.

NOW, THEREFORE, BE IT DETERMINED AND RESOLVED by the Planning Commission of the City of Lodi that Use Permit Application No. 12-U-11 is hereby approved, subject to the following conditions:

1. The property owner and/or developer and/or successors in interest and management shall, at their sole expense, defend, indemnify and hold harmless the City of Lodi, its agents, officers, directors and employees, from and against all claims, actions, damages, losses, or expenses of every type and description, including but not limited to payment of attorneys' fees and costs, by reason of, or arising out of, this development approval. The obligation to defend, indemnify and hold harmless shall include, but is not limited to, any action to arbitrate, attack, review, set aside, void or annul this development approval on any grounds whatsoever. The City of Lodi shall promptly notify the developer of any such claim, action, or proceeding and shall cooperate fully in the defense.
2. The property owner and/or developer and/or successors in interest and management shall insure that the serving of alcohol does not cause any condition that will cause or result in repeated activities that are harmful to the health, peace or safety of persons residing or working in the surrounding area. This includes, but is not limited to: disturbances of the peace, illegal drug activity, public intoxication, drinking in public, harassment of people passing by, assaults, batteries, acts of vandalism, loitering, excessive littering, illegal parking, excessive loud noises, traffic violations or traffic safety based upon last drink statistics, curfew violations, lewd conduct, or police detention and arrests.
3. The property owner and/or developer and/or successors in interest and management shall operate the project in strict compliance with the approvals granted herein, City standards, laws, and ordinances, and in compliance with all State and Federal laws, regulations, and standards. In the event of a conflict between City laws and standards and a State or Federal law, regulation, or standard, the stricter or higher standard shall control.
4. The project proponent/applicant/operator and/or developer and/or successors in interest and management shall operate and abide by the requirements and conditions of the State of California Department of Alcoholic Beverage Control License Type 2. The Type 2 License shall be limited to on-site sale and wine tasting during the hours that the business is open for operation, Monday – Thursday from 11:00 am to 9 pm, Friday - Saturday 11:00 am to 10:00 pm and Sunday from 11:00 am to 5:00 pm or as otherwise modified by the Community Development Director. The on-site sale and consumption of wine shall occur only during the said hours.
5. The City reserves the right to periodically review the area for potential problems. If problems (on-site or within the immediate area) including, but not limited to, public drunkenness, the illegal sale or use of narcotics, drugs or alcohol, disturbing the peace and disorderly conduct result from the proposed land use, the Use Permit may be subject to review and revocation by the City of Lodi after a public hearing and following the procedures outlined in the City of Lodi Municipal Code. Additional reviews may be prescribed by the Community Development Director, the Police Department and/or Planning Commission as needed during and after the first two years of probationary period. Further, starting from the effective date the business commences the sale of wine, this Use Permit shall be subject to a one year, and two year review by the Community Development Director. If the Director determines it

necessary, the Director shall forward the review to the Planning Commission to review the business's operation for compliance with the conditions of the Use Permit, and in response to any complaints thereafter.

6. Prior to the issuance of a Type-2 license, the project proponent/applicant/operator and/or developer and/or successors in interest and management shall complete Licensee Education on Alcohol and Drugs as provided by the State Department of Alcoholic Beverage Control.
7. The Lodi Police Department may, at any time, request that the Planning Commission conduct a hearing on the Use Permit for the purpose of amending or adding new conditions to the Use Permit or to consider revocation of said Use Permit if it becomes a serious policing problem.
8. That applicants shall be allowed to use the wine tasting space for various events such as wine dinners catered by outside vendors, private mixers, wine tasting and parties for organizations and businesses who want to rent space, and to host feature wine tasting events that may include educational components.
9. All temporary and permanent signage proposed in connection with the wine making and wine tasting operation shall be reviewed and approved by the Community Development Department.
10. The wine tasting operation shall be conducted in a manner that will not adversely impact neighboring properties or businesses.
11. No wine crushing and/or making operations shall occur at this site.
12. The subject site shall be maintained in a sanitary, litter-free, graffiti-free, and respectable appearance. Any damage or vandalism sustained to the property shall be repaired within a maximum of fourteen (14) days.
13. The project proponent/applicant/operator and/or developer and/or successors in interest and management shall obtain a Downtown Encroachment Permit from the Public Works Department for the proposed outdoor seating.
14. Due to change of use and/or occupancy of the building, the applicant shall submit complete and adequate Tenant Improvement plans to the Building Division for review and approval. The said plans shall be based on the City of Lodi Building Regulations and currently adopted 2010 California Building Code.
15. Any fees due the City of Lodi for processing this Project shall be paid to the City within thirty (30) calendar days of final action by the approval authority. Failure to pay such outstanding fees within the time specified shall invalidate any approval or conditional approval granted. No permits, site work, or other actions authorized by this action shall be processed by the City, nor permitted, authorized or commenced until all outstanding fees are paid to the City.
16. All project design and construction shall be in full compliance with the Americans with Disabilities Act (ADA). Project compliance with ADA standards is the developer's responsibility.
17. No variance from any City of Lodi adopted code, policy or specification is granted or implied by the approval of this resolution.

Dated: August 8, 2012

I certify that Resolution No. 12- was passed and adopted by the Planning Commission of the City of Lodi at a regular meeting held on August 8, 2012 by the following vote:

AYES: Commissioners:
NOES: Commissioners:
Abstain: Commissioners:
ABSENT: Commissioners:

ATTEST _____
Secretary, Planning Commission

Item 6a.



MEMORANDUM, City of Lodi, Community Development Department

To: City of Lodi Planning Commissioners
From: Rad Bartlam, Community Development Director
Date: Planning Commission Meeting of 08/08/2012
Subject: Past meetings of the City Council and other meetings pertinent to the Planning Commission

In an effort to inform the Planning Commissioners of past meetings of the Council and other pertinent items staff has prepared the following list of titles.

If you have any questions, please feel free to contact the Planning Department or visit the City of Lodi website at: <http://www.lodi.gov/city-council/AgendaPage.html> to view Staff Reports and Minutes from the corresponding meeting date.

Date	Meeting	Title
July 17, 2012	Shirtsleeve	Receive Presentation on Climate Action Plan (CM)
July 24, 2012	Shirtsleeve	Receive Update Regarding Harney Lane Grade Separation (PW)

Item 7a.

**CITY OF LODI
PLANNING COMMISSION
Staff Report**

MEETING DATE: August 8, 2012

APPLICATION NO: Not Applicable

SUBJECT: Development Code Update

BACKGROUND:

The City is in the process of updating zoning regulations to conform to the Land Use Element of the 2010 General Plan. The 2010 General Plan specifically recommended that the Development Code be updated in order to implement the land use vision contained in the General Plan through specific development standards, such as allowed uses, maximum heights, and minimum setback. This report provides an introduction to the zoning proposals for new zoning for the City's commercial and industrial districts. The current zoning regulations were written in the 1950s and although the Code has been amended since then, the City has not taken a comprehensive look at zoning rules in almost 60 years.

ANALYSIS:

Commercial Districts:

The zoning ordinance in effect contains several commercial districts with indiscernible differences. These commercial districts are C-1 (Neighborhood Commercial), C-2 (General Commercial), R-C-P, (Residential-Commercial-Professional), C-S (Commercial Shopping) and C-M (Commercial Light Industrial). The C-1 zoning district permits residential, retail businesses, trade, commercial enterprise or professional and business office use, undertaken for the purpose of rendering neighborhood service. The C-2 zoning district allows all uses permitted in the C-1 zoning district and other non-industrial commercial or business uses. The R-C-P zoning district allows business and professional offices such as accountant, architect, attorney, contractor, doctor, dentist, engineer, insurance agent, real estate agency, finance company, bank, chiropractor, governmental agency and drive-in offices. This district is found along South Fairmont Avenue, and areas around Ham Lane, Pine Street, Vine Street, and Kettleman Lane. C-S zoning district is effectively used for community/regional shopping centers. Finally, the C-M district is a transitional district from the commercial districts to industrial districts. This C-M district is found along Sacramento Street in the Downtown area.

The proposed Development Code consolidates the commercial districts into three districts illustrated on table below.

EXISTING COMMERCIAL ZONES	PROPOSED COMMERCIAL ZONES	RELATIONSHIP TO THE GENERAL PLAN
C-1 (Neighborhood Commercial)	GC (General Commercial) District.	Neighborhood/Community Commercial
C-2 (General Commercial)		
C-M (Commercial Light Industrial)		
R-C-P, (Residential-Commercial-Professional)	O (Office)	Office
C-S (Commercial Shopping)	CC (Community Commercial) District	General Commercial

The Development Code proposes to merge the C-1, C-2 and C-M zoning districts into a single zoning designation to create GC (General Commercial) District. The C-M zoning district is the City's only transitional district from commercial to industrial. C-M zoning district applies areas abutting Sacramento Street in the Downtown area. Because the C-1 and C-2 zoning districts abut residential properties, and the C-M district is similar to the C-2 district, the uses permitted over the years on these districts are similar in nature. It no longer makes planning sense to maintain separate zoning districts with near identical requirements and zoning regulations.

The R-C-P zoning district is found along Kettleman Lane, Fairmont Avenue and Orange Avenue. This is the area where medical, dental, and other health-care oriented services are located. The Development

Code proposes to re-designate the area as an Office use, which would permit medical and general offices. Finally, The CC district applies to the local and regional shopping centers. The Development Code proposes to re-designate the area with the same requirements in effect. The final product is a more user friendly document.

Industrial Districts:

Industrial uses vary from commercial uses in that industrial uses typically have increased noise, odor, dust, smoke, truck traffic, and other items that may be objectionable to adjacent uses. Additionally these uses tend to require less parking and have different hours of operation than commercial uses as they are focused on manufacturing products rather than selling to customers. There are two main purposes of the Industrial Zone: to provide an area of town where industrial uses can be clustered and to buffer these uses from residential and commercial uses so there are no negative affects from the industrial operations.

The Industrial Zones in the City can be found east of State Highway 99 and along the UPPR line (Main Street). Lodi Municipal Code in effect features two classifications: M-1(Light Industrial) and M-2 (Heavy Industrial). M-1 zoning district permits light industrial/manufacturing uses such as food processing, packaging and storage; bottling plants; manufacturing and assembling of jewelry, watches, clocks, precision instruments, appliances; and other similar manufacturing uses. The M-2 zoning district permits all uses permitted in the commercial and M-1 zoning districts. Because of that fact the Code in effect allows uses permitted in the Light Industrial districts in the M-2 zoning district, all types of uses can be found across both zoning districts, including more commercial type uses in this Industrial Zone. For this reason, the 2010 General Plan and the Development Code merged the two industrial zoning districts into a single zoning district as illustrated below.

EXISTING COMMERCIAL ZONES	PROPOSED COMMERCIAL ZONES	RELATIONSHIP TO THE GENERAL PLAN
M-1 (Light Industrial)	M (Industrial) District.	Industrial (.6 FAR)
M-2 (Heavy Industrial)		
BP (Business Park)	BP (Business Park) District.	Business Park (1.0FAR)

The proposed Development Code creates BP (Business Park) Zoning District. This is consistent with the 2010 General Plan and applies to new growth areas of the General Plan. The General Plan identifies the new growth areas appropriate for planned, visually attractive centers for business that do not generate nuisances (noise, clutter, noxious emissions, etc.). This zone accommodates campus-like environments for corporate headquarters, research and development facilities, offices, light manufacturing and assembly, industrial processing, general service, incubator-research facilities and other similar uses that generate high employment possibilities.

PLANNING COMMISSION ACTIONS:

This is an information item and an opportunity to discuss proposed changes to the zoning ordinance and map, and receive input from the Planning Commission and the public. Given that this is a discussion session, the Planning Commission has no cause for action. The City will conduct noticed public workshops and hearing in the future.

Respectfully Submitted,

Concur,

Immanuel Bereket
Associate Planner

Konradt Bartlam
Community Development Director

- Attachment:**
1. Industrial Zoning Districts
 2. Commercial Zoning Districts

Commercial Zoning Districts

CHAPTER 17.20 - COMMERCIAL ZONING DISTRICTS

Sections:

- 17.20.010 - Purpose of Chapter
- 17.20.020 - Purposes of Commercial Zoning Districts
- 17.20.030 - Commercial Zoning District Land Uses and Permit Requirements
- 17.20.040 - Commercial Zoning District General Development Standards
- 17.20.050 - Commercial Design Guidelines

17.20.010 - Purpose of Chapter

This Chapter lists the uses of land that may be allowed within the commercial zoning districts established by **Section 17.10.020 (Zoning Districts Established)**, determines the type of land use permit/approval required for each use, and provides basic standards for site development.

17.20.020 - Purposes of Commercial Zoning Districts

The commercial zoning districts are intended to provide for a range of local-serving and regional-serving commercial uses, emphasizing high quality development, and to encourage revitalization of existing commercial land uses. The purpose of the individual commercial zoning districts and the manner in which they are applied are as follows.

- A. CC (Community Commercial) District.** The CC zoning district is applied to areas suitable for a full range of retail, wholesale, and service uses including retail stores, apparel stores, specialty shops, shopping centers and home furnishings. The maximum floor area ratio (FAR) is 0.4. The CC zoning district is consistent with the Neighborhood/Community Commercial land use designation of the General Plan.
- B. GC (General Commercial) District.** The GC zoning district is applied to areas appropriate for a range of community serving commercial, regional retail, and service land uses. The FAR is 0.6. The GC zoning district is consistent with the General Commercial land use designation of the General Plan.
- C. O (Office).** The O zoning district is applied to areas appropriate for administrative, financial, business, professional, and medical offices. Support commercial uses are also permitted, subject to limitations. The maximum FAR is 0.6. The O zoning district is consistent with the Office land use designation of the General Plan.

17.20.030 - Commercial Zoning District Land Uses and Permit Requirements

Table 2-6 identifies the uses of land allowed by this Development Code in the commercial zoning districts, and the land use permit required to establish each use, in compliance with **Section 17.12.030 (Allowable Land Uses and Permit Requirements)**.

Note: where the last column in the tables ("Specific Use Regulations") includes a section number, the regulations in the referenced section apply to the use; however, provisions in other sections of this Development Code may also apply.

Commercial Zoning Districts

TABLE 2-6 Commercial Zones - Allowed Land Uses and Permit Requirements		A	Allowed Use			
		UP	Use Permit required			
Land Use		MUP	Minor Use Permit required			
		—	Use not allowed			
		PERMIT REQUIRED BY DISTRICT				Specific Use Regulations
		CC	GC	O		
AGRICULTURE AND OPEN SPACE						
Production of crops		A	A	A		
RECREATION, EDUCATION, & PUBLIC ASSEMBLY USES						
Clubs, lodges, & membership halls		—	A	UP		
Community centers		—	A	UP		
Health/fitness facilities		UP	UP	UP		
Indoor amusement/entertainment facilities		UP	UP	—		
Indoor sports facilities		UP	UP	UP		
Libraries, museums, galleries		A	A	A		
Outdoor recreation facilities		UP	UP	—		
Religious facilities		—	UP	—		
Schools - Private		—	UP	UP		
Schools - Specialized education and training		UP	A	A		
Studios - Art, dance, martial arts, music, etc.		UP	A	UP		
RESIDENTIAL USES						
Caretaker quarters		—	—	UP		
Residential shelters/transitional housing		—	UP	—		

Commercial Zoning Districts

TABLE 2-6 Commercial Zones - Allowed Land Uses and Permit Requirements	A	Allowed Use				
	UP	Use Permit required				
	MUP	Minor Use Permit required				
	—	Use not allowed				
Land Use	PERMIT REQUIRED BY DISTRICT					Specific Use Regulations
		CC	GC	O		

RETAIL TRADE

Accessory retail uses		A	A	A		
Alcoholic beverage sales, off-site		UP	UP	—		
Alcoholic beverage sales, on-site		UP	UP	—		
Animal sales and grooming		A	A	—		
Art, antique, collectible, and gift stores		A	A	—		
Auto parts sales		A	A	—		
Auto sales and rental		—	A	—		
Building material stores		A	A	—		
Construction/heavy equipment sales and rental		—	A	—		
Convenience stores		A	A	A		
Drive-in and drive-through sales and services		UP	UP	UP		
Furniture, furnishings & appliance stores		A	A	—		
Gas stations		UP	UP	—		
General retail sales		A	A	—		
Grocery stores		A	A	—		
Mobile home and RV sales		—	A	—		
Night clubs, bars, and cardrooms		—	UP	—		
Plant nurseries and garden supply stores		A	A	—		
Restaurants		A	A	A		
Warehouse retail		A	A	—		

Commercial Zoning Districts

TABLE 2-6 Commercial Zones - Allowed Land Uses and Permit Requirements	A	Allowed Use				
	UP		Use Permit required			
	MUP	Minor Use Permit required				
	—	Use not allowed				
Land Use	PERMIT REQUIRED BY DISTRICT					Specific Use Regulations
		CC	GC	O		

SERVICES – BUSINESS, FINANCIAL, PROFESSIONAL

Automated teller machines (ATMs)		A	A	A		
Banks and financial services		A	A	A		
Business support services		A	A	A		
Medical - Clinics, offices, and laboratories		A	A	A		
Medical - Extended care		—	A	A		
Medical - Hospitals		—	—	A		
Offices		A	A	A		
Professional Services		A	A	A		

SERVICES

Audio & video rental		A	A	—		
Auto repair and maintenance		—	UP	—		
Car wash		—	UP	—		
Community care facilities						
Residential care facility (6 or fewer)		—	—	—		
Residential care facility (6 or more)		—	—	—		
Family day care home (14 or fewer)		—	—	—		
Day care center		UP	UP	UP		
Hotels and motels		UP	UP	UP		
Mortuaries & funeral homes		—	A	UP		
Personal services		A	A	A		
Pharmacies		A	A	—		
Research and development		—	—	A		
Storage - indoor		—	UP	—		
Upholstering shops		—	UP	—		
Veterinary clinics, outpatient treatment only		A	A	A		
Veterinary clinics, animal hospitals, kennels		—	UP	—		

TABLE 2-6 Commercial Zones - Allowed Land Uses and Permit Requirements	A	Allowed Use				
	UP	Use Permit required				
	MUP	Minor Use Permit required				
	—	Use not allowed				
Land Use	PERMIT REQUIRED BY DISTRICT					Specific Use Regulations
	CC	GC	O			

INDUSTRIAL, MANUFACTURING & PROCESSING, WHOLESALING

Recycling facilities						
Small collection facility		MUP	MUP	—		
Large collection facility		—	—	—		

TRANSPORTATION, COMMUNICATIONS & INFRASTRUCTURE USES

Broadcast studios		UP	UP	UP		
Parking facilities/vehicle storage		—	UP	—		
Telecommunications facilities		UP	UP	—		
Utility Facility		UP	UP	UP		

Commercial Zoning Districts

17.20.040 - Commercial District General Development Standards~~Error! Bookmark not defined.~~

Subdivisions, new land uses and structures, and alterations to existing land uses and structures, shall be designed, constructed, and/or established in compliance with the requirements in Table 2-5, in addition to the applicable development standards (e.g., landscaping, parking and loading, etc.) in Article 3, (Site Planning and General Development Standards).

TABLE 2-7 - COMMERCIAL DISTRICT GENERAL DEVELOPMENT STANDARDS

Development Feature	Requirement by Zoning District		
	CC	GC	O
Minimum lot size	<i>Minimum area, width and depth required for new parcels.</i>		
Area	2.0 acres	8,000 sq. ft.	8,000 sq. ft.
Width	—	75 ft.	75 ft.
Depth	—	100 ft.	100 ft.
Setbacks	<i>Minimum and, where noted, maximum setbacks required. See Section 17.30.070 for exceptions to these requirements.</i>		
Front	25 ft.	10 ft.	10 ft.
Sides (each)	25 ft.	None	None
Street side	25 ft.	None ⁽¹⁾	10 ft.
Rear	25 ft.	None ⁽¹⁾	10 ft.
Floor Area Ratio (FAR)	0.60		
Height limit	2 stories, not to exceed 35 ft.		
Landscaping	As required by Chapter 17.32 (Landscaping)		
Parking	As required by Chapter 17.34 (Parking and Loading)		
Enclosure requirement	All uses shall be conducted within a completely enclosed building unless the specific use and zone permit otherwise. Uses allowed in an applicable zone that are determined by the Director to require outdoor storage or activities (for example, vehicle sales lots, service stations, etc.) may be exempted from this requirement.		

Notes:

(1) No setback is required unless adjacent to a residential use or zone where the setback shall be 10 ft..

Commercial Zoning Districts

17.20.050 - Commercial Design Guidelines

A. Purpose

The commercial design guidelines in this Chapter are intended to assist in preserving and rehabilitating the commercial areas of Lodi. The guidelines are also intended to provide for infill commercial development of high architectural quality that is compatible with existing architecturally superior development, to promote the conservation and reuse of existing buildings of high quality design. While these guidelines are primarily oriented toward the downtown area, they are also designed to enhance and preserve the small town character of the City's other commercial areas.

B. Applicability

See Section 17.40.020 (Applicability of Design Guidelines).

C. Site Planning Outside of the Downtown ~~Error! Bookmark not defined.~~

1. Building and parking location.

- a. General placement principles.** Buildings should generally be oriented parallel to streets and placed as close to the street as required setbacks permit. Buildings may be angled to create interesting juxtapositions if there is a specific design goal to be achieved. However, definition of the street edge is an important role for buildings that should be considered in design. Exceptions may occur for wider setbacks from the street if a compatible use is proposed (for example, outdoor dining or pedestrian rest area) or to maintain continuity with landscaped areas on adjacent properties.

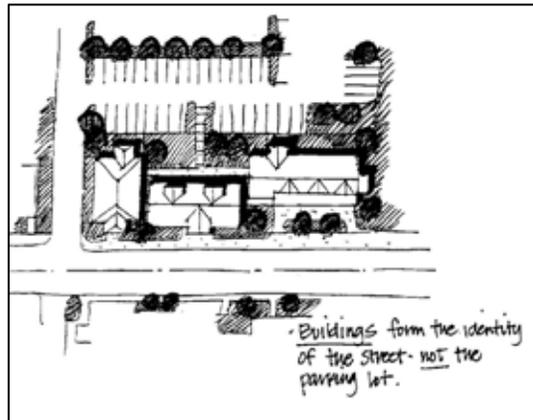


Figure 2-15 Building Placement at Street

Commercial Zoning Districts

- b. **Pedestrian orientation.** The orientation of buildings should establish and maintain pedestrian-oriented street frontages. Buildings with high pedestrian use should face, and be directly accessible from the public sidewalk.
- c. **Vehicle-oriented areas.** Buildings in areas of the City that rely more on the use of the automobile for access should be oriented to major open space and streetscape elements, and as much as possible should not be located behind parking lots located between the building and the street.
- d. **Corner buildings.** Corner buildings should have a strong tie to the setback lines of each street. The primary mass of the building should not be placed at an angle to the corner. This does not preclude angled building corners or an open plaza at the corner which are strongly encouraged.
- e. **Projects with multiple structures.** Multiple buildings in a single project should create a positive functional relationship with one another and the street. The first priority for multiple-building projects is to address the street with features that provide pedestrian interest and facilitate access to the project. Second, buildings should be clustered to achieve a "village" scale. This creates opportunities for internal plazas and pedestrian areas. When clustering is impractical, a visual link should be established between buildings. This link can be accomplished through the use of an arcade system, trellis, colonnade, and through enhanced paving.

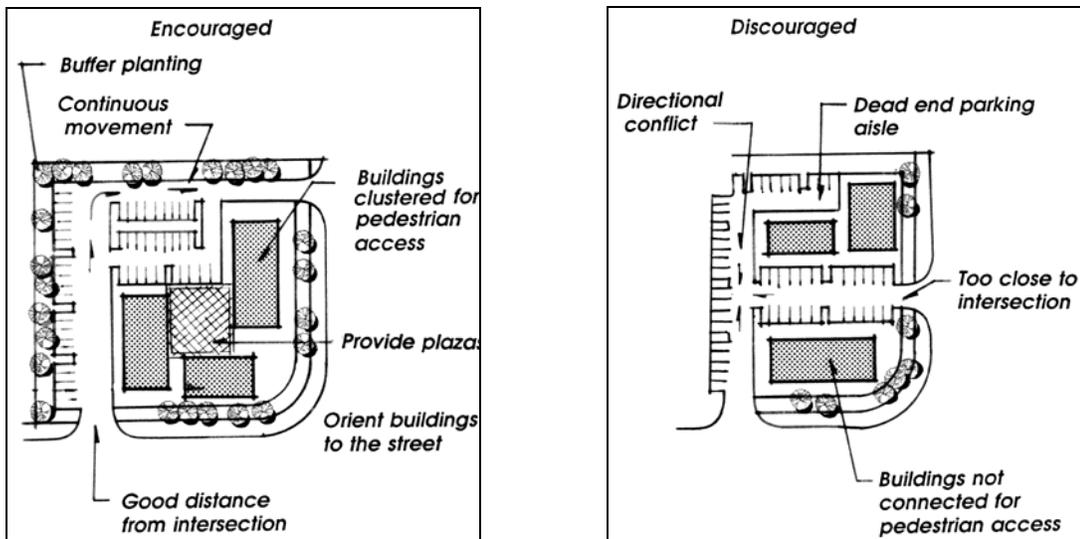


Figure 2-16 Location of Parking and Buildings

- f. **Open space areas.** Open space areas should be accessible from the majority of structures, and should be oriented to take advantage of sun or shade as appropriate.

Commercial Zoning Districts

- g. Pedestrian walkways.** Projects should connect the on-site pedestrian circulation system to the off-site public sidewalk at intervals of at least one connection for each 200 linear feet (or fraction thereof). Parking areas should be connected to building entrances by means of enhanced paving (patterned or stamped).

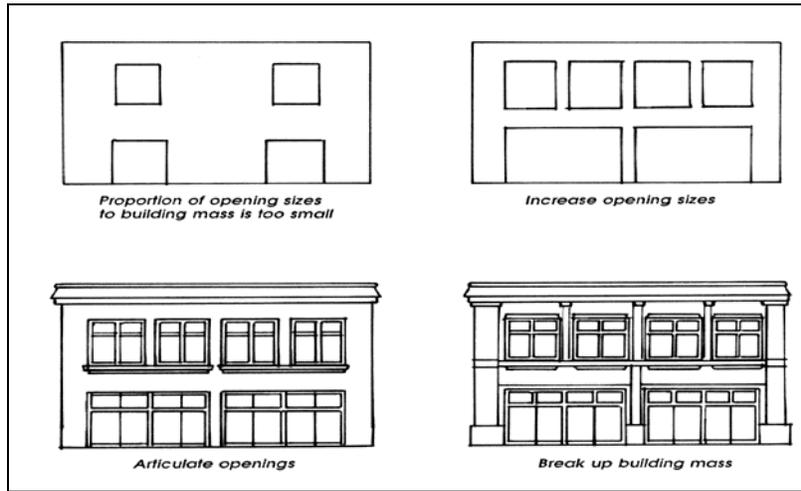
2. Building Orientation

The organization and orientation of commercial buildings and spaces is crucial in creating streets that are welcoming and friendly to pedestrians. Active public spaces and city streets that are heavily used by pedestrians contribute to the character and economic vitality of the City. High levels of pedestrian activity — shopping, eating, “people watching,” exercising, strolling, relaxing, walking from place to place — are valued and encouraged throughout the City’s commercial areas. These activities create interest, provide a sense of safety on the street, and add to community image and identity. The following guidelines provide a framework for site organization and orientation of commercial land uses.

a. Street orientation.

- (1)** Building facades, including storefronts, should be designed to orient to the major street frontage, with the primary building entry also oriented toward the major street.
- (2)** Buildings on corners should include storefront design features for at least 50 percent of the wall area on the side street elevation.
- (3)** Long, blank, unarticulated street wall facades are strongly discouraged unless unavoidable because of specific site circumstances. Monolithic street wall facades should be "broken" by vertical and horizontal articulation, characterized by:
 - (a)** Sculpted, carved or penetrated wall surfaces defined by recesses and reveals);
 - (b)** Breaks (reveals, recesses);
 - (c)** Window and door openings; or
 - (d)** Balconies, awnings and canopies.

Commercial Zoning Districts



Urban Design Studio

Figure 2-17 Storefront Proportions

- (4) Large unbroken facade surfaces should be avoided at the storefront level. This can be achieved in a number of ways including:
 - (a) Dividing the facade into a series of display windows with smaller panes of glass;
 - (b) Constructing the facade with small human scale materials such as brick or decorative tile along bulkheads;
 - (c) Providing traditional recessed entries;
 - (d) Careful sizing, placement and overall design of signage; and
 - (e) Providing consistent door and window reveals.

b. Street edge.

- (1) The first floor of any infill commercial building should be built directly at the front property line C abutting the sidewalk C unless minor variations are permitted by the City for plazas, public art or other pedestrian-oriented purpose.

Commercial Zoning Districts

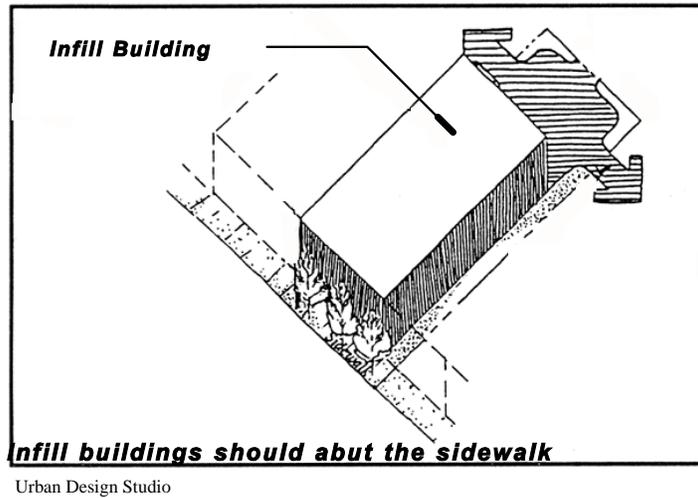


Figure 2-18 Placement of Infill Buildings

- (2) Awnings, trellises and other accessory structures that are relatively open and do not restrict pedestrian or vehicular movement may project over the right-of-way with City approval.

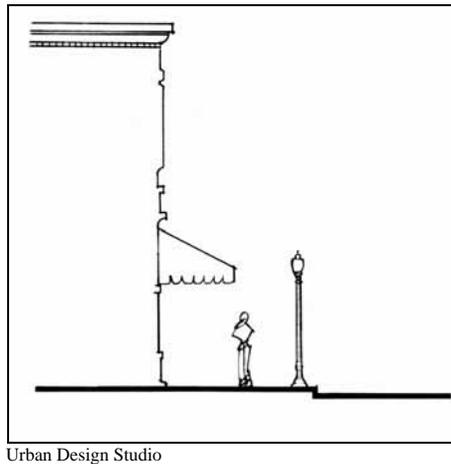


Figure 2-19 Awnings Provide Pedestrian Scale and Comfort

- (3) Any building located at a corner intersection should incorporate architectural features at the ground floor which emphasize the importance of pedestrian movement. These features may include building cut-offs, walk-through covered arcades, trellis structures and other elements which focus visual interest on the corners.

Commercial Zoning Districts



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Figure 2-20 Desirable Design Features for Street Corner Buildings

- c. **Public spaces.** Public space associated with commercial buildings should serve as a transition from the outside to the inside of buildings, and should be supportive of pedestrian activity. Public areas should be designed so that all members of the public feel that they may enter, and should be designed as extensions of the public space of the sidewalk (e.g., by providing pedestrian amenities such as benches and fountains, and by continuing the hardscape of the sidewalk).

(1) Plazas.

- (a) Plazas should be designed to supplement, rather than replace on-street activity areas. Paving and furniture used in private plazas should complement streetscape elements used in the public right-of-way.
- (b) Retail shops, restaurants, offices or other activity-generating uses should be located at the edges of plazas; blank walls should be minimized adjacent to pedestrian spaces.
- (c) Plazas should be designed with unimpeded lines-of-sight to and from the public sidewalk. Security fences, walls, and entry gates shall not block the sidewalk edge of the plaza or views into the plaza. At least 15 feet of building frontage should be transparent or visually penetrable to provide entry to and views into the plaza.
- (d) Entries to a plaza, and storefront entries within the plaza, should be designed and lit so they do not create hiding places.
- (e) Visual features, such as public art or a fountain, should be incorporated in plazas to attract pedestrians.
- (f) Seating should be provided at a ratio of at least one seating area for each 100 square feet of plaza, in addition to any outdoor dining areas.
- (g) Plazas, including all entrances and exits, should be fully illuminated one-half hour after sunset to one-half hour before sunrise to facilitate natural surveillance opportunities and to discourage illegitimate activities. Lighting

Commercial Zoning Districts

should be designed to help define, order and further develop the design concept of the space in a manner that appears welcoming to pedestrians.

- (h) Signs or other mechanisms should identify that the plaza is available for public use during business hours.
- (i) Security gates and security fencing may not be used in plazas, except as may be permitted by the Review Authority when granting the land use permit for the project, based on a determination that unusual circumstances justify their use.

(2) Entry courtyards. Where used, courtyards should:

- (a) Include focal elements of sculpture, water, plantings and seating niches.
- (b) Be provided trees that are drought tolerant, and planted in symmetric patterns.
- (c) Be designed to provide both visibility and separation from the street.

D. Building Design and Architecture

The visual relationships between individual commercial buildings contribute to community identity, levels of pedestrian activity, and economic vitality. When the architectural features (for example, entry spacing, window lines, and signs) of commercial buildings are complementary, the larger district image becomes more positive and unified. Building facades, in particular, influence cohesiveness, legibility and aesthetic pride. Likewise, storefront design can encourage shopping, increase a sense of security, and generate pedestrian activity. Where commercial buildings are neighbors to residential buildings, consideration of scale, detail, and materials are even more important.

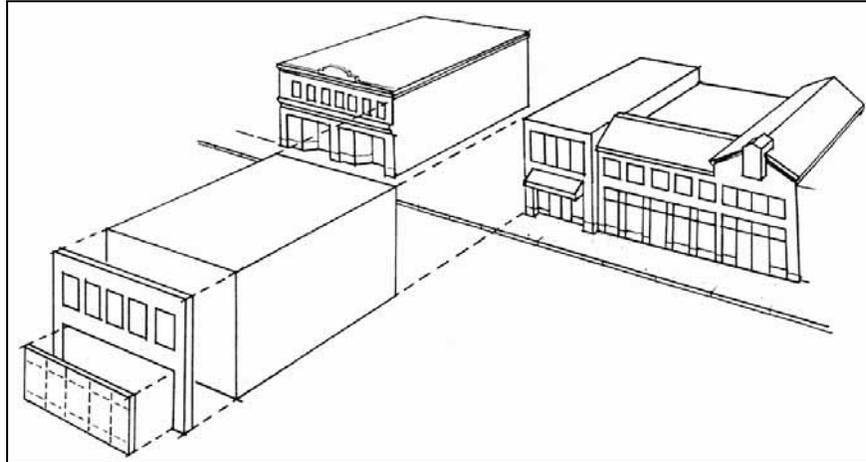
- 1. Mass and scale.** Building scale and massing contributes to the unique character and pedestrian-orientation of the downtown and some of the other commercial areas of Lodi. Smaller-scale buildings, or buildings perceived to be of small-scale, are most suitable to creating the atmosphere desired in the City. Human-scale buildings are comfortable and create a friendly atmosphere that enhances the marketability of commercial areas. To this end, buildings – and their parts – should impart a sense of human scale and assume a reinforcing relationship with the sidewalk, street, and pedestrian activity. The relationship of the building elements to the overall building should create a clear set of meaningful relationships between the building and the elements and the elements to each other. The elements of the building should not appear as random or unrelated to each other.

The height and scale of infill development and alterations to existing development within commercial areas should complement existing structures while providing a sense of human scale and proportion. Infill structures should be designed to provide storefront windows, doors, entries, transoms, awnings, cornice treatments and other architectural features designed to complement existing structures without duplicating a particular architectural style. Specific guidelines related to the mass and scale of building architecture are as follows:

Commercial Zoning Districts

a. **General design principles.**

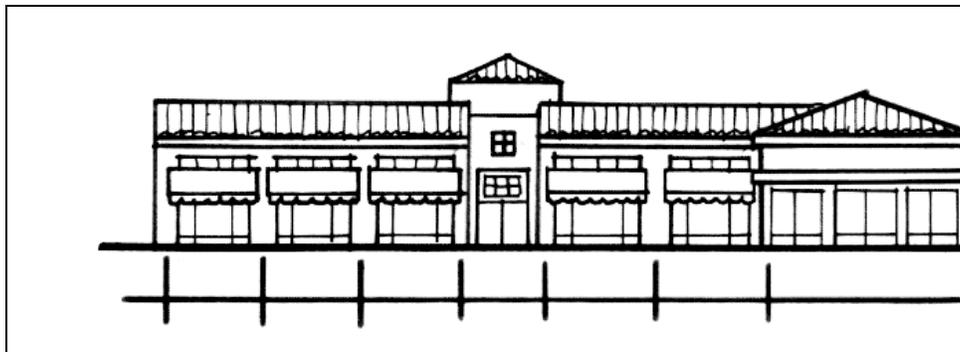
- (1) The characteristic proportion (relationship of height to width) of existing facades should be considered in relation to infill development, alterations and additions.



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Figure 2-21 Infill Façade Proportions

- (2) Whenever an infill building is proposed which is much "wider" than the existing characteristic facades on the street, the infill facades should be broken down into a series of appropriately proportioned "structural bays" or components typically segmented by a series of columns or masonry piers which frame window, door and bulkhead components. Creating and reinforcing a facade rhythm helps tie the commercial street together visually and provides the pedestrian with a standard measurement of his or her progress.

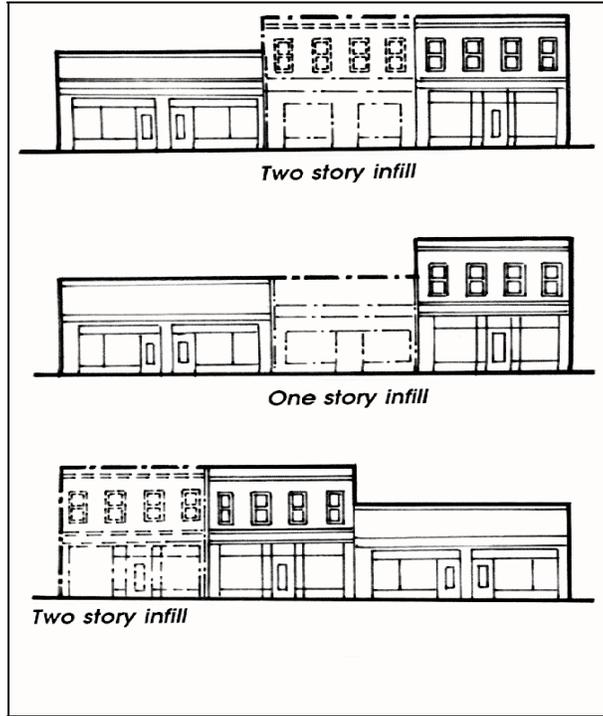


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Figure 2-22 Infill Structure Façade Divided into Bays

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- (3) The commercial buildings in Lodi are generally in the one- to three-story range. Infill buildings or upper story additions to existing buildings should not be much higher or lower than the height of surrounding structures.



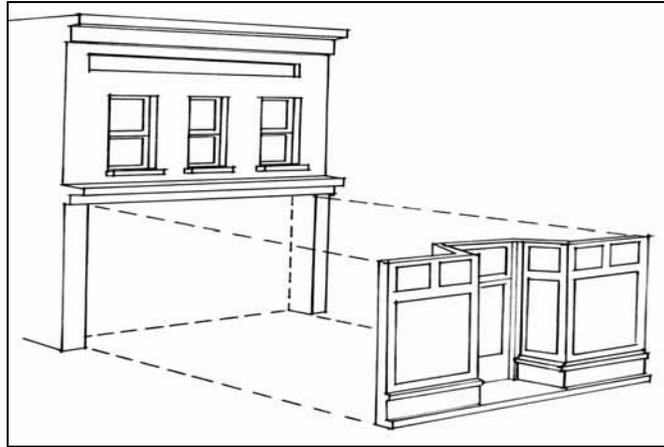
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Figure 2-23 Infill Consistent with Context

Commercial Zoning Districts

b. Building proportions.

- (1) Maintain a clear visual distinction between upper story openings and street level storefront openings (windows and doors). There should usually be more window area at the storefront level to provide greater interior visibility for pedestrians, as opposed to upper stories with smaller window openings.



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Figure 2-24 New Storefront Proportions Consistent with Context

- (2) Whenever an infill building is proposed between two adjacent commercial structures, the characteristic rhythm, proportion and spacing of existing door and window openings should be maintained.

c. Horizontal rhythms/alignment of architectural elements.

- (1) When an infill building is proposed, the common horizontal elements (e.g. cornice line, window height/width and spacing) of neighboring structures should be identified and the infill should have similar rhythm or alignment.
- (2) If maintaining a horizontal rhythm or alignment in an infill building is very difficult or otherwise impossible, the use of fabric canopies or awnings is encouraged to establish a shared horizontal storefront rhythm.

d. Architectural features.

- (1) Features such as balconies, open and enclosed turrets, finials and bay windows that help give human scale and interest to buildings are encouraged.
- (2) Decorative ornamentation, the decorative use of color and integral color materials, and architectural composition that employs abstract or representational ornament to help order the facade or emphasize the relative importance of different building elements are encouraged.

2. **Architectural elements/materials.** The types of materials and architectural elements incorporated into commercial buildings contribute to visual interest, community image,

Commercial Zoning Districts

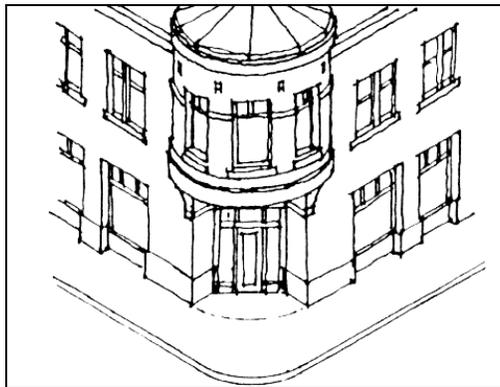
business identity and architectural quality. The following guidelines provide a framework for creating a cohesive commercial character while providing flexibility and promoting unique architectural features.

a. Entries and doorways.

(1) Main building entries should emphasize the point of arrival in one or more of the following ways:

- placement of art or decorative detailing
- a turret or balcony over the entrance
- change of material or detailing
- greater concentration of ornaments
- flanked columns, decorative fixtures or other details
- recesses within a larger arched or cased decorative opening
- a portico (formal porch) projecting from or set into the building face
- changes in roofline, a tower, or a break in the surface of the subject wall
- architectural features above it

(2) Street corner commercial buildings should provide a prominent entrance to street level shops or lobby space at or near the corner.



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Figure 2-25 Prominent Corner Entrance

Commercial Zoning Districts

b. Doors and windows.

- (1) Retail store doorways should contain a high percentage of glass to allow pedestrians to see the retail contents and activities in stores.

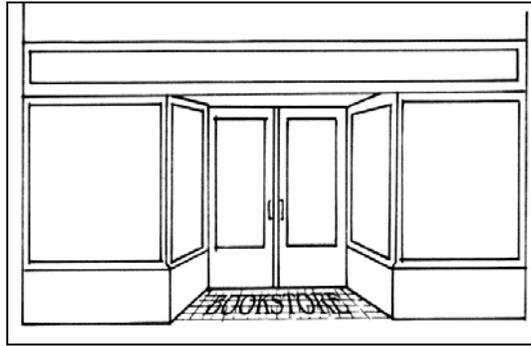


Figure 2-26 Maintain Extensive Storefront Glass Area

- (2) When windows are added or changed, it is important that the design be compatible with the facade theme of the block (streetscape).
- (3) Use clear glass (at least 88 percent light transmission) in first floor windows.
- (4) Storefront windows should be as large as possible and no closer than 18 inches from the ground (bulkhead height). By limiting the bulkhead height, the visibility to the storefront displays and retail interior is maximized. Maximum bulkhead heights for infill construction should be 17 inches.

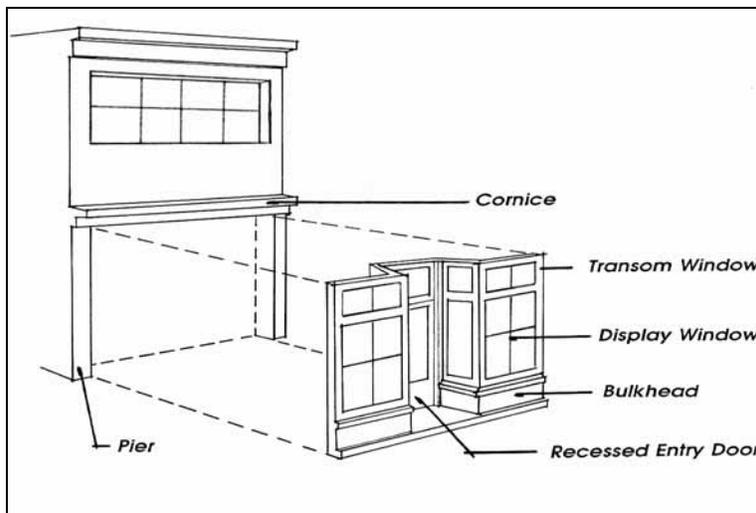


Figure 2-27 Storefront Components

Commercial Zoning Districts

- (5) Introducing or changing the location or size of windows or other openings that alter the architectural rhythm or character of the original building is discouraged.
- (6) Permanent, fixed security grates or grilles in front of windows are not permitted. Any necessary security grilles should be placed inside, behind the window display area.
- (7) Traditional storefront transom windows should be retained whenever feasible. If the ceiling inside the structure has been lowered, the ceiling should be stepped up to meet the transom so that light will penetrate the interior of the building.



Figure 2-28 – Transom windows

c. Awnings and canopies

- (1) Where the facade of a commercial building is divided into distinct structural bays (sections defined by vertical architectural elements, such as masonry piers), awnings should be placed within the vertical elements rather than overlapping them. The awning design should respond to the scale, proportion and rhythm created by these structural bay elements and *nestle* into the space created by the structural bay.

Commercial Zoning Districts

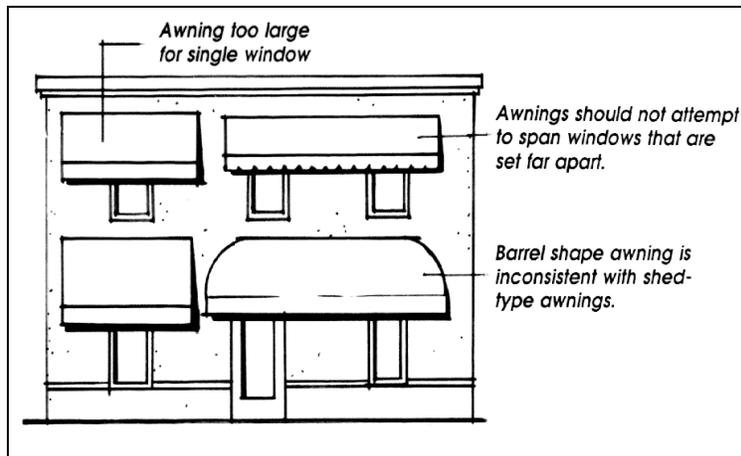
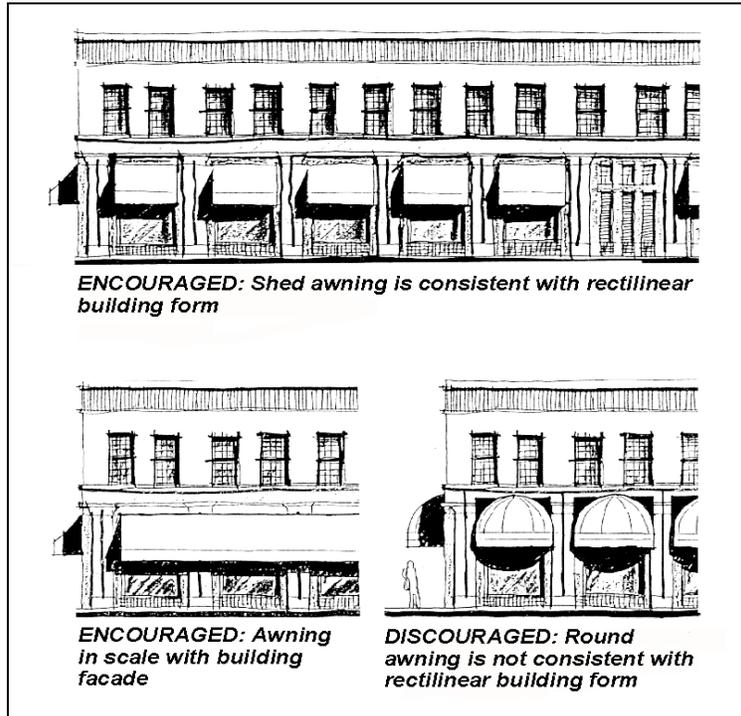


Figure 2-29 Appropriate and Inappropriate Awning Forms

- (2) Awning shape should relate to the window or door opening. Barrel-shaped awnings should be used to complement arched windows while square awnings should be used on rectangular windows.
- (3) Awnings shall not be internally illuminated.

Commercial Zoning Districts

- d. **Grillwork/metalwork and other details.** There are a number of details, often thought of as mundane, which may be incorporated into the design to add a degree of visual richness and interest while meeting functional needs. These details include the following:
- (1) Light fixtures, wall mounted or hung with decorative metal brackets.
 - (2) Metal grillwork, at vent openings or as decorative features at windows, doorways or gates.
 - (3) Decorative scuppers, catches and down-spouts, preferably of copper.
 - (4) Balconies, rails, finials, corbels, plaques, etc.
 - (5) Flag or banner pole brackets.



Figure 2-30 Metalwork Design Elements

- e. **Exterior walls and materials.** The exterior wall design elements of commercial buildings involve two aspects C color and texture. Materials with integral color such as hard smooth troweled plaster, tile and stone is encouraged. If the building's exterior design is complicated, with many design features, the wall texture should be simple and subdued. However, if the building design is simple (perhaps more monolithic), a finely textured material, such as patterned masonry, can greatly enrich the building's overall character.

Storefront materials should complement the materials used on significant adjacent buildings. The following materials are considered appropriate for commercial buildings within Lodi. The number of different wall materials used on any one building should be kept to a minimum, ideally two or less.

- clear glass
- glass block (transom)

Commercial Zoning Districts

- exterior plaster (smooth trowelled preferred)
- new or used face-brick
- cut stone, rusticated block (cast stone)
- ceramic tiles (bulkhead)
- clapboard (where appropriate)

The following exterior building materials are considered inappropriate in Lodi commercial areas and are discouraged:

- Mirror glass and heavily tinted glass.
- Windows with false divisions (i.e., a window where the glass continues uninterrupted behind a surface mounted mullion)
- Vinyl and aluminum siding
- Painted or baked enamel metal awnings
- Rough “Spanish lace” stucco finish
- Unpainted plywood
- False stone veneer
- Bulky cast stone window and door surrounds
- Corrugated sheet metal
- Corrugated fiberglass
- Split face concrete block
- Exposed concrete block without integral color

f. Lighting.

- (1) Lighting should be designed as an integral part of the overall site and building design. It should contribute to and help define the character and the spaces created by the building and its site development. Lighting design should have a conscious purpose of helping to strengthen the constituent elements of the site through means such as highlighting areas that are more important, or by picking out areas of the site that have a different character from the rest of the site. Lighting should complement architectural elements, changes in material of the ground plane and landscaping. The intensity, color, placement of the light and the placement and design of the light fixtures should be part of this effort.
- (2) Lighting should provide illumination for the security and safety of on-site areas such as entrances, exits, parking, loading, pathways, and working areas.
- (3) Lighting should be provided for the pedestrian to create a sense of welcoming on the public sidewalk, that the pedestrian is literally being accompanied by light. Providing a greater number of softer light sources is strongly encouraged over having only a few very bright lights.
- (4) The design of light fixtures and their structural supports should be architecturally compatible with the main buildings on the site. Fixtures should be integrated within the architectural design for the buildings.
- (5) As a security device, lighting should be adequate but not overly bright. Building entrances should be well lighted. The lighting should be designed so that the lighting is an attractive element in its own right, acting as a public amenity.

Commercial Zoning Districts

- (6) All lighting should be shielded to confine light spread within the site boundaries. Lighting should be provided from one-half hour after sunset to one-half hour before sunrise at all exits, entrances, loading areas, parking lots, plazas, and alleys. An average of one foot candle evenly distributed across the site is a suggested minimum; with up to two foot candles at entrances, exits and loading areas.
 - (7) The following lighting fixtures and lamps are considered inappropriate in Lodi commercial areas and are discouraged:
 - Mercury vapor lights (metal halide or high-pressure sodium lamps are preferable for most applications)
 - Fluorescent light tubes that are exposed without filtering lenses
 - Fluorescent lamps without non-color corrected bulbs (color correction may also be accomplished by a color-correcting lens)
 - The Development Code prohibits flashing or blinking lights.
 - (8) Focus light downward. A good rule of thumb is to make sure that direct light shines a minimum of 20 degrees below a horizontal plane and in no case above the horizontal plane.
 - (9) Test installations for glare. After installation, check to make sure that glare will not be a problem for neighbors, pedestrians, or motorists.
 - (10) Illuminate signs and billboards from above, not below.
 - (11) Avoid reflective surfaces beneath down-lit signs.
- g. Rear entrances.** Rear entrance design should consider a number of issues. In general, the rear entrance must respond to the same needs as the storefront, but at a reduced scale. It must also meet the functional service needs of the business, including providing a loading area. Since these two functions are often in conflict, the design of the rear entrance must be carefully planned. A particular concern is the storage and disposal of refuse. All trash cans, dumpsters, and other containers must be hidden and screened from public view as required by the Zoning Ordinance. Exterior utilities must be screened. Regular maintenance is of paramount importance.

The design of a rear entrance should be appropriate to its surroundings. The visual character of rear facades, alleys, and parking lots is relatively casual and utilitarian, especially when compared to formal street facades. In this context, a refined or grand design can look out of place. The design should instead be pleasantly inviting, and architecturally compatible with the front, but very simple in detail.

Commercial Zoning Districts



**Figure 2-31
Rear Entrances should be
Aesthetically Simple and Utilitarian**

- (1) An awning can soften a rear facade and provide a pleasant protected space.
- (2) The rear entry door should be wood and glass similar to the front door. Special security glass (i.e. wire imbedded) is allowed.
- (3) Security lighting should be modest and should focus on the rear entry door.
- (4) Selective use of tree plantings, potted plants and other landscaping can subtly improve a rear facade.
- (5) Refuse containers and service facilities must be screened from view by solid walls. The use of landscaping (shrubs and vines) to screen walls can help deter graffiti.

Commercial Zoning Districts

3. **Alterations - Restoration and remodeling.** Renovating or remodeling commercial structures provides an excellent means of maintaining and reinforcing the character and image desired in Lodi. Renovation and expansion not only increases property values in the area but also serves as an inspiration to other property owners and designers to make similar efforts.

When an existing structure is to be renovated or added to, the work should respect the original design character of the structure. The following design guidelines apply where appropriate and whenever a structure is to be renovated or expanded.

- a. **Traditional features & decoration.**

- (1) Sensitive response to existing materials, details, proportions, as well as patterns of materials and openings is required when any such work will affect the appearance of an existing building's exterior.
- (2) Storefront remodeling often retains original decorative details only as visual "leftovers" or simply covers them. If a building is to be refurbished, these forgotten details should not be wasted. If enough of them remain, they can be restored as part of the original design. If only a few remain, they can be incorporated as design features in a new storefront. In either case, the design of any improvements should grow out of the remaining traditional details and create a harmonious background which emphasizes them.

- b. **Removal of elements inconsistent with original facade.** Existing building elements incompatible with the original facade design should be removed. These include: excessive use of exterior embellishments and "modernized" elements (metal grilles, oversized molding cornices or rusticated materials, etc.). Buildings are often altered over time by owners or shopkeepers to "keep up with changing times" or to "remake a tired image." Unfortunately, such changes are often done in a "tacked-on" manner and result in gradual but severe erosion of original character and cohesion of the commercial area.

- c. **Storefront renovation.**

- (1) Where the original storefront remains (little or no remodeling has occurred), it should be preserved and repaired with as little alteration as possible.
- (2) Where only part of the original storefront remains (limited remodeling has occurred), the storefront should be repaired, maintaining historic materials where possible, including the replacement of extensively deteriorated or missing parts with new parts based upon surviving examples of transoms, bulkheads, pilasters, signs, etc.

Commercial Zoning Districts

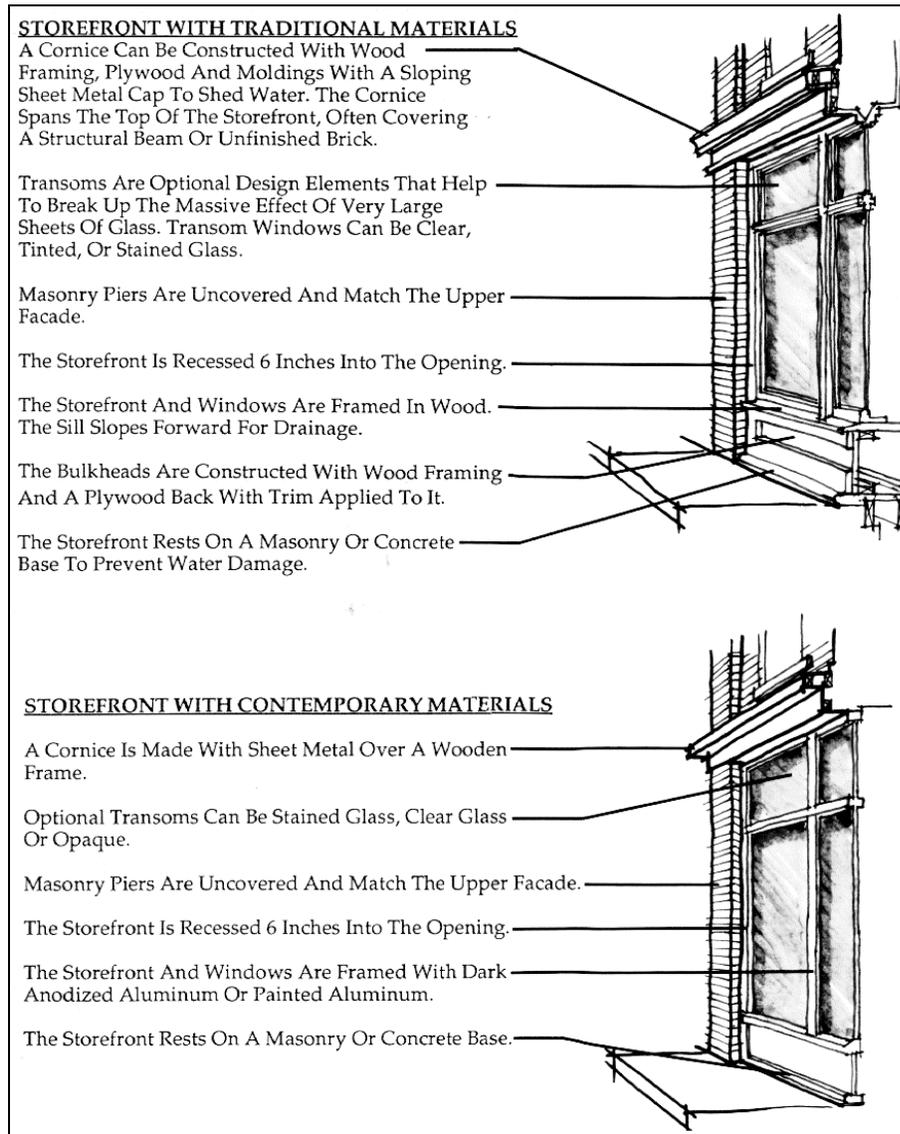


Figure 2-32

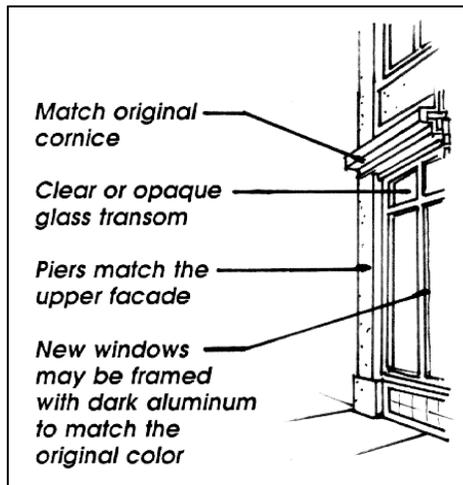
- (3) Where the original storefront is completely missing (extensive remodeling has occurred), the first priority is to reconstruct the storefront based upon historical, pictorial and physical documentation. If that is not practical, the design of the new storefront should be compatible with the size, scale, proportion, material and color of the existing structure.

d. Window replacement.

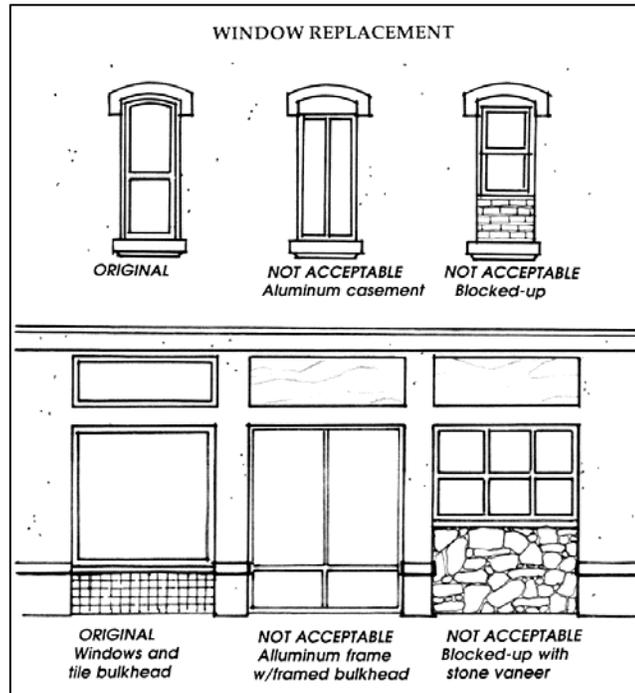
- (1) The impact of windows on the facade is determined by the size, shape, pattern of openings, spacing and placement within the facade. When altering or reconstructing windows, consideration of these elements is crucial to retaining the structure's original architectural balance and integrity.

Commercial Zoning Districts

- (2) Wherever possible retain original window openings. If the existing ceiling has been lowered, pull the dropped ceiling back from the original window.
- (3) If possible, save and restore original windows and frames. Replace missing, rotting or broken sash, frames, mullions and muntins with similar material.
- (4) If original window openings have been altered, restore the openings to their original configuration and detail. Avoid blocking or filling window openings that contribute to the overall facade design.
- (5) When replacing windows, consideration should be given to the original size and shape detailing and framing materials. Replacement windows should be the same operating type as the original window.



Commercial Zoning Districts



e.

Door replacement.

Figure 2-33 – Window replacement

(1) Original windows should be repaired and refinished provided they can comply with the requirements of the Americans with Disabilities Act (ADA).

(2) If replacement doors are necessary, they should be compatible with the historical character and design of the structure.

f. Additions to existing structures.

(1) The design of a proposed addition should follow the general scale, proportion, massing and detailing of the original structure and should result in a harmonious—rather than stark-contrast.

(2) Additions should be interpretations of the existing buildings, with the main characteristics of the existing structure incorporated using modern construction methods. This may include: the extension of architectural lines from the existing structure to the addition; repetition of window and entrance spacing; use of harmonizing colors and materials; and the inclusion of similar, yet distinct, architectural details (i.e., window and door trim, lighting fixtures, tile or brick decoration, etc.).

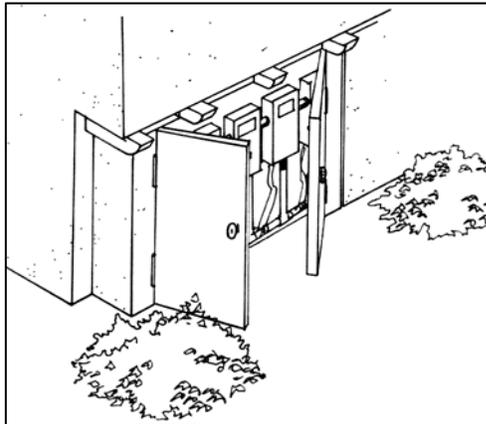
Commercial Zoning Districts

(3) Additions should be designed so that if the addition were to be removed in the future, the essential form and integrity of the original structure would be unimpaired.

g. Seismic retrofitting. Where structural improvements for seismic retrofitting affect the building exterior, such improvements should be done with care and consideration for the impact on appearance of the building. Where possible, such work should be concealed. Where this is not possible, the improvements should be planned to carefully integrate into the existing building design.

4. Equipment and utilities.

- a. All mechanical or utility equipment, whether on the roof, ground or side of building must be screened from view, above or below. The method of screening should be architecturally integrated with the structure in terms of materials, color, shape and size. The design of the screening should be done in concert with and as a part of the design of the building, rather than as an afterthought.
- b. Roof mounted mechanical or utility equipment must be screened. The method of screening should be architecturally integrated with the structure in terms of materials, color, shape and size. It is preferable to screen equipment with solid building elements (e.g. parapet wall) instead of after-the-fact add-on screening (e.g. wood or metal slats).
- c. Air conditioning units placed in individual windows and window transom areas are *strongly* discouraged.



Commercial Zoning Districts

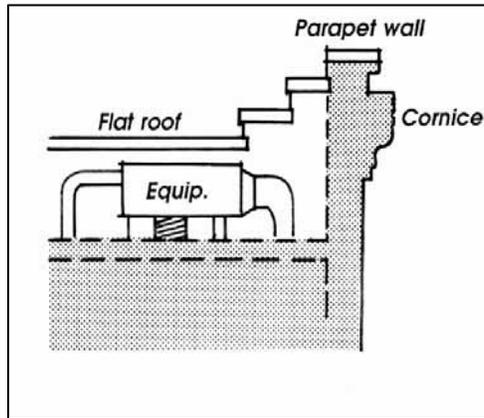


Figure 2-34 – Equipment Screening

E. Security

Safety in commercial areas is important to business owners, workers and visitors, and to the overall economic well-being of businesses. Design strategies incorporating safety should be based on the practice called Crime Prevention Through Environmental Design or CPTED (pronounced “sep-ted”). CPTED focuses on the proper design and use of the built environment to enhance opportunities for positive, constructive activities while minimizing opportunities for illegitimate or undesirable activities. CPTED is a measure for proactive and retrofit crime prevention planning – one that can help reduce the incidence and fear of crime, and improve overall quality of life. By no means is CPTED a panacea in the efforts to rid communities of crime; however, it is one strategy among others that should be considered in commercial development. There are five principles used in the application of CPTED: Natural Surveillance, Natural Access Control, Territorial Reinforcement, and Management and Maintenance.

1. **Natural surveillance.** The organization of physical features, activities and people in such a way as to maximize visibility. That is, a space should be designed so that users feel that they will be seen or observed if they do something illegitimate. The placement of windows, doors, and plazas, the alignment of sidewalks and paths, the minimization of large areas of blank walls, the locations and levels of lighting, and the proper design and size of open spaces can contribute to natural surveillance opportunities.
2. **Natural access control.** The physical guidance of people coming and going from a space by the judicious placement of entrances, exits, signs, fencing, landscaping and lighting. A space should be designed so that it is conducive to the activities undertaken by legitimate, law-abiding users but discouraging to those carrying out undesirable activities.
3. **Territorial reinforcement.** The use of physical attributes that express ownership, such as fences, pavement treatments, art, signage and landscaping. CPTED stresses the importance of clearly designing a hierarchy of spaces: public (i.e. intended for all to use), semi-private (i.e. intended for specific users or uses), private (i.e. intended for private use by businesses).

Commercial Zoning Districts

- 4. **Management and maintenance.** The continued use of a space for its intended purpose, which serves as an additional expression of ownership (e.g., complying with landscape maintenance and lighting standards to ensure that visibility is not reduced). Although the physical dimensions of CPTED are important, no effort is sustained unless it is properly maintained and operated.
- 5. **Design considerations.** The following design considerations, as well as many of the preceding guidelines, incorporate CPTED principles and should be considered for new commercial development in Lodi.

- a. **Window materials.** Storefront security may be enhanced with shatter-resistant laminated vigil pane security glass (or glass-clad polycarbonate windows).

- b. **Security screens.** The use of exterior scissors-style security screens is strongly discouraged. Any use of interior scissors screens shall be concealed from public view when not in use by retracting the screen into casings which are in proportion and scale with the building architecture. However, although they are allowed, the use of interior scissors screens is strongly discouraged since they communicate a message of high crime and cannot be integrated visually into the overall design of a building or storefront.

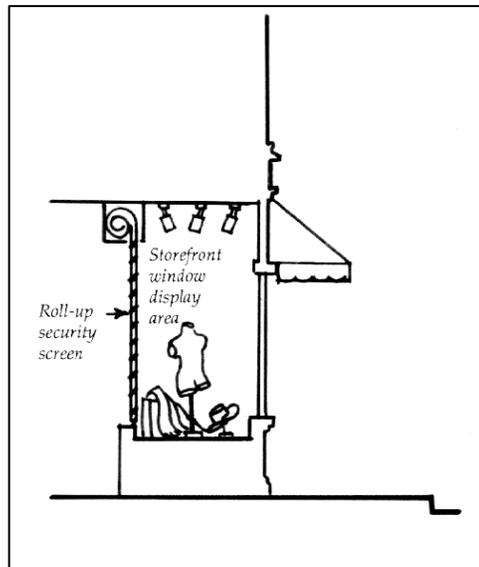


Figure 2-35 – Security Screen Placement

- c. **Security bars.** Permanent security bars (those clearly visible and fixed to windows or the facade) and exterior roll-up metal security doors are strongly discouraged.
- d. **Lighting placement.** Exterior lights that are a part of streetscape improvements should provide adequate lighting levels. However, in the case of a deep threshold to a building, a light applied to the ceiling of this area is strongly recommended to illuminate building entrances.
- e. **Lighting design.** Lighting should be designed to satisfy both functional and decorative needs. Storefront lighting should complement the architectural style of the building while providing illumination of building facades and entrances.
- f. **Rear security lighting.** Rear security lighting should be provided and maintained at 12 foot candles per square foot. The level of lighting should be measured at ground level. All security lighting should be designed as part of an overall lighting plan rather than as single stand-alone elements.

Commercial Zoning Districts

- g. Window signs.** Any window signs should be placed to provide a clear and unobstructed view of the interior of the business establishment from the sidewalk (and are not permitted by this Zoning Ordinance to exceed 20 percent of the window area).
- h. Street addresses.** Street addresses should be identified by posting numbers so they are clearly visible from the public right-of-way wherever possible.



**Figure 2-36
Street Addresses Should Help Easy
Identification**

- i. Safety behind buildings.** Safety behind buildings should be ensured through use of:
 - (1) Adequate security lighting for parking areas and pedestrian ways;
 - (2) Limited access (walls, fences, gates, shrubs);
 - (3) Signage;
 - (4) Introduction of activities (e.g., rear entrances for commercial activities) that increase surveillance;
 - (5) Surveillance through windows or with cameras; and
 - (6) Ongoing maintenance of storage areas and alleys.

Commercial Zoning Districts

F. Walls and Fences

If walls or fences are not required for a specific screening or security purpose they should not be used. Any necessary walls or fences should be as low as possible while still performing their screening and security functions.

- 1. Materials and colors.** Fences and walls should be designed with materials and colors that complement project architecture. When selecting wood as a material, it is important to consider its increased maintenance demands. Landscaping should be used in combination with walls whenever possible.
- 2. Design.** Long expanses of wall or fence surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets should be provided along the wall.
- 3. Security fencing.** When required, security fencing should be a combination of solid pillars, or short solid wall segments, and wrought iron grille work.

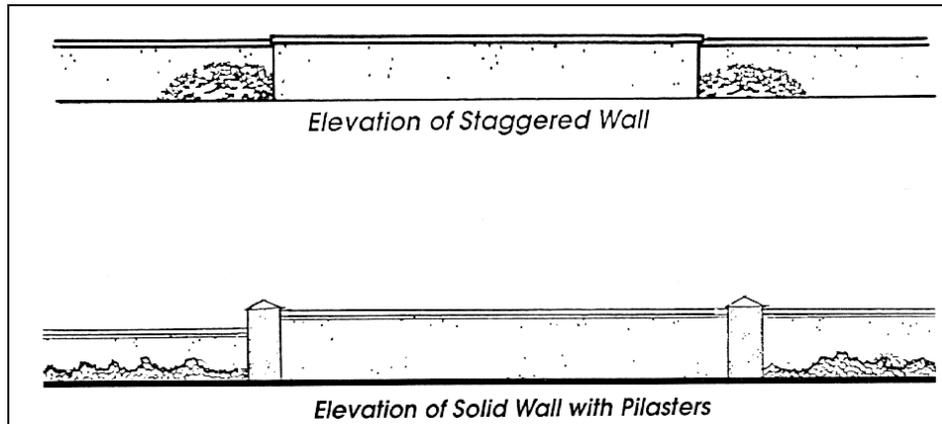


Figure 2-37 Appropriate Wall Design

Industrial Zoning Districts

CHAPTER 17.24 - INDUSTRIAL ZONING DISTRICTS

Sections:

- 17.24.010 - Purpose of Chapter
- 17.24.020 - Purposes of Industrial Zoning Districts
- 17.24.030 - Industrial Zoning District Land Uses and Permit Requirements
- 17.24.040 - Industrial Zoning District General Development Standards
- 17.24.050 - Industrial Design Guidelines

17.24.010 - Purpose of Chapter

This Chapter lists the uses of land that may be allowed within the industrial zoning districts established by **Section 17.10.020 (Zoning Districts Established)**, determines the type of land use permit/approval required for each use, and provides basic standards for site development.

17.24.020 - Purposes of Industrial Zoning Districts

The industrial zoning districts are intended to provide for a range of industrial uses, emphasizing high quality development, and to encourage revitalization of existing industrial land uses. The purpose of the individual industrial zoning districts and the manner in which they are applied are as follows.

- A. M (Industrial) District.** The M zoning district is applied to areas appropriate for a mix of heavy manufacturing, warehousing, general service, storage, and distribution activities. The maximum FAR is 0.6. The M zoning district is consistent with the Industrial land use designation of the General Plan.
- B. BP (Business Park) District.** The BP zoning district is applied to areas appropriate for office activities that generate high employment yield per acre. It accommodates campus-like environments for a corporate headquarters and other office parks. This designation may also provide for light industrial and production facilities. The maximum FAR is 1.0. The BP zoning district is consistent with the Business Park land use designation of the General Plan.

17.24.030 - Industrial Zoning District Land Uses and Permit Requirements

Table 2-10 identifies the uses of land allowed by this Development Code in the industrial zoning districts, and the land use permit required to establish each use, in compliance with **Section 17.12.030 (Allowable Land Uses and Permit Requirements)**.

Note: where the last column in the tables ("Specific Use Regulations") includes a section number, the regulations in the referenced section apply to the use; however, provisions in other sections of this Development Code may also apply.

Industrial Zoning Districts

TABLE 2-10 Industrial - Allowed Uses and Permit Requirements	A	Allowed Use		
	UP	Use Permit Required		
	MUP	Minor Use Permit Required		
	—	Use not allowed		
LAND USE	PERMIT REQUIRED			Specific Use Regulations
		M	BP	

AGRICULTURE AND OPEN SPACE

Production of crops		A	A	
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RECREATION, EDUCATION, & PUBLIC ASSEMBLY USES

City offices		A	A	
Clubs, lodges & membership halls		A	—	
Health/fitness facilities		—	A	
Indoor sports facility		—	A	
Outdoor recreation facilities		UP	UP	
School - Specialized education and training		A	A	
Studio - Art, dance, martial arts, music, etc.		UP	UP	

RESIDENTIAL USES

Residential shelters/transitional housing		UP	—	
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RETAIL TRADE

Accessory retail uses		A	A	
Adult entertainment business		A ⁽¹⁾	—	
Alcoholic beverage sales, on-site		UP	UP	
Auto parts sales		A	—	
Auto sales and rental		A	A	
Building material stores		A	—	
Construction/heavy equipment sales and rental		A	A	
Convenience stores		—	UP	
Drive-in and drive-through sales and services		—	UP	
Gas Stations		A	UP	
Mobile home and RV sales		A	A	
Plant nurseries and garden supply stores		A	—	
Restaurants		A	A	

Industrial Zoning Districts

TABLE 2-10 Industrial - Allowed Uses and Permit Requirements	A	Allowed Use		
	UP	Use Permit Required		
	MUP	Minor Use Permit Required		
	—	Use not allowed		
LAND USE	PERMIT REQUIRED			Specific Use Regulations
	M	BP		

SERVICES – BUSINESS, FINANCIAL, PROFESSIONAL

Automated teller machines (ATMs)		A	A	
Banks and financial services		A	A	
Business support services		A	A	
Medical - Clinics, offices, and laboratories		—	A	
Medical - Extended care		—	A	
Medical - Hospitals		—	A	
Offices		A	A	
Professional Services		A	A	

SERVICES

Auto repair and maintenance		A	—	
Car wash		—	UP	
Contractor storage yard		A	—	
Food locker/distribution		A	—	
Hotels and motels		UP	UP	
Mortuaries and funeral homes		A	—	
Research and development		A	A	
Storage - Indoor		A	—	
Upholstering shops		A	—	
Veterinary clinics, outpatient treatment only		A	—	
Veterinary clinics, animal hospitals, kennels		A	—	

Industrial Zoning Districts

TABLE 2-10 Industrial - Allowed Uses and Permit Requirements		A	Allowed Use		
		UP	Use Permit Required		
LAND USE		MUP	Minor Use Permit Required		
		—	Use not allowed		
		PERMIT REQUIRED		Specific Use Regulations	
		M	BP		
INDUSTRY, MANUFACTURING & PROCESSING, WHOLESALING					
Accessory uses - industrial		A	A		
Auto dismantling		UP	—		
Chemical manufacturing and processing		UP	UP		
Electronics, equipment, and appliance manufacturing		A	A		
Food and beverage product manufacturing		A	UP		
Furniture/fixtures manufacturing, cabinet shops		A	UP		
Handcraft industries, small-scale manufacturing		A	UP		
Laundries and dry cleaning plants		A	—		
Lumberyards		A	—		
Metal products fabrication, machine/welding shops		A	—		
Milling		A	—		
Paper and allied product manufacturing		A	—		
Petroleum/coal product storage and processing		UP	—		
Printing and publishing		A	A		
Product assembly and packaging		A	A		
Quarry materials storage and processing		UP	—		
Railyard/boat/aircraft manufacturing and repair		UP	—		
Recycling facilities					
Small collection facility		—	—		
Large collection facility		UP	—		
Plastics manufacturing		A	UP		
Warehouses, wholesaling and distribution		A	UP		

Industrial Zoning Districts

TABLE 2-10 Industrial - Allowed Uses and Permit Requirements	A	Allowed Use	
	UP	Use Permit Required	
	MUP	Minor Use Permit Required	
	—	Use not allowed	
LAND USE	PERMIT REQUIRED		Specific Use Regulations
		M BP	

TRANSPORTATION, COMMUNICATION & INFRASTRUCTURE USES

Broadcasting studios		A	UP	
Parking facilities/vehicle storage		A	—	
Telecommunications facilities		MUP	—	
Truck and freight terminals		A	—	
Utility Facility		UP	—	

Notes:

- (1) Subject to all requirements of Municipal Code Chapter 5.40.

Industrial Zoning Districts

17.24.040 - Industrial Zoning District General Development Standards

Subdivisions, new land uses and structures, and alterations to existing land uses and structures, shall be designed, constructed, and/or established in compliance with the requirements in Table 2-11, in addition to the applicable development standards (e.g., landscaping, parking and loading, etc.) in **Article 3, (Site Planning and General Development Standards)**.

TABLE 2-11 - INDUSTRIAL DISTRICT GENERAL DEVELOPMENT STANDARDS

Development Feature	Requirement by Zoning District	
	M	BP
Minimum lot size	<i>Minimum area, width, and depth required for new parcels.</i>	
Area	10,000 sq. ft.	1 acre
Width and depth	75 ft. wide/100 ft. deep	75 ft. wide/100 ft. deep
Setbacks	<i>Minimum and, where noted, maximum setbacks required. See Section 17.30.070 for exceptions to these requirements.</i>	
Front	10 ft.	25 ft.
Street side	10 ft.	10 ft.
Sides (each)	None ⁽¹⁾	None. ⁽¹⁾
Rear	None ⁽¹⁾	None ⁽¹⁾
Floor Area Ratio (FAR)	0.60	1.0
Height limit	70 ft. ⁽²⁾	
Landscaping	Landscaping shall be provided: 1. As required by Chapter 17.32 (Landscaping) ; and 2. A minimum of 10 ft. of landscaping shall be provided across the entire frontage of an industrial building.	
Parking	As required by Chapter 17.34 (Parking and Loading)	
Enclosure requirement	All uses shall be conducted within a completely enclosed building unless the specific use and zone permit otherwise. Uses allowed in an applicable zone that are determined by the Director to require outdoor storage or activities (for example, vehicle sales lots, service stations, etc.) may be exempted from this requirement.	

Notes:

- (1) None required except: When adjacent to a res. zone boundary where a min. of 20 ft. is required.
- (2) Additional height is allowed by use permit.

Industrial Zoning Districts

17.24.050 - Industrial Design Guidelines

A. Purpose

The Industrial Design Guidelines are intended to provide clear and useful recommendations for the design, construction, review, and approval of Industrial development in the City of Lodi. The City of Lodi recognizes the unique requirements of industrial development and the potential for such development, through careful design, to make a positive contribution to the appearance of the community. Site and building design provisions herein are focused on the view from public rights-of-way and compatibility with surrounding development. This chapter will help ensure that new projects will be well designed, uphold the City’s vision, and contribute to the quality of the public realm.

B. Applicability

1. The Design Guidelines in this chapter apply to the sites designated as Industrial in the General Plan. The General Plan Industrial land use designation includes the General Mills factory and sites along the railroad and east of State Route 99. The design elements of each project (including site design, architecture, landscaping, parking design) will be reviewed on a comprehensive basis.
2. The review authority may interpret these design guidelines with some flexibility in their application to specific projects, as not all design criteria may be workable or appropriate for each project. In some circumstances, one guideline may be relaxed to facilitate compliance with another guideline determined by the review authority to be more important in the particular case. The overall objective is to ensure that the intent and spirit of the design guidelines are followed.

C. Building Orientation

1. Design industrial sites to minimize the visual impacts of parking areas, equipment, and outdoor storage from public rights-of-way. Mitigate these undesirable visual impacts through proper placement and design of buildings, screen walls, and landscaping.
2. New buildings shall be oriented toward the adjoining public streets, so that public entrances are a focal point on the building and site layout.
3. Industrial buildings shall be oriented so that bays and loading docks do not directly face the primary street frontage. Loading areas shall be located in the rear or side of the building whenever possible; however, they should never face adjacent residential uses.

D. Building Design and Architecture

1. **Massing and Scale**
 - a. Organize the massing of larger buildings into components that more readily relate to the human scale.
 - b. Reduce the mass and scale of large industrial buildings with building facades and wall height variations, and by articulating rooflines and wall planes.

Industrial Zoning Districts

- c. Avoid long, repetitive, monotonous facades – particularly those that repeat the same design element several times along the same elevation.
- d. The scale of building(s) on the site edge shall be compatible with the scale of adjoining development. Where surrounding development is of a small scale, large scale buildings shall be located internal to the site and transition down in scale as the outer edge of the site approaches.

2. Articulation and Detail

- a. Individual design elements shall be faithful to the overall architecture of the building. Architectural details shall be part of the design and not something applied as an afterthought. A consistent design theme provides continuity and avoids confusion of style.
- b. Front and street-side facades of large buildings visible from a public street or adjacent residential property shall include architectural features such as reveals, windows and openings, trellises, changes in color, texture, and material to add interest to the building elevation and reduce its visual mass.
- c. Consideration shall be given to recessing building floors above the first story and providing vertical or horizontal offsets in the wall surfaces at regular intervals, including columns, projections, and recesses, (e.g. every 20 feet).
- d. All sides of the building visible from the street or residential property shall have the same level of architectural detailing as the main elevation.
- e. Primary building entries shall have pedestrian scale and shall be expressed with windows, awnings, trellises, articulation, arcades, landscape, planters, and material changes or other design elements so that the building entry is easily identified and visible from the street and parking lot.
- f. Architectural detail at the street level shall establish human scale and enhance the building's and the streetscape's appearance.

3. Roofs

- a. Roofs shall be compatible with the architectural style and scale of the building.
- b. The roof shape shall reflect the configuration of the building's mass and volume, and shall be consistent in its character where visible from public streets.
- c. Rooflines for large buildings shall be broken up and varied by providing change in the height of a portion of the roof(s), change in form, or other articulations. High pitched "A-frame" type rooflines and partial mansards should be avoided.

4. Building Materials and Colors

- a. Innovative use of durable, high quality materials such as brick, stone, tile, stucco, and concrete is encouraged.

Industrial Zoning Districts

- b. Incorporation of energy conservation features is encouraged in industrial buildings to exceed California's 2005 Title 24 regulation standards for building energy efficiency.
- c. Use a color scheme (for example: two analogous colors plus a complementary trim color) to create visual interest and enhance the streetscape appearance of the building.
- d. Use earthtone colors and keep the number of colors used on the building and within the overall streetscape to a minimum.
- e. Use smaller, articulated, varied color and texture wall surfaces rather than larger homogenous ones.
- f. The same exterior colors and finishes shall be used on all sides of the structure.
- g. Building color shall not be used as signage, branding, or business identification.
- h. Awning materials shall be compatible with the overall design and character of the building. The use of fabric awnings is encouraged. The use of vinyl and plastic awnings is discouraged.

E. Parking and Circulation

1. Site access shall allow for easy circulation throughout the project to minimize conflicts between vehicles and pedestrians, between employee/visitor traffic and truck shipping/delivery. Separate walkways shall be provided to the public way from the main entrance. Main pedestrian routes through parking areas to building entrances shall have decorative paving treatment or other demarcation of pedestrian right-of-way.
2. Enhance primary entry drives for automobiles, especially visitors, with ornamental landscaping, special treatment, and monument type signs to communicate site access locations.
3. Parking lots shall not be the dominant visual element of the site. It is generally more visually appealing to locate parking lots along the side or the rear of buildings. Small customer-oriented parking lots are appropriate toward the front of the site; however, the majority of employee parking should be located to the rear of the site to the maximum extent feasible. To avoid large expanses of paved areas, large parking lots should be divided into smaller parking areas. Buildings should not be located in a manner that make them appear like "islands" surrounded by paved areas.
4. The number of site accesses (ingress/egress) shall be controlled in terms of the location and number of driveways to minimize traffic safety conflicts, street congestion, and unnecessarily disrupted street frontage. Where possible, adjoining properties should share access driveways to minimize the number of driveways along public streets. Shared service or secondary access alleys shall also be considered. Use of an existing side street for primary or secondary access is encouraged as opposed to creating one or more new curb cuts on a collector or arterial street.
5. New streets for industrial projects shall be designed to handle heavier loads associated with truck operations, as necessary. Larger turning movements shall also be included in new streets to facilitate truck movements.

Industrial Zoning Districts

F. Landscaping

1. Emphasis shall be placed on California or Mediterranean style landscaping, particularly indigenous plants, ornamental vines, and flowers in either container pots or as part of an arbor/trellis. Landscaping must be well maintained with drip irrigation systems for trees/garden beds and pots that do not drain across the pavement.
2. All landscaping shall employ features and techniques that reduce the demand for and consumption of water, including appropriate low-water plants, a high degree of paving permeability and water conserving irrigation techniques and systems. Recycled water should be utilized to the extent possible.
3. Trees in paved areas shall be provided with “deep root” barriers, deep root automatic irrigation, and expandable metal tree grates of adequate size. Root barriers shall be of a material specifically designed for containing tree roots. Irrigation shall be adapted for deep watering.
4. Planting plans for building setbacks should include a hierarchy of plantings in terms of size and types of plant materials that mark the transition between the horizontal ground plane at the sidewalk or parking area and the tall, vertical facades of buildings.
5. The use of vines, trellises, and landscape screen walls is strongly encouraged on larger areas of unadorned building facades to soften the appearance and to deter graffiti.
6. Increase the amount of landscaping and special landscape features at project entries and pedestrian gathering areas.
7. Minimize the quantity of impervious paving in parking areas and maximize the use of landscaping or permeable pavement on-site to reduce surface water runoff and the need for water detention basins.
8. Where it is infeasible to locate all large parking areas to the rear of the building, views of expansive paved areas from public rights-of-way and/or residential areas shall be minimized by landscaping within and along the perimeter of parking lots.
9. Required parking lot landscaping shall include the use of landscape planters along parking aisles throughout the parking lot.
10. Streetscapes shall incorporate a mix of trees, shrubbery, and ground cover to establish a landscape corridor presence.
11. Consideration shall be given to the incorporation of open space areas in the form of courtyards, plazas, shaded arcades and functional landscaped areas. These features should link adjoining buildings and take advantage of outdoor as well as indoor space. These features can be located in areas with recessed facades or setbacks in excess of minimum standards, and may be designed for use by employees and/or customers. Pedestrian features such as benches, tables, fountains, artwork, and landscaping should be incorporated as focal points or relaxation area.

Industrial Zoning Districts

G. Lighting

1. The design and location of outdoor lighting fixtures shall preclude direct glare onto adjoining non-industrial property and streets.
2. Outdoor lighting shall be designed to foster security. Illumination should be increased at building entries to increase visibility and safety.
3. Use landscape lighting and exterior lighting to enhance project design features. Lighting fixtures shall have sharp cut off so that no direct lighting falls onto adjacent properties or the public right-of-way.
4. As applicable, illuminate flags only with very narrow spot type lamps directed no more than 15 degrees from vertical. Shield fixtures so that the light source is not visible from off-site.

H. Equipment, Service Area, and Refuse Area Screening

1. Roof-mounted utility and communication equipment shall be screened from view by structural features that are an integral part of the building's architectural design.
2. Loading and service areas shall be concealed from view within the building envelope or shall be located to the rear of the site and designed for minimal visual impact and circulation conflicts.
3. When trash enclosures, loading docks, utility equipment, and similar uses are visible from a street, they shall be screened using materials, colors, and landscaping that are harmonious with the site design and building architecture.
4. Trash enclosures shall provide an area for recycling.

I. Metal Buildings

Metal buildings present their own special set of challenges and opportunities in the community context. If metal buildings are well articulated and surfaces are judiciously mixed with other materials, or textures, and colors, they can make an attractive contribution to industrial park appearance.

1. Metal buildings should incorporate architectural features such as gable, hip, and other sloping roof forms, prominent main entries, windows, storefront, glazed doors, canopies and window awnings, use of brick or stone at building base, and architectural lighting fixtures to make a positive contribution to the streetscape.
2. Street frontages of metal buildings shall present sloping or articulated roof forms. Eaves and steeper roof pitches are more attractive than shallow roofs and roofs without eaves.
3. As with all building design, roofs, particularly metal roofs, contribute significantly to a building's appearance and character. Variety in roof shapes and colors should complement the scale of the building. Darker, non-glare colors help reduce the mass of metal roof designs. Metal roofs can incorporate standing seam, tile and shake materials to create visual interest in design.

Industrial Zoning Districts

4. Long, stark, and uninterrupted panels used for metal buildings shall be avoided. Use of panels with continuous vertical seams shall also be avoided. Other building materials should be incorporated into structural design to add contrast, variety, and visual interest in building form. Wall systems should use techniques that hide or disguise wall fastening systems and seams. Building features such as columns, curved metal corners, deep reveals at construction joints or other details should be incorporated into building design to add interest into the architectural design.
5. Window treatments can provide a key design element for metal buildings. Windows shall particularly be incorporated along the street front elevation(s) to help metal buildings incorporate human-scale design elements that address the building to the street. Windows should incorporate changes in building plane by either recessing or projecting them as integral parts of the overall design theme. Detailed window fenestration should be incorporated around windows including change in relief, color, pattern, and/or materials.
6. Downspouts shall be concealed unless they are part of the design, in which case downspouts shall be coated to match the wall color.
7. Any freestanding outbuildings shall use forms, shapes and materials that are consistent with the main structure.
8. Large expanses of light colored metal wall materials shall be avoided. Darker colors help visually reduce the prominence of large metal buildings. Horizontal color bands, and wall projections and recesses, provide shadowing to accentuate differentiation for wall designs. Consider using corrugated or flat fiber-reinforced cement panels, as well as metal.